Standard Drawing Register Issue Date 19/12/2025 Approved: _Brian Trower_____ Date: 19/12/25

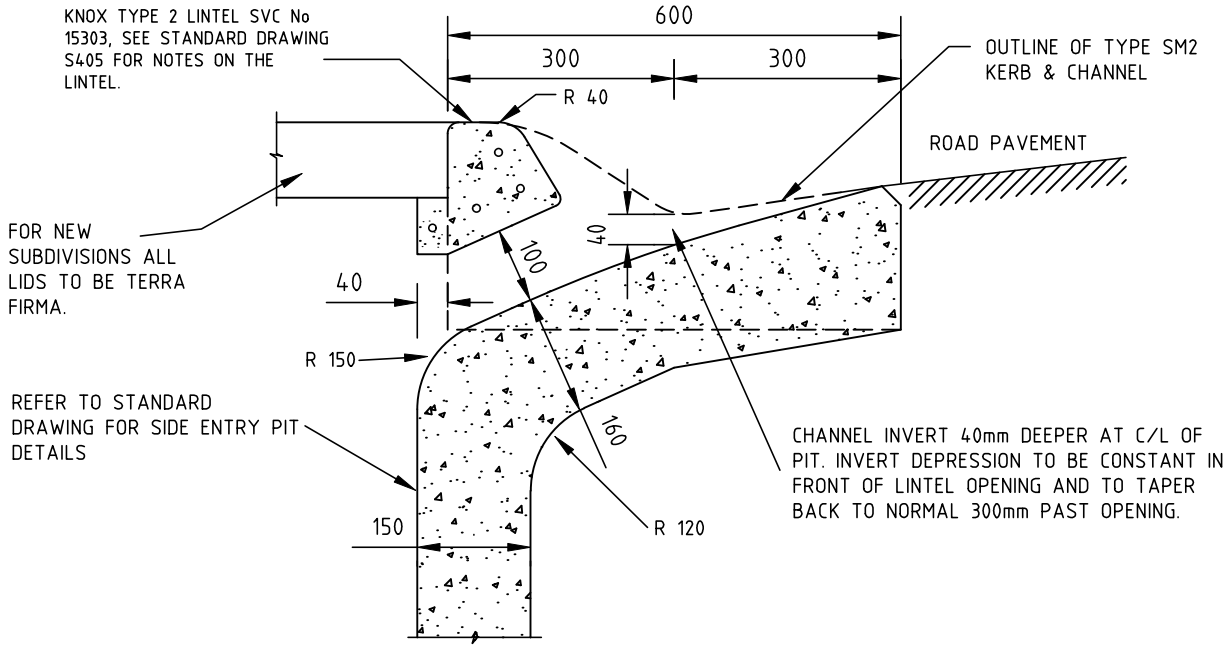
Category	DWG NO.	DRAWING NAME	CURRENT ISSUE DATE
Kerb Profiles	S101	Semi-Mountable kerb type SM1 & SM2 and Modification at side entry pits for types SM2	19/12/2025
	S102	Roll over kerb and channel Modification at side entry pits	19/12/2025
	S103	Kerb and channel type BK1, BK3 and Type BK2	19/12/2025
	S104	Profile of Concrete Flood Dish	19/12/2025
	S105	Semi-Mountable kerb Type SM3 & Outfall Rollover kerb and channel Type R2	19/12/2025
Vehicle Crossings	S201	Standard Vehicle Crossing for residential areas	19/12/2025
	S202	Reverse fall Vehicle Crossing for residential areas	19/12/2025
	S203	Heavy duty vehicle crossing for use in Industrial and commercial areas	19/12/2025
	S204	Side entry pit modification for vehicle crossing construction	19/12/2025
	S205	Standard Vehicle Crossing for residential areas on arterial roads	19/12/2025
	S206	Reverse fall vehicle crossing for residential areas on arterial roads	19/12/2025
Paths	S302	Concrete footpaths	19/12/2025
	S303	Concrete shared path for bicycles and pedestrians	19/12/2025
	S304	Granitic sand shared path for bicycle and pedestrians within reserves	19/12/2025
Pit Details	S401	Step iron details	19/12/2025
	S402	Type 1 junction pit less than 1.2m deep and for pipe sizes up to 450 dia.	19/12/2025
	S403	Type 2 Junction pit greater than 1.2m deep and less than 2.4m deep and for pip sizes up to 675 dia.	19/12/2025
	S404	Single side entry pit detail	19/12/2025
	S405	Pre-cast lintel ‘Knox’ Type 2 black coloured	19/12/2025
	S406	Type 3 Junction Pit dimensions and construction notes	19/12/2025
	S409	Double side entry pit	19/12/2025
	S410	Channel Grate Pit for roll over kerb and channel Type R1	19/12/2025
	S411	Single Under Channel Grate pit for kerb and channel Type BK1, R1, SM2	19/12/2025
	S412	Double Under Channel Grate pit for kerb and channel Type BK1, R1, SM2	19/12/2025
	S413	Single Side Entry Grate pit for kerb and channel Type BK1, R1, SM2	19/12/2025
	S414	Double Side Entry Grate pit for kerb and channel Type BK1, R1, SM2	19/12/2025
	S415	Junction Pit Cover Rising	19/12/2025
	S416	Junction Pit Cover Lowering	19/12/2025

Standard Drawing issue current at time of construction must be used.

Standard Drawing Register Issue Date 19/12/2025 Approved: _Brian Trower____ Date: 19/12/25

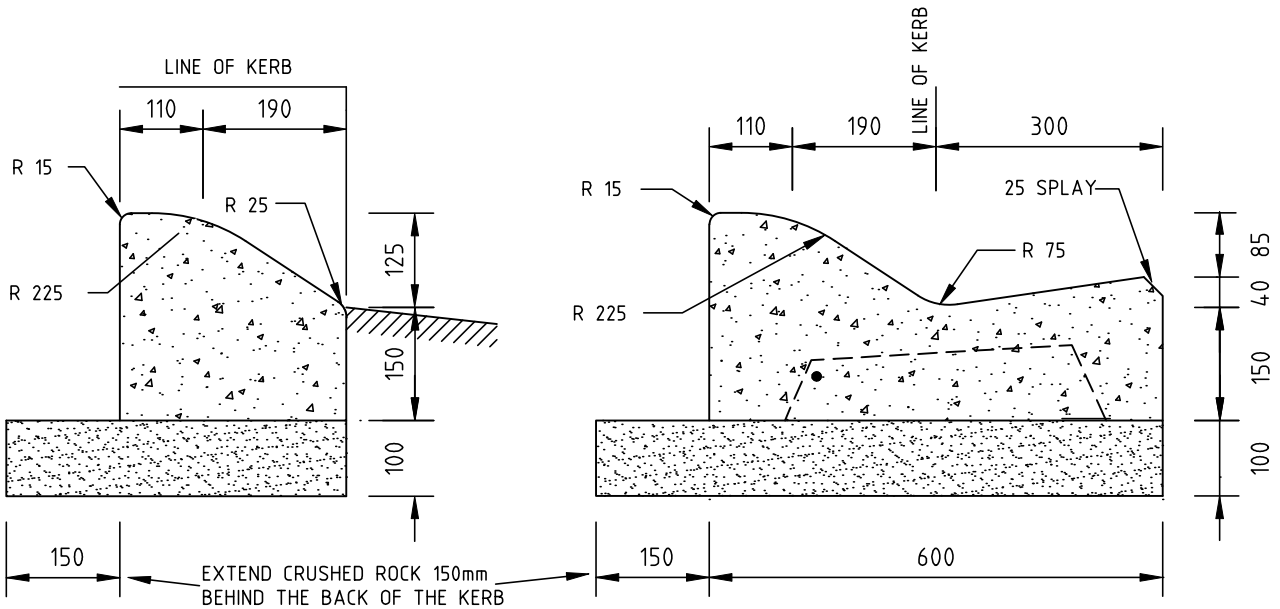
Category	DWG NO.	DRAWING NAME	CURRENT ISSUE DATE
S/W Pipe Connections	S501	Connection of house stormwater drain to kerb and channel	19/12/2025
	S503	Connection of 100 or 150mm drainage pipes to Council concrete stormwater drain.	19/12/2025
	S504	Pipe to pipe connection to council pipes for branch pipes greater than 150mmØ	19/12/2025
	S505	Pipe backfill detail residential pavements, under kerb or in nature strips within 1m of back of kerb	19/12/2025
	S506	Pipe backfill detail easements	19/12/2025
	S507	Pipe backfill under kerb and channel	
Traffic	S601	Give Way & Stop Linemarking general Urban and Rural use	19/12/2025
	S602	Standard Raised Pavement for Non Bus routes (Separate drawing for Bus routes)	19/12/2025
	S603	Standard Raised Pavement for Bus Routes	
	S604	Watts Profile Speed Hump	19/12/2025
	S605	Standard Right of Way construction details	19/12/2025
Tactile Indicators	S701	Tactile indicator requirements for pram crossings flow chart	19/12/2025
	S702	Warning Tactile Indicators	19/12/2025
	S703	Directional Tactile Indicators	19/12/2025
	S704	Path of travel for sight impaired	19/12/2025
	S705	Criteria such that tactile indicators are not required	19/12/2025
	S706	Pram crossing layout	19/12/2025
	S707	Pram ramp dimensions for pram crossings with tactile indicators	19/12/2025
	S708	Tactile indicator layout for flat / angled / long pram ramp	19/12/2025
	S709	Example tactile indicator layout if criteria 1 is not complied with	19/12/2025
	S710	Example tactile indicator layout if criteria 3 is not complied with	19/12/2025
	S711	Change of grade between approach and ramp so that tactile indicators are not required	19/12/2025
	S712	Splitter island example tactile indicator layout	19/12/2025
	S713	Mid block crossings tactile indicator layout	19/12/2025
	S715	Pram ramp dimensions for crossings without tactile indicators	19/12/2025

Standard Drawing issue current at time of construction must be used.



MODIFICATION AT SIDE ENTRY PITS FOR TYPE SM2

-ONLY TO BE USED IN LOW RISK LOCATIONS WHERE CHANNEL GRATE WILL NOT WORK (REFER TO S410)



TYPE SM1

TYPE SM2

NOTES:

ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN. CONCRETE STRENGTH SHALL BE 32 MPA MIN AT 28 DAYS. BEDDING SHALL CONSIST OF A MIN. 100mm COMPACTED DEPTH OF CLASS 2 3% CEMENT STABILISED CRUSHED ROCK 20mm NOMINAL TO FINAL LEVEL & GRADE UNLESS OTHERWISE SPECIFIED
KERB AND CHANNEL COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF 2.0% BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT

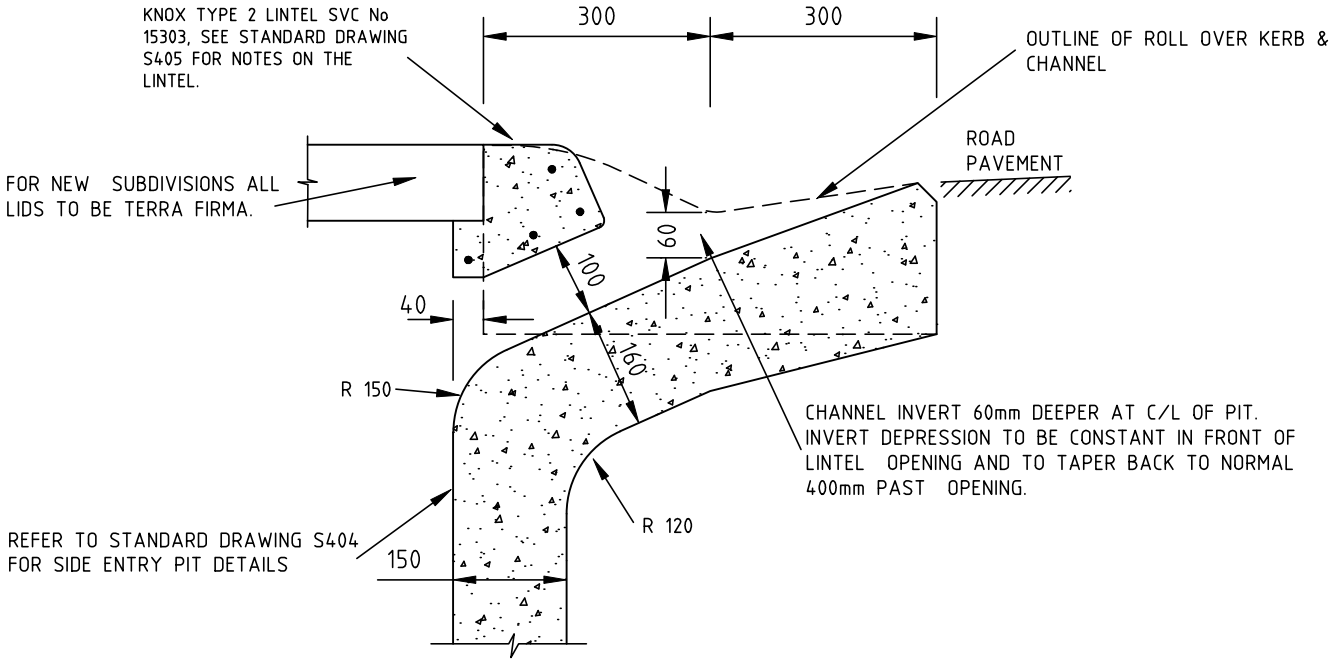
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S101

ISSUE DATE: 19/12/25

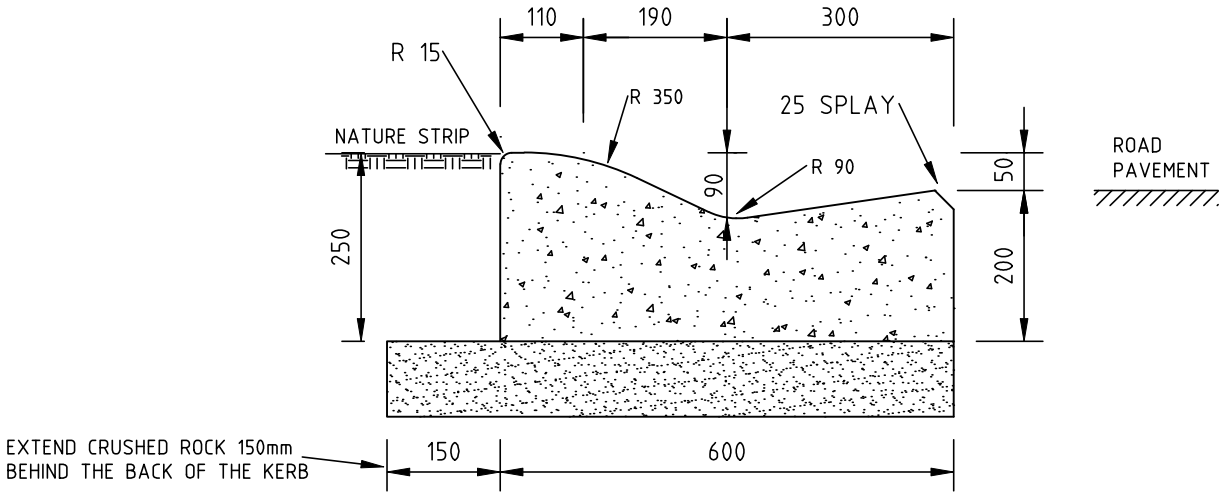
SEMI - MOUNTABLE KERB AND CHANNEL TYPE SM1 AND SM2
MODIFICATION AT SIDE ENTRY PITS FOR TYPE SM2

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



ROLL OVER KERB MODIFICATION AT SIDE ENTRY PITS

-ONLY TO BE USED IN LOW RISK LOCATIONS WHERE CHANNEL GRATE WILL NOT WORK (REFER TO S410)



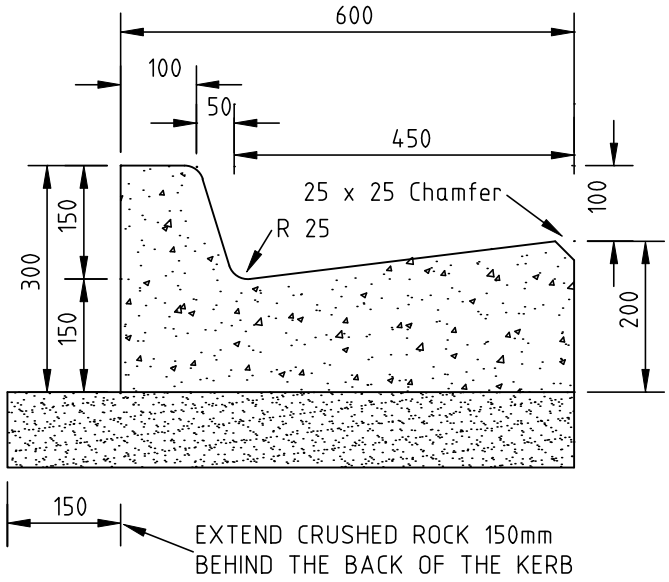
ROLL OVER KERB AND CHANNEL (TYPE R1)

-SEE S105 FOR OUTFALL ROLL OVER KERB AND CHANNEL (TYPE R2)

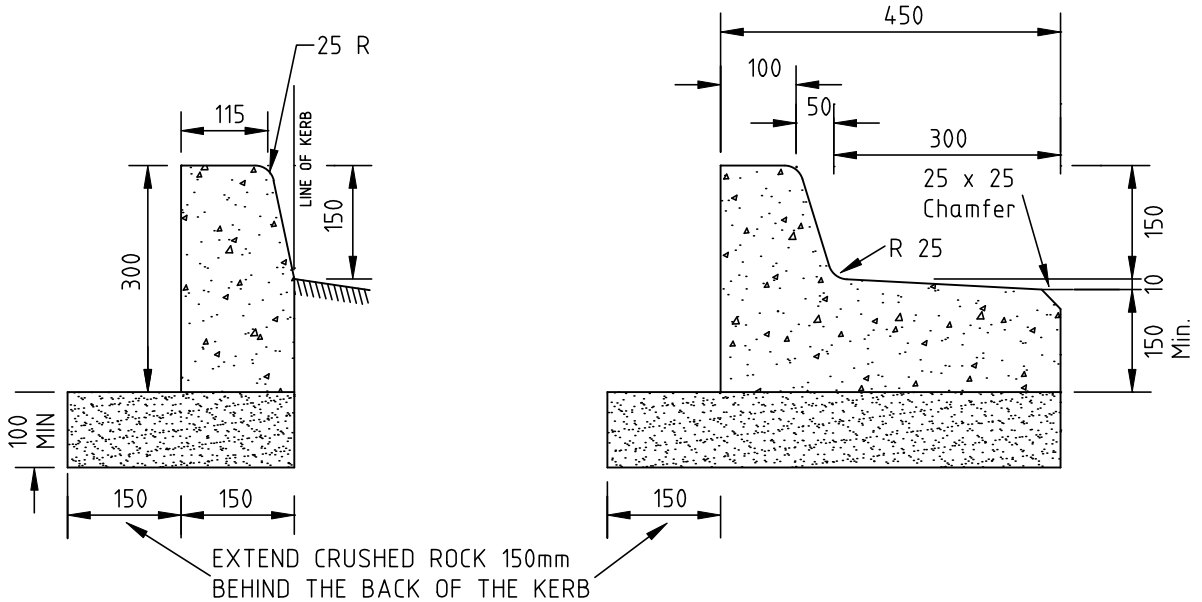
NOTES:

ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN. CONCRETE STRENGTH SHALL BE 32 MPA MIN AT 28 DAYS. BEDDING SHALL CONSIST OF A MIN. 100mm COMPACTED DEPTH OF CLASS 2 3% CEMENT STABILISED CRUSHED ROCK 20mm NOMINAL TO FINAL LEVEL & GRADE UNLESS OTHERWISE SPECIFIED
KERB AND CHANNEL COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF 2.0% BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.

KINGSTON CITY COUNCIL STANDARD DRAWING	DRG. NO. S102
	ISSUE DATE: 19/12/25
ROLL OVER KERB AND CHANNEL TYPE R1 MODIFICATION AT SIDE ENTRY PITS	STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



KERB AND CHANNEL (TYPE BK1)



BARRIER KERB (TYPE BK2)

KERB AND CHANNEL (TYPE BK3)

OUTFALL TRAY

NOTES:

ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN. CONCRETE STRENGTH SHALL BE 32 MPA MIN AT 28 DAYS.
BEDDING SHALL CONSIST OF A MIN. 100mm COMPACTED DEPTH OF CLASS 2, 3% CEMENT STABILISED CRUSHED ROCK 20mm NOMINAL TO FINAL LEVEL & GRADE UNLESS OTHERWISE SPECIFIED
KERB AND CHANNEL COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF 2.0% BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.

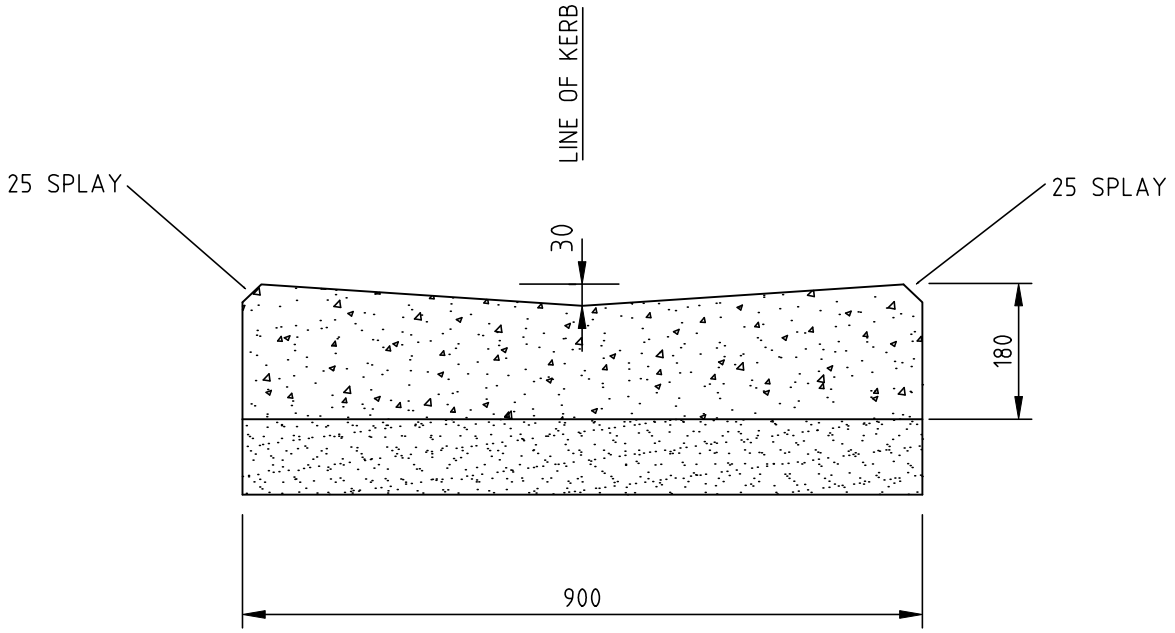
KINGSTON CITY COUNCIL
STANDARD DRAWING

KERB AND CHANNEL TYPE BK1, BK3
AND KERB TYPE BK2

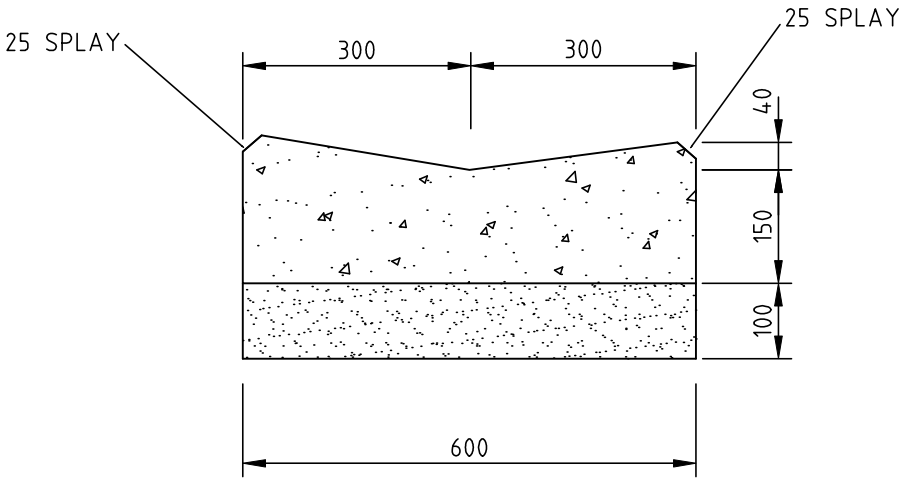
DRG. NO. S103

ISSUE DATE: 19/12/25

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



FLOOD DISH (TYPE FD1)



FLOOD DISH (TYPE FD2)

NOTES:

ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN. CONCRETE STRENGTH SHALL BE 32 MPA MIN AT 28 DAYS. BEDDING SHALL CONSIST OF A MIN. 100 mm COMPACTED DEPTH OF CLASS 2 3% CEMENT STABILISED CRUSHED ROCK 20mm NOMINALTO FINAL LEVEL & GRADE UNLESS OTHERWISE SPECIFIED.
KERB AND CHANNEL COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF 2.0% BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.

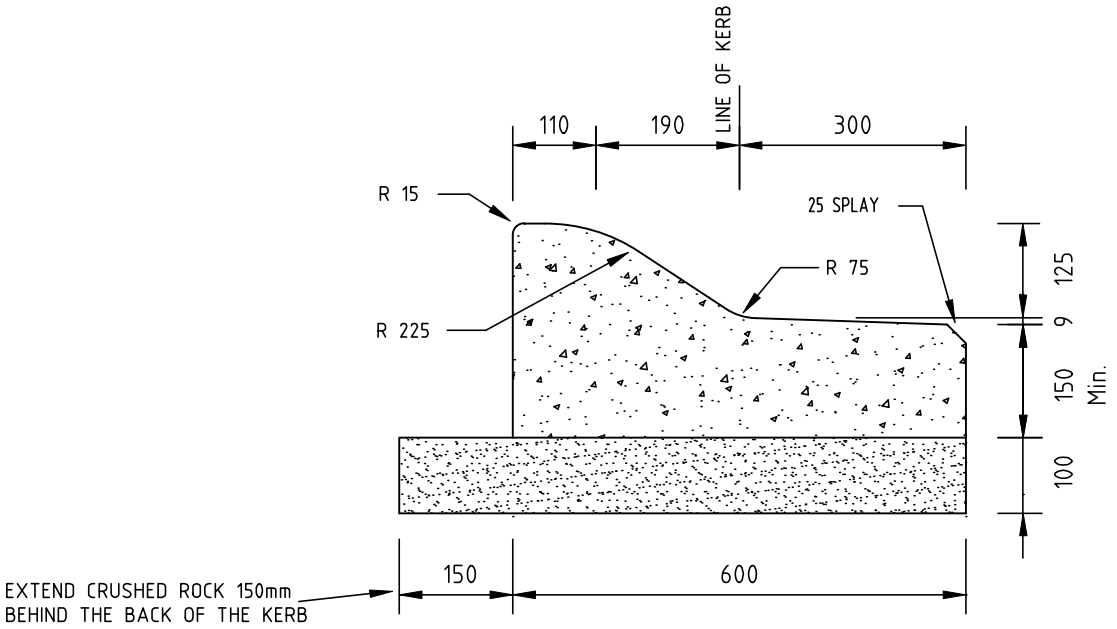
KINGSTON CITY COUNCIL
STANDARD DRAWING

PROFILE OF CONCRETE FLOOD DISH

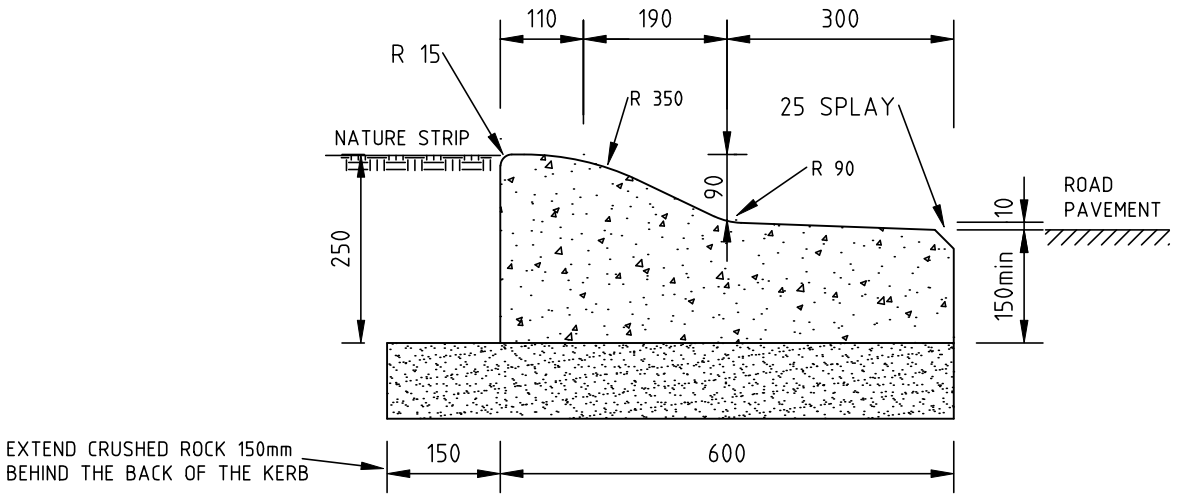
DRG. NO. S104

ISSUE DATE: 19/12/25

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



TYPE SM3



OUTFALL ROLLOVER KERB AND CHANNEL TYPE R2

NOTES:

ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN. CONCRETE STRENGTH SHALL BE 32 MPA MIN AT 28 DAYS. BEDDING SHALL CONSIST OF A MIN. 100mm COMPACTED DEPTH OF CLASS 2 3% CEMENT STABILISED CRUSHED ROCK 20mm NOMINAL TO FINAL LEVEL & GRADE UNLESS OTHERWISE SPECIFIED

KERB AND CHANNEL COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF 2.0% BY WEIGHT IN GREY CEMENT OR OR APPROVED EQUIVALENT.

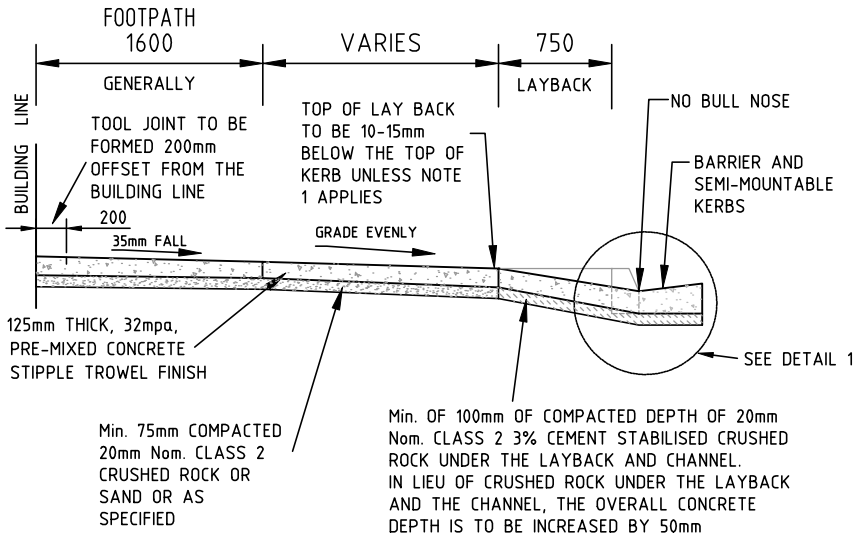
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S105

ISSUE DATE: 19/12/25

SEMI - MOUNTABLE KERB AND CHANNEL TYPE SM3
OUTFALL ROLL OVER KERB AND CHANNEL TYPE R2

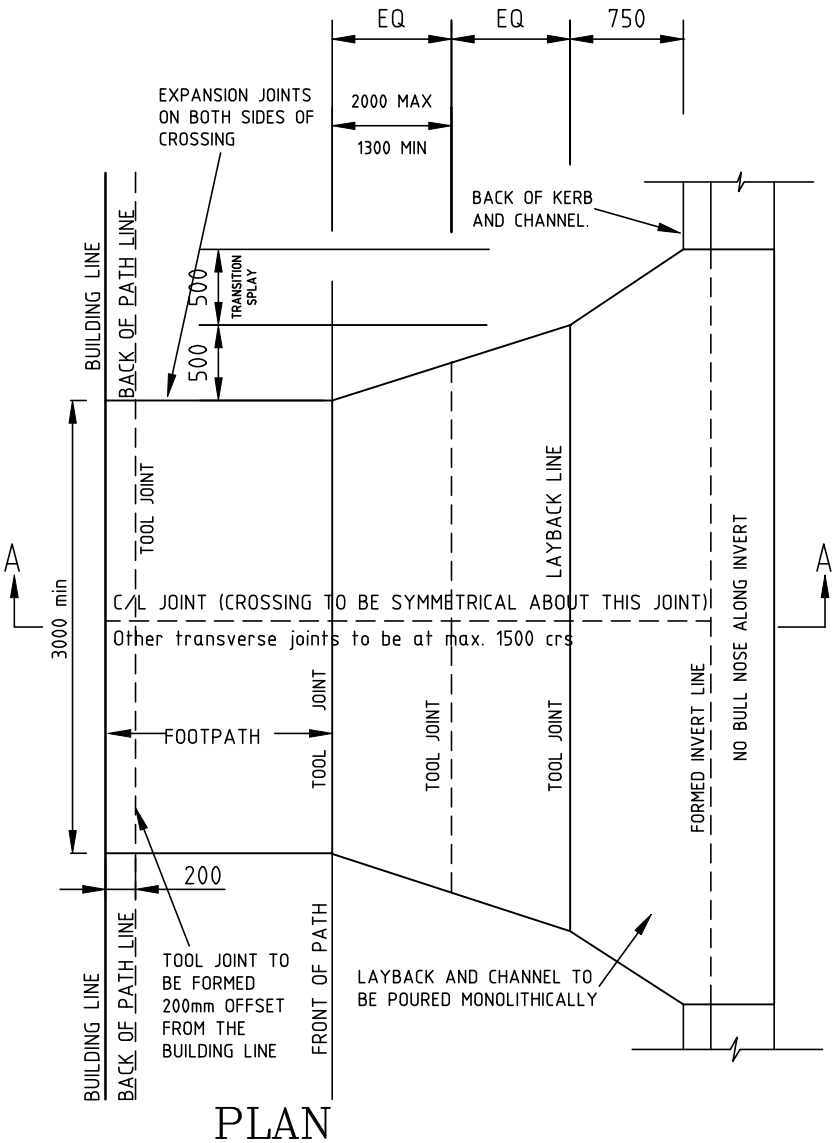
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



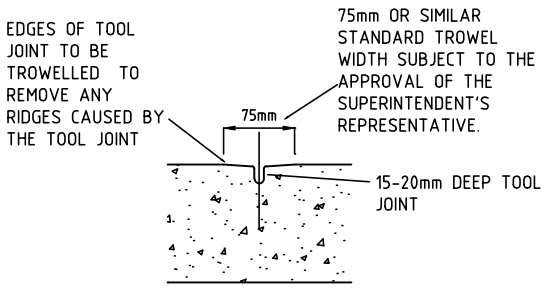
SECTION A – A

NOTES:

1. WHEN LEVEL DIFFERENCE BETWEEN BUILDING LINE AND TOP OF KERB IS LESS THAN 50mm, CROSSING AND LAY BACK ARE TO GRADE EVENLY FROM INVERT OF CHANNEL TO FOOTPATH.
2. ALL NEW CONCRETE INCLUDING VEHICLE CROSSING, FOOTPATH AND KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT. LIQUID WILL NOT BE ACCEPTED.
3. ALL ITEMS TO BE DEMOLISHED SHALL BE SAW CUT AT JOINTS PRIOR TO REMOVAL (INCLUDES ASPHALT TO CONCRETE INTERFACE).
4. EXPANSION JOINTS TO BE 10mm WIDE & 125mm DEPTH ABLEFLEX FOR FULL WIDTH & DEPTH OF EDGE.
5. EXISTING KERB & CHANNEL TO REMAIN FOR ROLL-OVER KERB (WHERE TOP OF KERB IS LESS THAN 90mm ABOVE INVERT). CROSSING TO BE TIED TO BACK OF KERB.
6. WHERE PARTIAL CROSSING REPLACEMENT / CONSTRUCTION IS TO OCCUR, COMPLETE BAYS ARE TO BE REMOVED. REINSTATED BAYS ARE TO BE TIED TO EX. CONCRETE WITH N12 BARS @ 400mm CENTRES, 300mm LONG, 150mm INTO EX. CONCRETE.

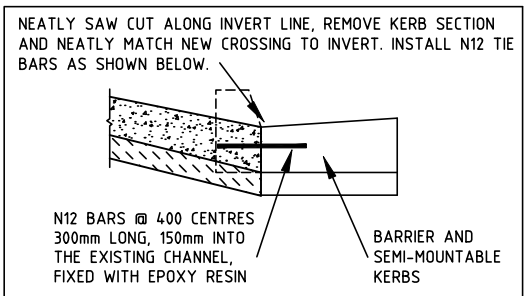


PLAN



TYPICAL TOOL JOINT DETAIL

NOTE - NO HIGHTLIGHTING OF THE TOOL JOINT



DETAIL 1

RETENTION OF THE EXISTING CHANNEL IS ONLY APPLICABLE WHEN WRITTEN CONSENT FROM COUNCIL IS OBTAINED FOR RETENTION OF THE CHANNEL. IF NO CONSENT IS OBTAINED FULL KERB AND CHANNEL MUST BE REMOVED.

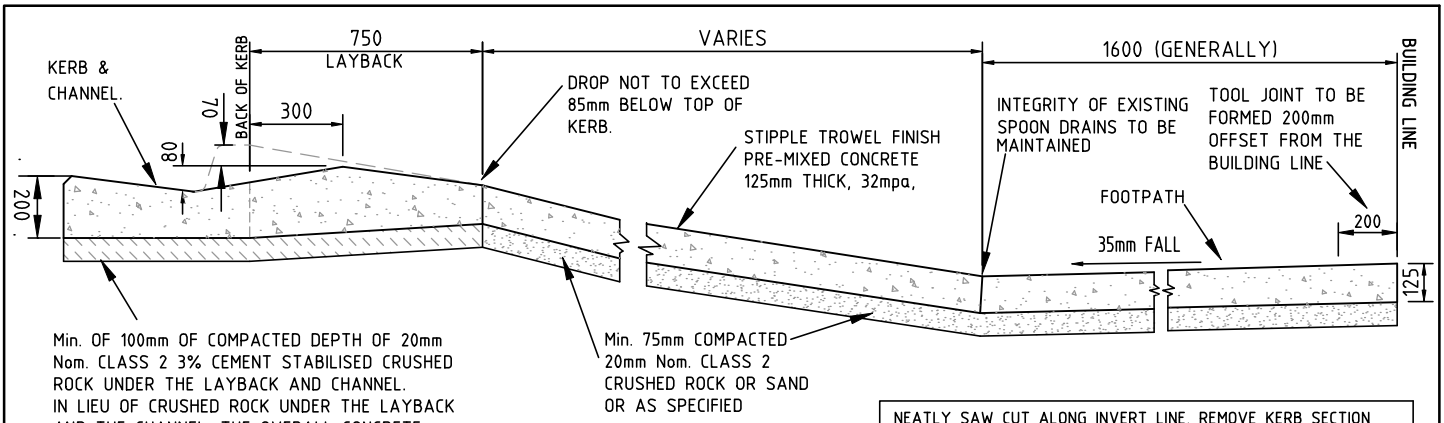
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S201

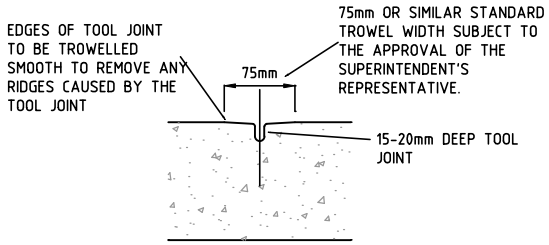
ISSUE DATE: 19/12/25

STANDARD VEHICLE CROSSING FOR RESIDENTIAL AREAS
IF BUILDING LINE >50mm BELOW TOP OF KERB S202 APPLIES

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

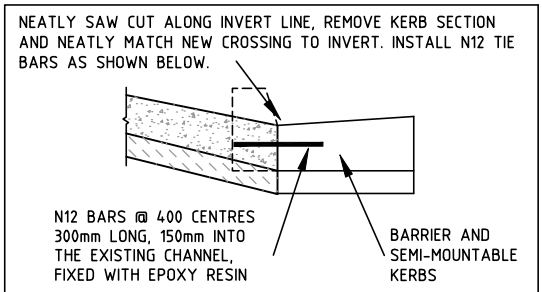


SECTION A–A



TYPICAL TOOL JOINT DETAIL

NOTE - NO HIGHTLIGHTING OF THE TOOL JOINT

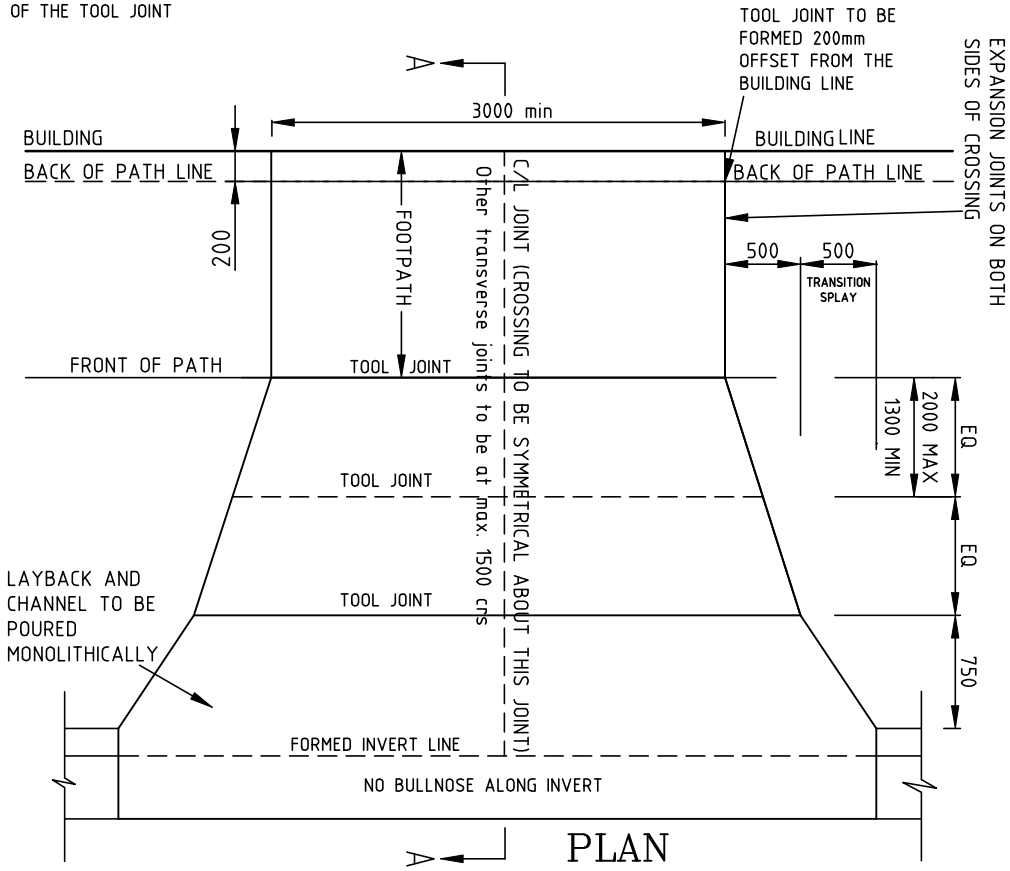


DETAIL 1

RETENTION OF THE EXISTING CHANNEL IS ONLY APPLICABLE WHEN WRITTEN CONSENT FROM COUNCIL IS OBTAINED FOR RETENTION OF THE CHANNEL. IF NO CONSENT IS OBTAINED FULL KERB AND CHANNEL MUST BE REMOVED.

NOTES:

1. ALL NEW CONCRETE INCLUDING VEHICLE CROSSING, FOOTPATH AND KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT. LIQUID WILL NOT BE ACCEPTED
2. ALL ITEMS TO BE DEMOLISHED SHALL BE SAW CUT AT JOINTS PRIOR TO REMOVAL (INCLUDES ASPHALT TO CONCRETE INTERFACE).
3. EXPANSION JOINTS TO BE 10mm WIDE & 125mm DEPTH ABLEFLEX FOR FULL WIDTH & DEPTH OF EDGE.
4. EXISTING KERB & CHANNEL TO REMAIN FOR ROLL-OVER KERB (WHERE TOP OF KERB IS LESS THAN 90mm ABOVE INVERT). CROSSING TO BE TIED TO BACK OF KERB.
5. WHERE PARTIAL CROSSING REPLACEMENT / CONSTRUCTION IS TO OCCUR, COMPLETE BAYS ARE TO BE REMOVED. REINSTATED BAYS ARE TO BE TIED TO EX. CONCRETE WITH N12 BARS @ 400mm CENTRES, 300mm LONG, 150mm INTO EX. CONCRETE.



PLAN

KINGSTON CITY COUNCIL
STANDARD DRAWING

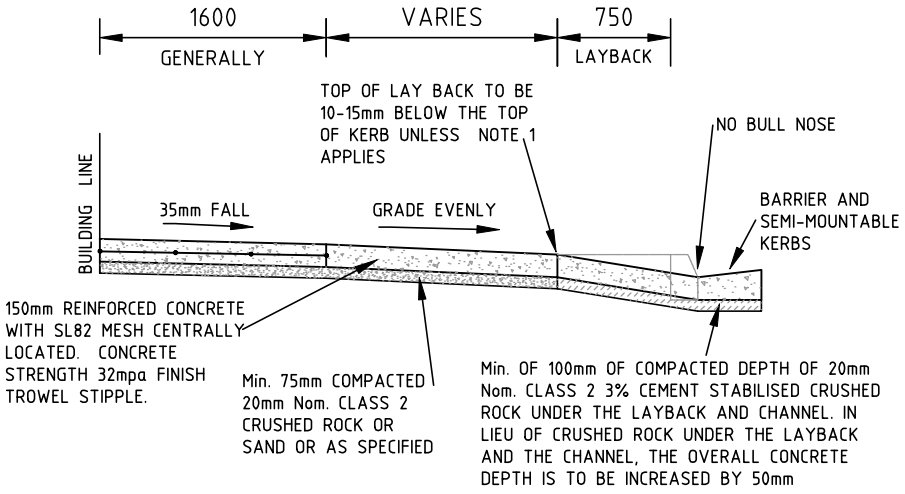
DRG. NO. S202

ISSUE DATE: 19/12/25

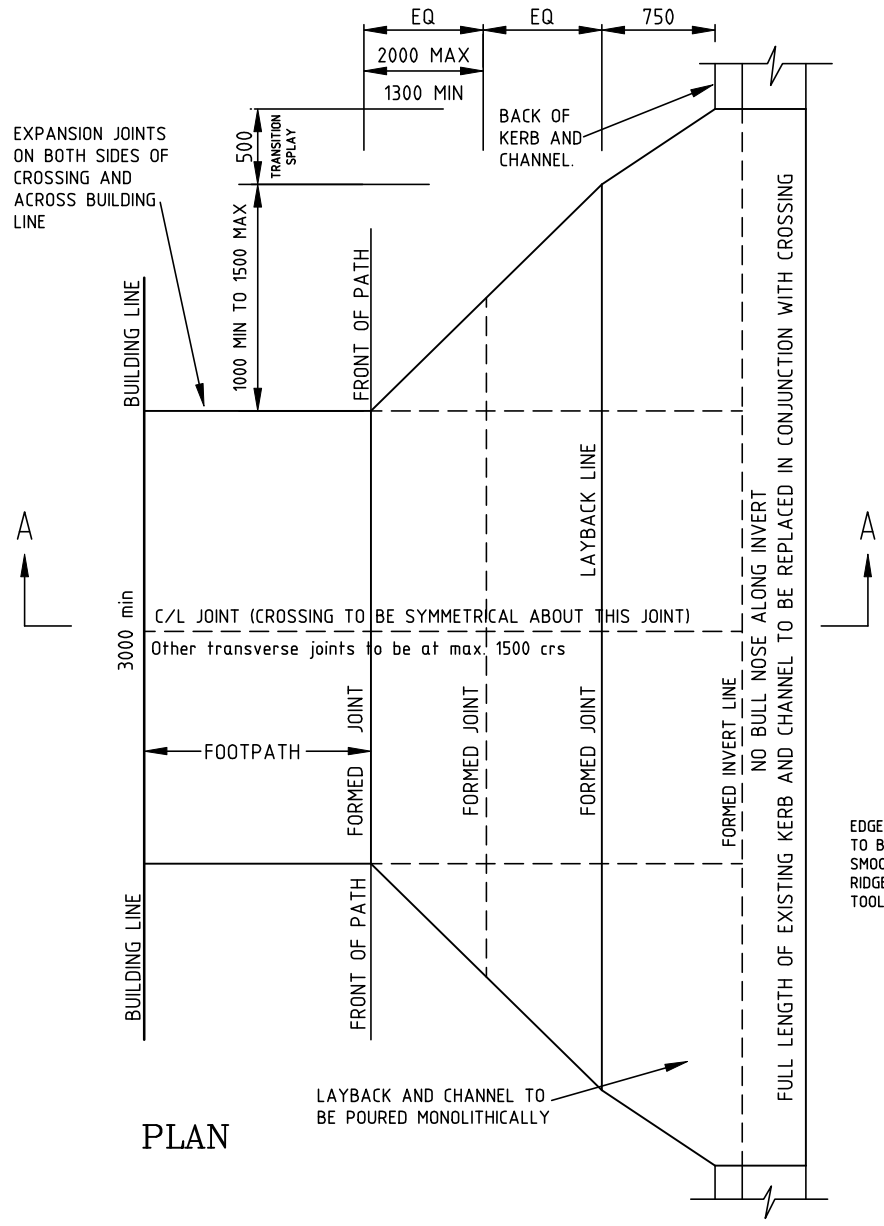
REVERSE FALL VEHICLE CROSSING FOR RESIDENTIAL AREAS

BUILDING LINE IS MORE THAN 50mm BELOW TOP PF KERB LEVEL - SEE S201 IF THIS CONDITION IS NOT MET

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



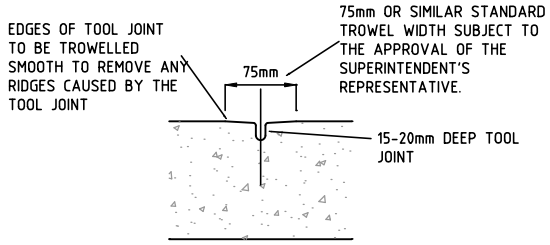
SECTION A – A



PLAN

NOTES:

1. WHEN LEVEL DIFFERENCE BETWEEN BUILDING LINE AND TOP OF KERB IS LESS THAN 50mm, CROSSING AND LAY BACK ARE TO GRADE EVENLY FROM INVERT OF CHANNEL TO FOOTPATH.
2. ALL NEW CONCRETE INCLUDING VEHICLE CROSSING, FOOTPATH AND KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT. LIQUID WILL NOT BE ACCEPTED.
3. ALL ITEMS TO BE DEMOLISHED SHALL BE SAW CUT AT JOINTS PRIOR TO REMOVAL (INCLUDES ASPHALT TO CONCRETE INTERFACE).
4. EXPANSION JOINTS TO BE 10mm WIDE & FILLED WITH CORK OR BITUMINOUS IMPREGNATED PARTICLEBOARD FOR FULL WIDTH AND DEPTH OF THE EDGE.
5. WHERE PARTIAL CROSSING REPLACEMENT / CONSTRUCTION IS TO OCCUR, COMPLETE BAYS ARE TO BE REMOVED. REINSTATED BAYS ARE TO BE TIED TO EX. CONCRETE WITH N12 BARS @ 400mm CENTRES, 300mm LONG, 150mm INTO EX. CONCRETE
6. IF THE BUILDING LINE IS GREATER THAN 50mm BELOW THE TOP OF KERB NO PREPARATION WORKS ON THE VEHICLE CROSSING ARE TO COMMENCE UNTIL A DRAINAGE PLAN IS APPROVED BY COUNCIL AND THE APPROVED DRAINAGE WORKS ARE CONSTRUCTED.



TYPICAL TOOL JOINT DETAIL

NOTE - NO HIGHTLIGHTING OF THE TOOL JOINT

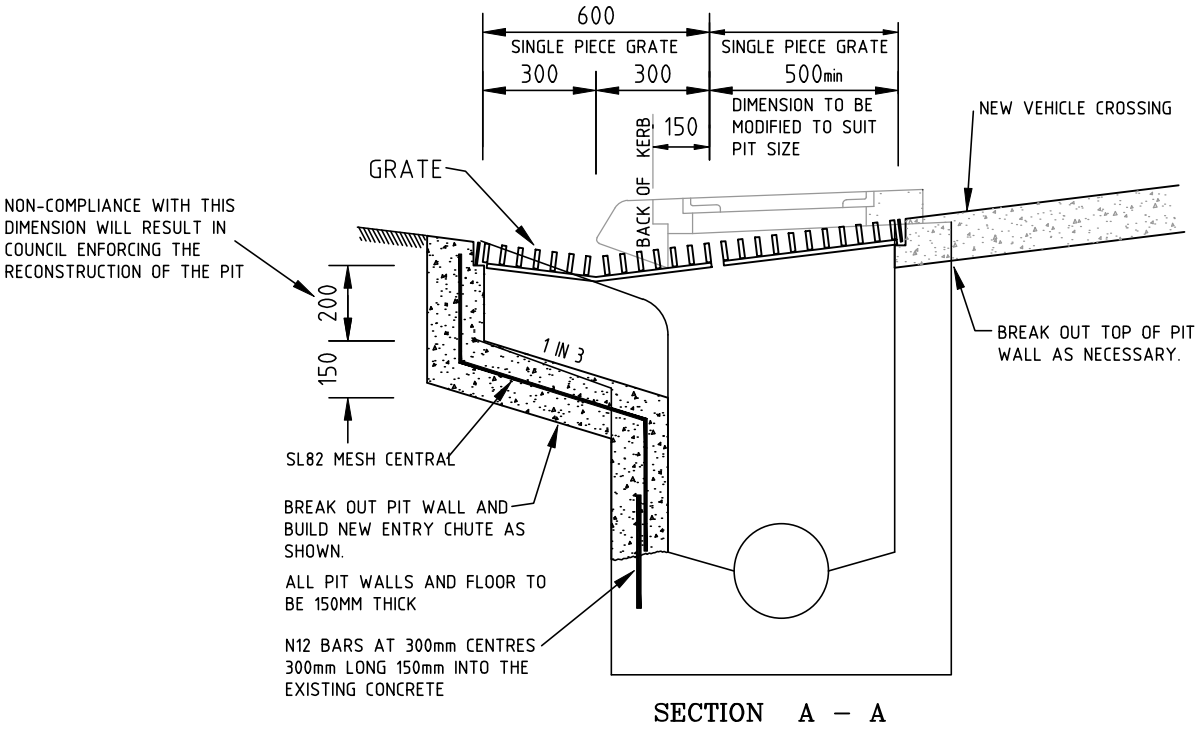
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S203

ISSUE DATE: 19/12/25

HEAVY DUTY VEHICLE CROSSING
FOR USE IN INDUSTRIAL AND COMMERCIAL AREAS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



SECTION A – A

NOTES

1. ALL NEW COVER DIMENSIONS GIVEN ON THIS DRAWING MUST BE VERIFIED FOR SUITABILITY AGAINST THOSE OF THE PIT BEING MODIFIED BEFORE ORDERING COMPONENTS.

2. ALL GRATE COMPONENTS TO BE CLASS D AND GALVANISED. APPROVED SUPPLIERS AS OF FEBRUARY 2025.
   R&S GRATING, BIKE SAFE GRATES
   13 HEALY ROAD DANDENONG SOUTH.
   Ph. 9238 5888
   www.grating.com.au
   SVC CIVIL, PRECAST CONCRETE PRODUCTS
   39 JAPADDY STREET, MORDIALLOC
   Ph. 1300 287 782
   https://svc.com.au/

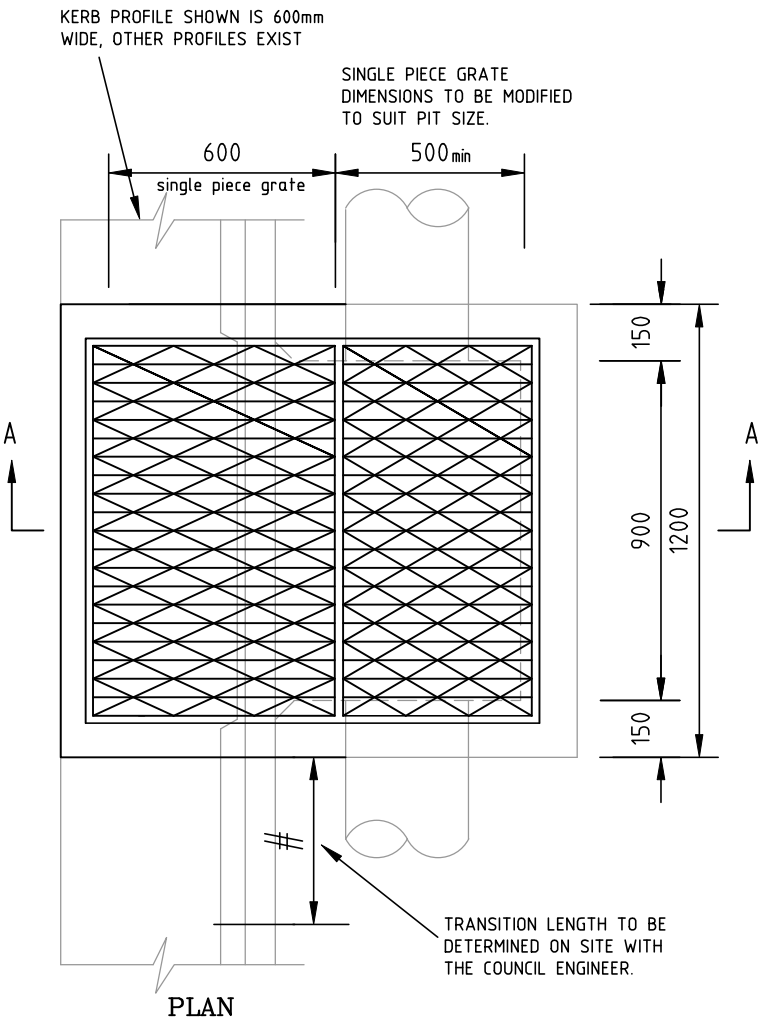
3. DIMENSIONS GIVEN ON THIS DRAWING ARE FOR THE CURRENT STANDARD 900x600 SIDE ENTRY PIT. DUE TO THE LARGE VARIATION IN PIT SIZES WITHIN THE CITY OF KINGSTON, INDIVIDUAL PITS MUST BE MEASURED BEFORE ORDERING COVER COMPONENTS. MOST EXISTING PITS WILL REQUIRE MODIFICATION FROM THE DIMENSIONS SHOWN ON THIS DRAWING.

4. THIS DRAWING GIVES DETAILS FOR A SINGLE FRAME (2 SINGLE PIECE GRATES) WHICH IS SUITABLE FOR VEHICLE CROSSINGS WITH MINIMAL SLOPE. FOR STEEP VEHICLE CROSSINGS, A TWO PIECE FRAME WILL BE REQUIRED WITH A CHANGE IN GRADE BETWEEN FRAMES.

5. ALL PROPRIETARY COMPONENTS TO BE SUPPLIED BY MANUFACTURERS LISTED OR OTHERS IF APPROVED BY COUNCIL.

6. THIS MODIFICATION CANNOT BE LOCATED WITHIN THE TRANSITION SPLAY ZONE OF THE VEHICLE CROSSING. SEE VEHICLE CROSSING STANDARD DRAWINGS S201, S202 AND S203 FOR THE DEFINITION OF THE TRANSITION SPLAY.

7. ALL GRATES TO HAVE A NON SLIP FINISH APPLIED TO THE TOP OF THE GRATE.



PLAN

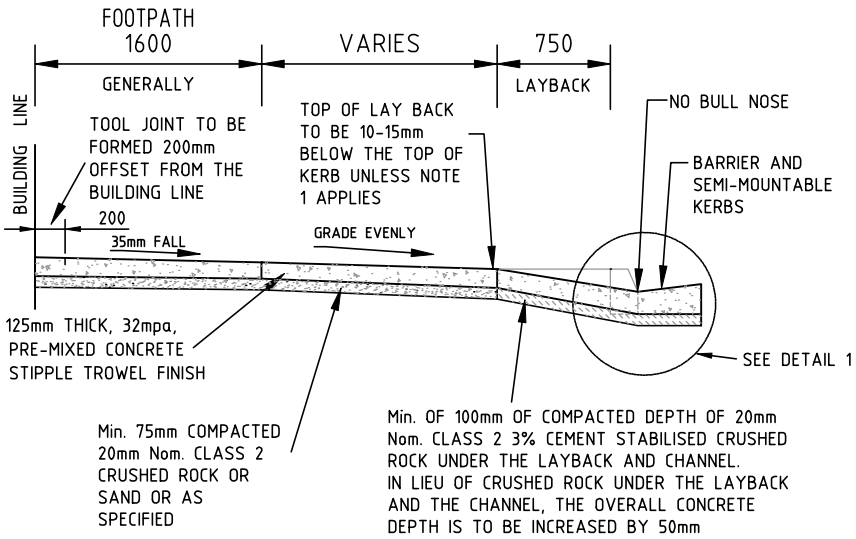
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S204

ISSUE DATE: 19/12/25

SIDE ENTRY PIT MODIFICATION FOR VEHICLE CROSSING CONSTRUCTION

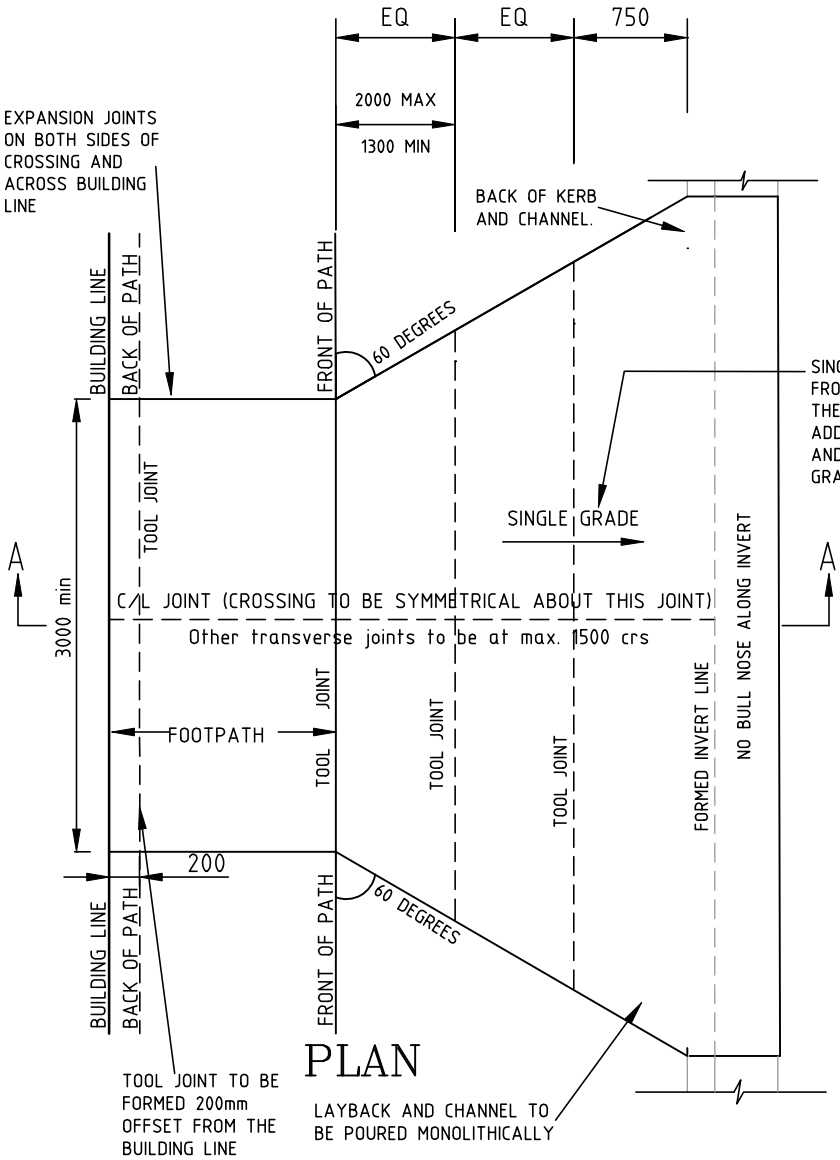
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



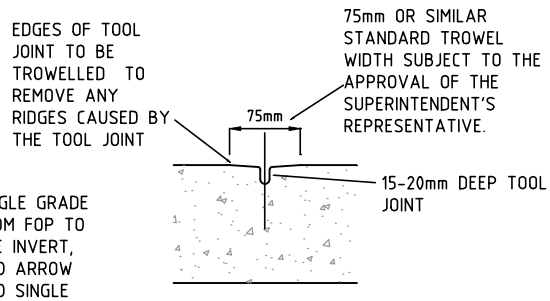
SECTION A – A

NOTES:

1. WHEN LEVEL DIFFERENCE BETWEEN BUILDING LINE AND TOP OF KERB IS LESS THAN 50mm, CROSSING AND LAY BACK ARE TO GRADE EVENLY FROM INVERT OF CHANNEL TO FOOTPATH.
2. ALL NEW CONCRETE INCLUDING VEHICLE CROSSING, FOOTPATH AND KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT. LIQUID WILL NOT BE ACCEPTED.
3. ALL ITEMS TO BE DEMOLISHED SHALL BE SAW CUT AT JOINTS PRIOR TO REMOVAL (INCLUDES ASPHALT TO CONCRETE INTERFACE).
3. EXPANSION JOINTS TO BE 10mm WIDE & 125mm DEPTH ABLEFLEX FOR FULL WIDTH & DEPTH OF EDGE.
4. EXISTING KERB & CHANNEL TO REMAIN FOR ROLL-OVER KERB (WHERE TOP OF KERB IS LESS THAN 90mm ABOVE INVERT). CROSSING TO BE TIED TO BACK OF KERB.
5. WHERE PARTIAL CROSSING REPLACEMENT / CONSTRUCTION IS TO OCCUR, COMPLETE BAYS ARE TO BE REMOVED. REINSTATED BAYS ARE TO BE TIED TO EX. CONCRETE WITH N12 BARS @ 400mm CENTRES, 300mm LONG, 150mm INTO EX. CONCRETE.



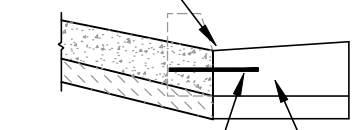
PLAN



TYPICAL TOOL JOINT DETAIL

NOTE - NO HIGHTLIGHTING OF THE TOOL JOINT

NEATLY SAW CUT ALONG INVERT LINE, REMOVE KERB SECTION AND NEATLY MATCH NEW CROSSING TO INVERT. INSTALL N12 TIE BARS AS SHOWN BELOW.



DETAIL 1

RETENTION OF THE EXISTING CHANNEL IS ONLY APPLICABLE WHEN WRITTEN CONSENT FROM COUNCIL IS OBTAINED FOR RETENTION OF THE CHANNEL. IF NO CONSENT IS OBTAINED FULL KERB AND CHANNEL MUST BE REMOVED.

KINGSTON CITY COUNCIL

STANDARD DRAWING

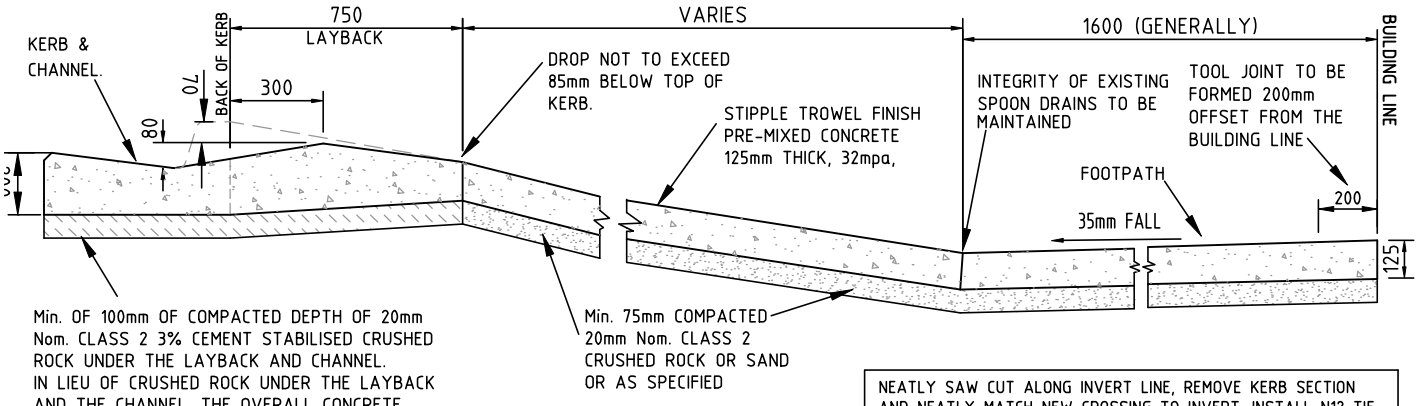
DRG. NO. S205

ISSUE DATE: 19/12/25

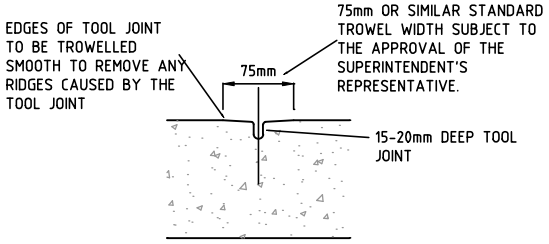
STANDARD VEHICLE CROSSING FOR RESIDENTIAL AREAS ON ARTERIAL ROADS

IF BUILDING LINE >50mm BELOW TOP OF KERB S206 APPLIES

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



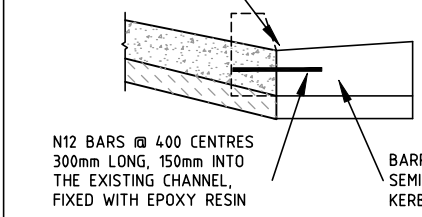
SECTION A–A



TYPICAL TOOL JOINT DETAIL

NOTE - NO HIGHTLIGHTING OF THE TOOL JOINT

NEATLY SAW CUT ALONG INVERT LINE, REMOVE KERB SECTION AND NEATLY MATCH NEW CROSSING TO INVERT. INSTALL N12 TIE BARS AS SHOWN BELOW.



DETAIL 1

RETENTION OF THE EXISTING CHANNEL IS ONLY APPLICABLE WHEN WRITTEN CONSENT FROM COUNCIL IS OBTAINED FOR RETENTION OF THE CHANNEL. IF NO CONSENT IS OBTAINED FULL KERB AND CHANNEL MUST BE REMOVED.

NOTES:

ALL NEW CONCRETE INCLUDING VEHICLE CROSSING, FOOTPATH AND KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT. LIQUID WILL NOT BE ACCEPTED.

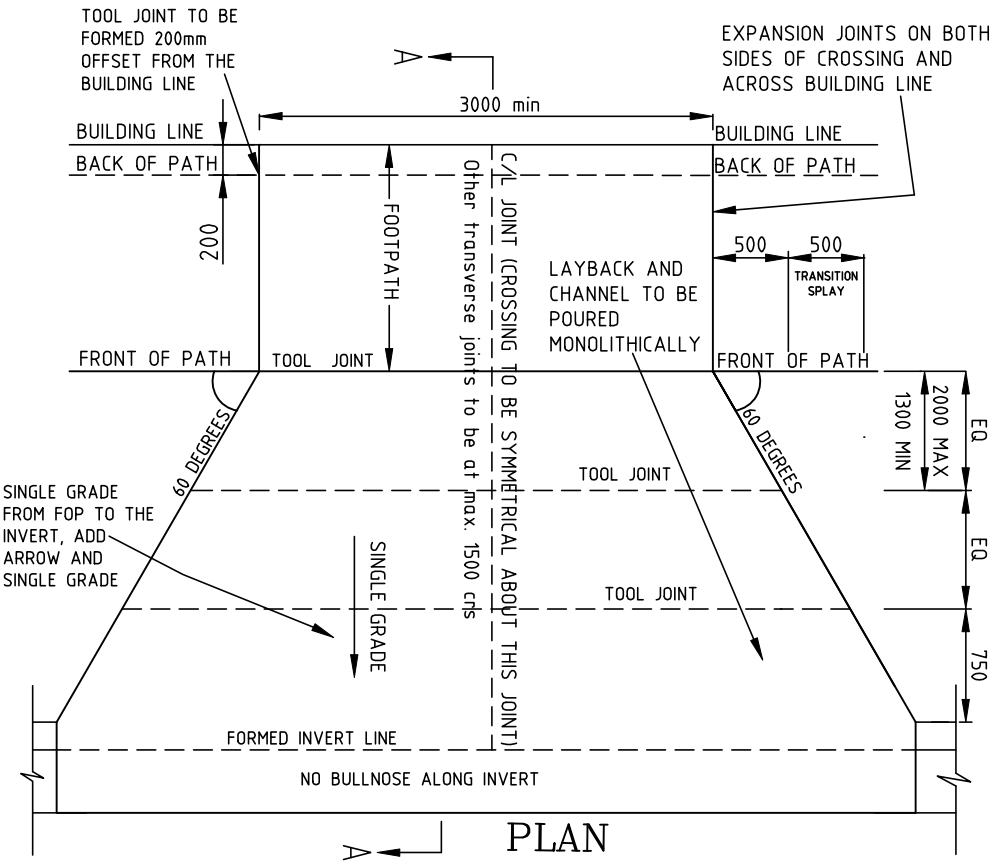
. ALL ITEMS TO BE DEMOLISHED SHALL BE SAW CUT AT JOINTS PRIOR TO REMOVAL (INCLUDES ASPHALT TO CONCRETE INTERFACE).

. EXPANSION JOINTS TO BE 10mm WIDE & 125mm DEPTH ABLEFLEX FOR FULL WIDTH & DEPTH OF EDGE.

. EXISTING KERB & CHANNEL TO REMAIN FOR ROLL-OVER KERB (WHERE TOP OF KERB IS LESS THAN 90mm ABOVE INVERT). CROSSING TO BE TIED TO BACK OF KERB.

. WHERE PARTIAL CROSSING REPLACEMENT / CONSTRUCTION IS TO OCCUR, COMPLETE BAYS ARE TO BE REMOVED. REINSTATED BAYS ARE TO BE TIED TO EX. CONCRETE WITH N12 BARS @ 400mm CENTRES, 300mm LONG, 150mm INTO EX. CONCRETE.

. FOR LOCATION, SPECIFICATIONS AND INSPECTION REQUIREMENTS REFER TO "CITY OF KINGSTON VEHICLE CROSSING GUIDELINES".



PLAN

KINGSTON CITY COUNCIL

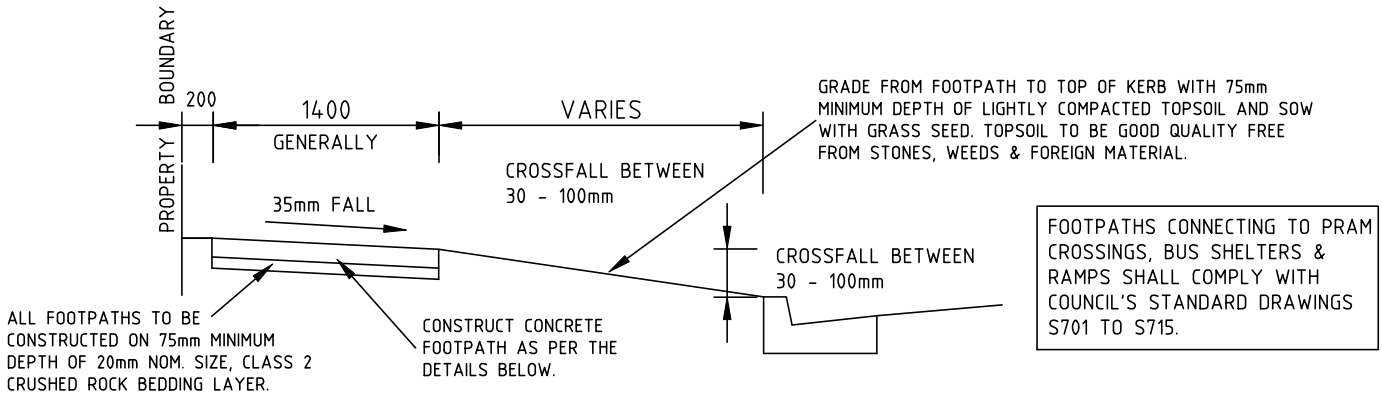
STANDARD DRAWING

DRG. NO. S206

ISSUE DATE: 19/12/25

REVERSE FALL VEHICLE CROSSING FOR RESIDENTIAL AREAS ON ARTERIAL ROADS – IF BUILDING LINE IS MORE THAN 50mm BELOW TOP OF KERB LEVEL – SEE S205 IF THIS CONDITION IS NOT MET

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



FOOTPATHS CONNECTING TO PRAM CROSSINGS, BUS SHELTERS & RAMPS SHALL COMPLY WITH COUNCIL'S STANDARD DRAWINGS S701 TO S715.

LOCATION	PAVEMENT COMPOSITION
RESIDENTIAL AREAS	75mm DEPTH OF CONCRETE, 125mm DEPTH OF CONCRETE FOR SUBDIVISIONS
INDUSTRIAL AREAS COMMERCIAL AREAS	150mm DEPTH OF CONCRETE WITH SL82 CENTRAL MESH.
WITHIN SHOPPING CENTRES	150mm DEPTH OF CONCRETE WITH SL82 CENTRAL MESH AND 600x600mm 40mm DEEP SAWCUT PATTERN.

NOTES:

1. THIS DRAWING SHOWS DETAILS FOR A TYPICAL FOOTPATH LAYOUT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SPECIFIC SITE CONDITIONS HAVE BEEN ALLOWED FOR. REFER TO COUNCIL ENGINEERS FOR A DECISION AT LOCATIONS WHERE THE CONSTRUCTION DOES NOT HAVE CROSSFALL FALLING TOWARDS THE KERB IN THE RANGE OF 30-100mm.
2. IN AREAS OF LOW CBR's AND EXPANSIVE CLAYS THE DEPTH OF THE CONCRETE FOOTPATH AND CRUSHED ROCK BEDDING TO BE REFERRED TO COUNCILS ENGINEERS FOR A DECISION ON REQUIRED DEPTHS.
3. FOR PROPOSED PATHS WHICH EXTEND FROM THE BUILDING LINE TO THE BACK OF KERB REFER TO COUNCIL ENGINEERS FOR GUIDANCE.
4. ALL NEW CONCRETE FOR FOOTPATHS IN RESIDENTIAL AND INDUSTRIAL AREAS TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
5. ALL NEW CONCRETE FOR FOOTPATHS IN SHOPPING CENTRES AND COMMERCIAL AREAS TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **8.3%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
6. WHERE PUBLIC UTILITIES' ASSETS (POWER POLES, STORMWATER PITS, TREE ROOT SYSTEMS, ETC) ARE LOCATED WITHIN THE ALIGNMENT OF THE PROPOSED SHARED PATH, THESE ASSETS MUST BE AVOIDED, RELOCATED OR MODIFIED AT THE DEVELOPER'S COST. ALL SUCH WORKS TO COMPLY WITH THE RELEVANT AUTHORITIES' REQUIREMENTS.
7. CONCRETE STRENGTH SHALL BE 32 MPA MINIMUM AT 28 DAYS. ALL CONCRETE TO BE PRE-MIXED AND HAVE A LIGHT BROOM FINISH.
8. BOTH EDGES OF THE PATH MUST BE POURED AGAINST SMOOTH FORMWORK.
9. ALL EDGES TO BE ROUNDED BY THE PROPER USE OF A SUITABLE EDGE TOOL.
10. NO HIGHLIGHTING OF EDGES AND JOINTS.
11. SPACING OF EXPANSION JOINTS MUST NOT EXCEED 15m. EXPANSION JOINTS ARE TO BE CONNOLLY JOINTS. REFER TO:
    CONNOLLY KEY JOINTS
    9/63-69 PIPE ROAD, LAVERTON NORTH
    Ph. 0477 012 256
    https://www.connollykeyjoint.com/our-products/jointing-solutions/expansion-joint/
    TOOLED JOINTS (5mm WIDE x 20mm DEEP) TO BE FORMED AT 1.5m MAXIMUM SPACING.
12. THE SITE SHALL BE MAINTAINED AT ALL TIMES IN ACCORDANCE WITH A.S 1742.3
13. ALL EXISTING PATHS OR KERBS TO BE DEMOLISHED SHALL BE SAWCUT AT THE JOINTS PRIOR TO REMOVAL.
14. AT THE COMPLETION OF THE CONSTRUCTION WORKS, ALL AREAS DISTURBED DURING THE CONSTRUCTION e.g. KERBS, FOOTPATHS, VEHICLE CROSSINGS, ROAD PAVEMENT, SIGNS etc. ARE TO BE REINSTATED BY THE CONTRACTOR. ALL CONCRETE REINSTATEMENT IS TO BE CARRIED OUT BETWEEN EXISTING JOINTS. EXCAVATED MATERIAL, INCLUDING PIPES, PITS AND BROKEN CONCRETE IS TO BE REMOVED FROM THE SITE AND CARTED TO A SITE DESIGNATED BY THE CONTRACTOR, OR IF DIRECTED BY THE SUPERINTENDENT'S REPRESENTATIVE, TO A SITE WITHIN THE CITY OF KINGSTON. ALL TIPPING CHARGES TO BE BORNE BY THE CONTRACTOR.
15. UPON COMPLETION OF CONSTRUCTION THE WHOLE SITE SHALL BE CLEANED UP, ALL RUBBISH REMOVED AND THE SITE LEFT IN A CLEAN AND TIDY CONDITION TO THE SATISFACTION OF THE SUPERINTENDENT'S REPRESENTATIVE.
16. IT IS THE CONTRACTORS RESPONSIBILITY TO MAINTAIN THE CONSTRUCTION AREA IN A SAFE MANNER AND TO BE SURE THAT ADEQUATE BARRIERS, LIGHTS AND SIGNS ARE INSTALLED AND MAINTAINED WHERE NECESSARY IN ACCORDANCE WITH A.S.1742.3 AND AS DIRECTED BY COUNCIL OFFICERS.

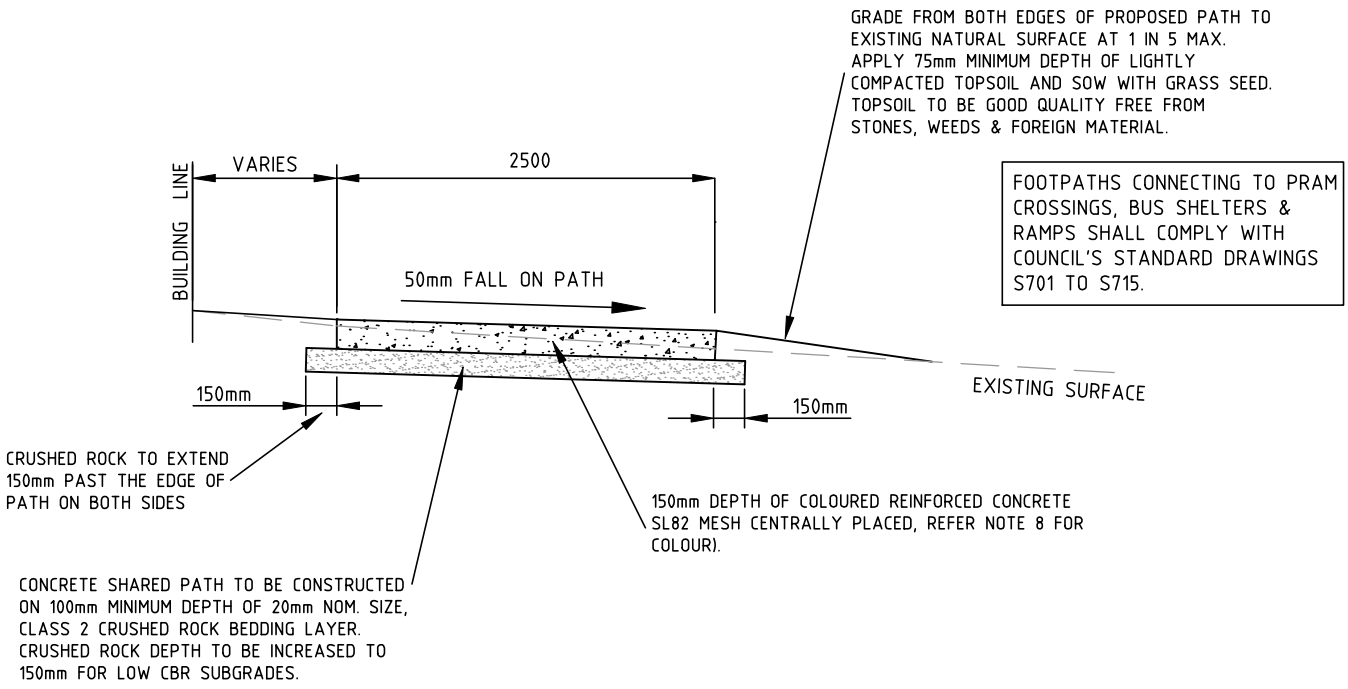
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S302

ISSUE DATE: 19/12/25

CONCRETE FOOTPATHS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



NOTES:

1. THIS DRAWING SHOWS DETAILS FOR A TYPICAL SHARED PATH LAYOUT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SPECIFIC SITE CONDITIONS HAVE BEEN ALLOWED FOR. REFER TO COUNCIL ENGINEER'S FOR A DECISION AT LOCATIONS WHERE THE PROPOSED PATH IS ABOVE EITHER THE BUILDING LINE OR TOP OF KERB LEVELS.
2. WHERE PUBLIC UTILITIES' ASSETS (POWER POLES, STORMWATER PITS, TREE ROOT SYSTEMS, ETC) ARE LOCATED WITHIN THE ALIGNMENT OF THE PROPOSED SHARED PATH, THESE ASSETS MUST BE AVOIDED, RELOCATED OR MODIFIED AT THE DEVELOPER'S COST. ALL SUCH WORKS TO COMPLY WITH THE RELEVANT AUTHORITIES' REQUIREMENTS.
3. CONCRETE STRENGTH SHALL BE 32 MPA MINIMUM AT 28 DAYS. ALL CONCRETE TO BE PRE-MIXED AND HAVE A STIPPLE TROWEL FINISH.
4. BOTH EDGES OF THE PATH MUST BE POURED AGAINST SMOOTH FORMWORK.
5. ALL EDGES TO BE ROUNDED BY THE PROPER USE OF A SUITABLE EDGE TOOL.
6. NO HIGHLIGHTING OF EDGES AND JOINTS.
7. SPACING OF EXPANSION JOINTS MUST NOT EXCEED 15m. EXPANSION JOINTS ARE TO BE CONNOLLY JOINTS. REFER TO:
   CONNOLLY KEY JOINTS
   9/63-69 PIPE ROAD, LAVERTON NORTH
   Ph. 0477 012 256
   https://www.connollykeyjoint.com/our-products/jointing-solutions/expansion-joint/
   TOOLED JOINTS (5mm WIDE x 20mm DEEP) TO BE FORMED AT 1.5m MAXIMUM SPACING.
8. ALL SHARED PATHS MUST BE FULL DEPTH BLACK COLOURED CONCRETE. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF 2.0% IN GREY CEMENT.
9. THE SITE SHALL BE MAINTAINED AT ALL TIMES IN ACCORDANCE WITH A.S 1742.3
10. ALL EXISTING PATHS OR KERBS TO BE DEMOLISHED SHALL BE SAWCUT AT THE JOINTS PRIOR TO REMOVAL.
11. THE APPLICANT IS RESPONSIBLE FOR ENSURING THAT THE WORKS COMPLY WITH ALL REQUIREMENTS OF THE KINGSTON PLANNING SCHEME AND APPLICABLE PERMITS.
12. SHARED USER PATHS SHALL HAVE SIGNS INSTALLED. BICYCLE PATH DESIGN & ASSOCIATED 'SHARED FOOTWAY' SIGNS SHALL COMFORM WITH 'GUIDE TO TRAFFIC ENGINEERING PRACTICE, PART 14 - BICYCLES'.
13. ALL LINEMARKING TO BE INSTALLED TO USE 2 COATS OF CHLORINATED RUBBER PAINT IN ACCORDANCE WITH MANUFACTURER SPECIFICATIONS.
14. SAWCUT JOINTS ARE TO BE PROVIDED AT 2.5m SPACING AT 40mm DEPTH. SAWCUTS MUST BE DONE WITHIN 24 HOURS OF CONCRETE POUR.

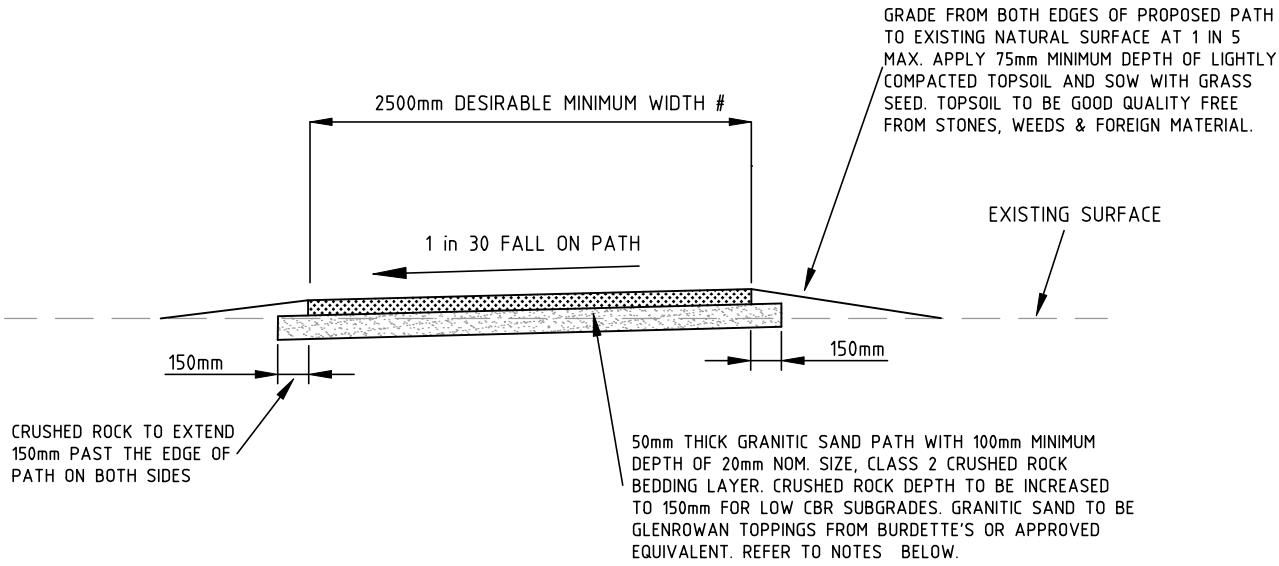
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S303

ISSUE DATE: 19/12/25

CONCRETE SHARED USER PATHS FOR BICYCLES AND PEDESTRIANS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



APPLY TO COUNCIL FOR CONSIDERATION OF ALTERNATIVE WIDTHS UNDER SPECIAL CIRCUMSTANCES.

NOTES:

1. THIS DRAWING SHOWS DETAILS FOR A TYPICAL SHARED PATH LAYOUT. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SPECIFIC SITE CONDITIONS HAVE BEEN ALLOWED FOR. REFER TO COUNCIL ENGINEER'S FOR A DECISION AT LOCATIONS WHERE LEVELS INDICATE THAT THE PROPOSED PATH WOULD CAUSE STORM WATER RUNOFF TO POND.
2. WHERE PUBLIC UTILITIES' ASSETS (POWER POLES, STORMWATER PITS, TREE ROOT SYSTEMS, ETC) ARE LOCATED WITHIN THE ALIGNMENT OF THE PROPOSED SHARED PATH, THESE ASSETS MUST BE AVOIDED, RELOCATED OR MODIFIED AT THE DEVELOPER'S COST. ALL SUCH WORKS TO COMPLY WITH THE RELEVANT AUTHORITIES' REQUIREMENTS.
3. THE SITE SHALL BE MAINTAINED AT ALL TIMES IN ACCORDANCE WITH A.S 1742.3
4. ALL EXISTING ASPHALT OR CONCRETE PATHS OR KERBS TO BE DEMOLISHED SHALL BE SAWCUT AT THE JOINTS PRIOR TO REMOVAL.
5. THE APPLICANT IS RESPONSIBLE FOR ENSURING THAT THE WORKS COMPLY WITH ALL REQUIREMENTS OF THE KINGSTON PLANNING SCHEME AND APPLICABLE PERMITS.
6. GRANITIC SAND TO CONSIST OF CLAY FINES AND SAND PARTICLES TO 10mm DIAMETER. THE CHARACTERISTICS OF THE GRANITIC SAND MATERIAL MUST SATISFY THE REQUIREMENTS OF 'AUSTROADS GUIDE TO PAVEMENT TECHNOLOGY PART 6: UNSEALED PAVEMENTS - SECTION 3.2.1'. GEOTECHNICAL REPORT CONFIRMING THESE REQUIREMENTS MUST BE PROVIDED PRIOR TO DEIVERY.
7. CONTRACTOR IS RESPONSIBLE FOR STOCKPILING GRANITIC SAND ON SITE IF GRANITIC SAND DELIVERED TO SITE CONTAINS HIGHER THAN OPTIMUM MOISTURE CONTENT REQUIRED TO GAIN THE REQUIRED COMPACTION. ANY STOCKPILING TO BE FORMED AND MAINTAINED IN A SHAPE TO ALLOW FOR DRYING AND TO PREVENT NUISANCE TO SURROUNDING PROPERTIES. TIMING FOR PLACEMENT OF GRANITIC SAND IN PATH MUST TAKE INTO ACCOUNT WEATHER CONDITIONS TO ENABLE COMPLETION OF PROJECT WITHIN SPECIFIED TIMELINES.
8. ALL GRANITIC SAND PATHS WILL REQUIRE A 12 MONTH PERIOD OF FINE TUNING THE FINISHED SURFACE LEVEL INCLUDING REPAIR OF SOFT SPOTS. DESIGN FINISHED SURFACE LEVELS MUST BE MAINTAINED OVER THIS 12 MONTH PERIOD. THESE CONDITIONS WILL FORM PART OF ANY PLANNING PERMIT CONDITIONS TO CONSTRUCT GRANITIC SAND PATHS.

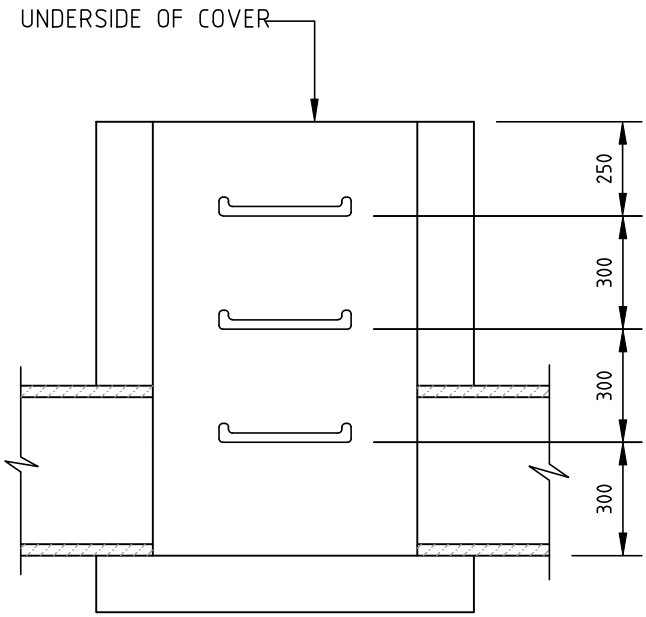
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S304

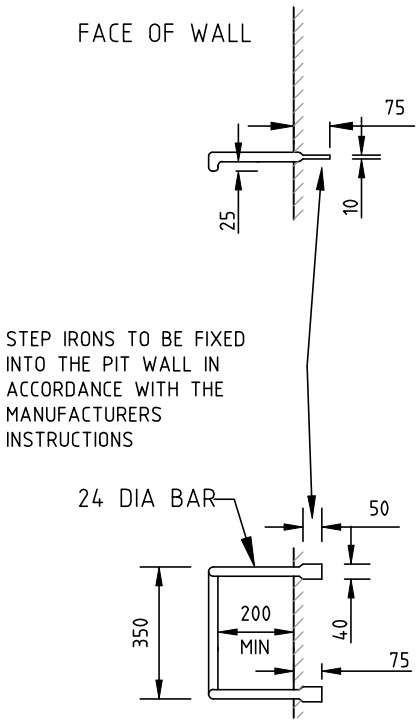
ISSUE DATE: 19/12/25

GRANITIC SAND SHARED PATH FOR BICYCLES AND PEDESTRIANS WITHIN RESERVES

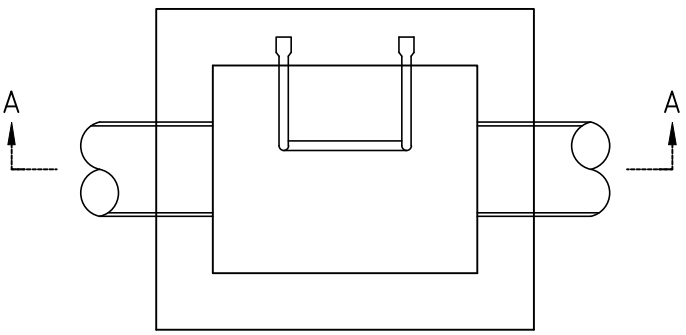
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



SECTION A – A



STEP IRON DETAILS



PLAN

NOTES:

1. PITS DEEPER THAN 1000mm AND OF MINIMUM SIZE OF 600x600mm SHALL BE FITTED WITH STEP IRONS
2. STEP IRONS SHALL BE LOCATED
   - DIRECTLY BELOW THE OPENING IN THE COVER
   - DESIRABLY ON A WALL WITHOUT PIPE OPENINGS
   - DESIRABLY ON ONE OF THE LONG SIDES OF THE PIT
   - APPROVED DRAINAGE PLANS TO NOMINATE PIT WALLS WHERE STEP IRONS ARE TO BE LOCATED.
3. MATERIAL FOR STEP IRONS SHALL BE STRUCTURAL GRADE 250 TO AS 1204.
4. STEP IRONS SHALL HAVE SHARP EDGES ROUNDED.
5. STEP IRONS TO BE HOT DIPPED GALVANISED AFTER FABRICATION.

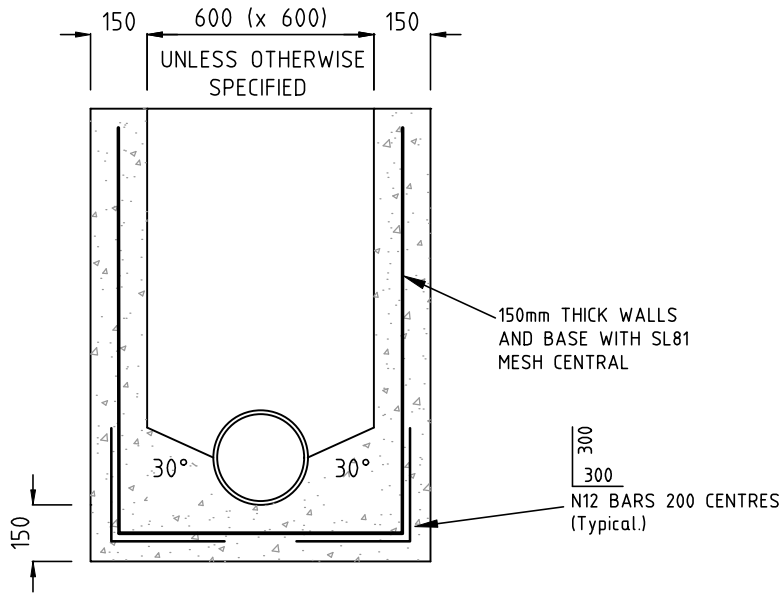
KINGSTON CITY COUNCIL
STANDARD DRAWING

STEP IRON DETAILS

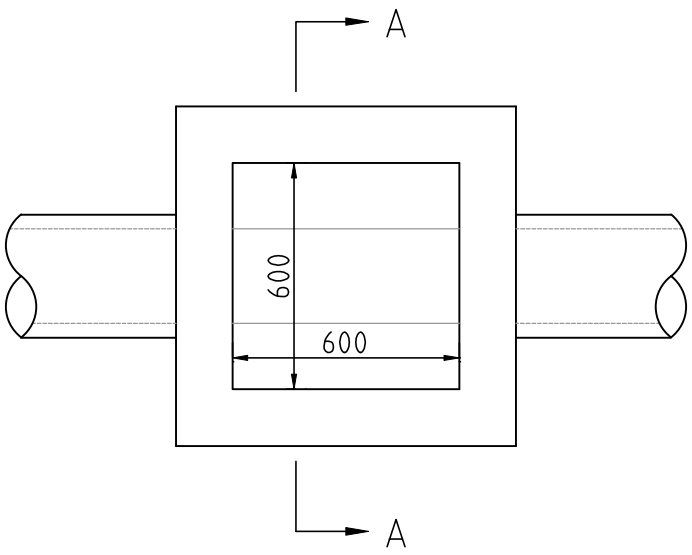
DRG. NO. S401

ISSUE DATE: 19/12/25

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



SECTION A–A



PLAN

NOTES:

1. CONCRETE STRENGTH TO BE 32MPa AT 28 DAYS.
2. FLOORS AND WALLS OF PIT TO BE A MINIMUM OF 150mm THICK COMPACTED CONCRETE.
3. PIT DIMENSIONS ARE INDICATIVE ONLY, REFER TO PIT SCHEDULE FOR PIT SIZES.
4. FOR PIT LID DETAILS REFER TO THE PIT SCHEDULE, RELEVANT STANDARD DRAWINGS AND THE CITY OF KINGSTON ROADS DRAINS DESIGN STANDARDS.
5. PIT WALLS ARE TO BE WATER TIGHT.

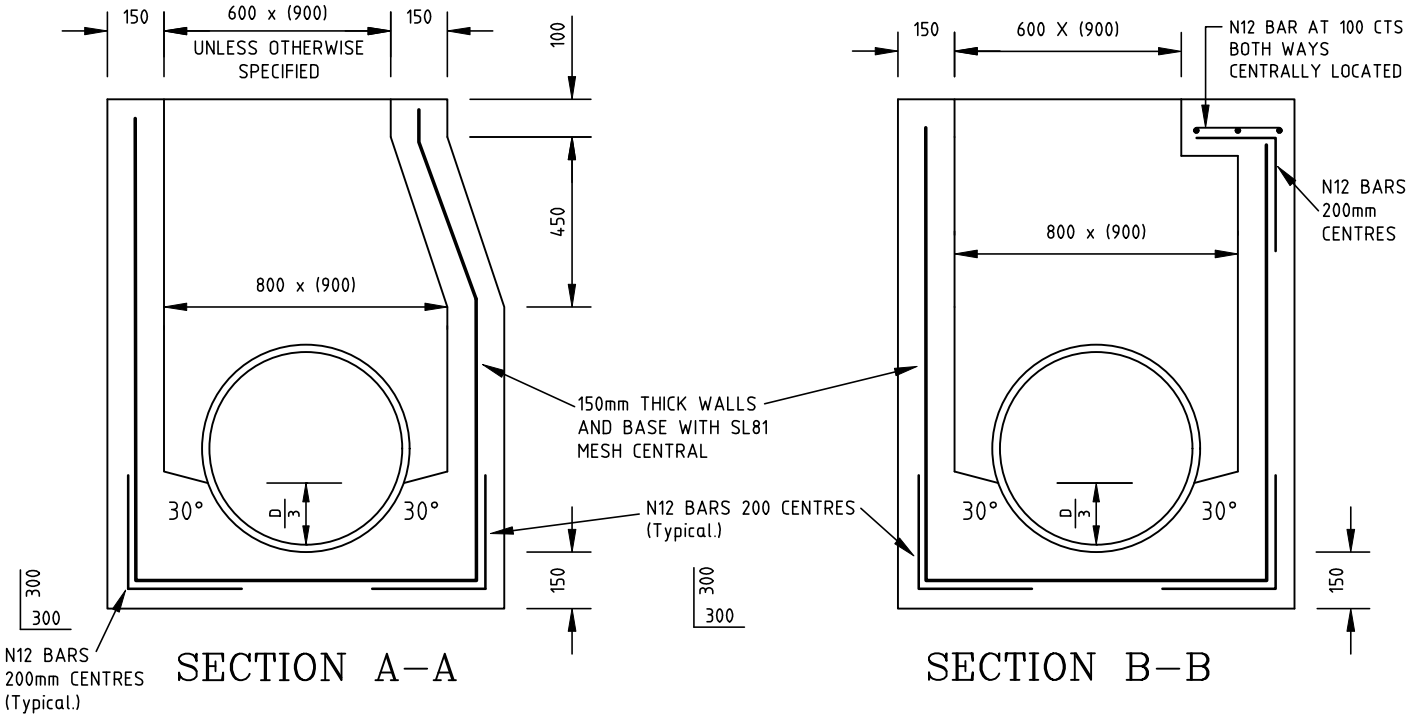
KINGSTON CITY COUNCIL
STANDARD DRAWING

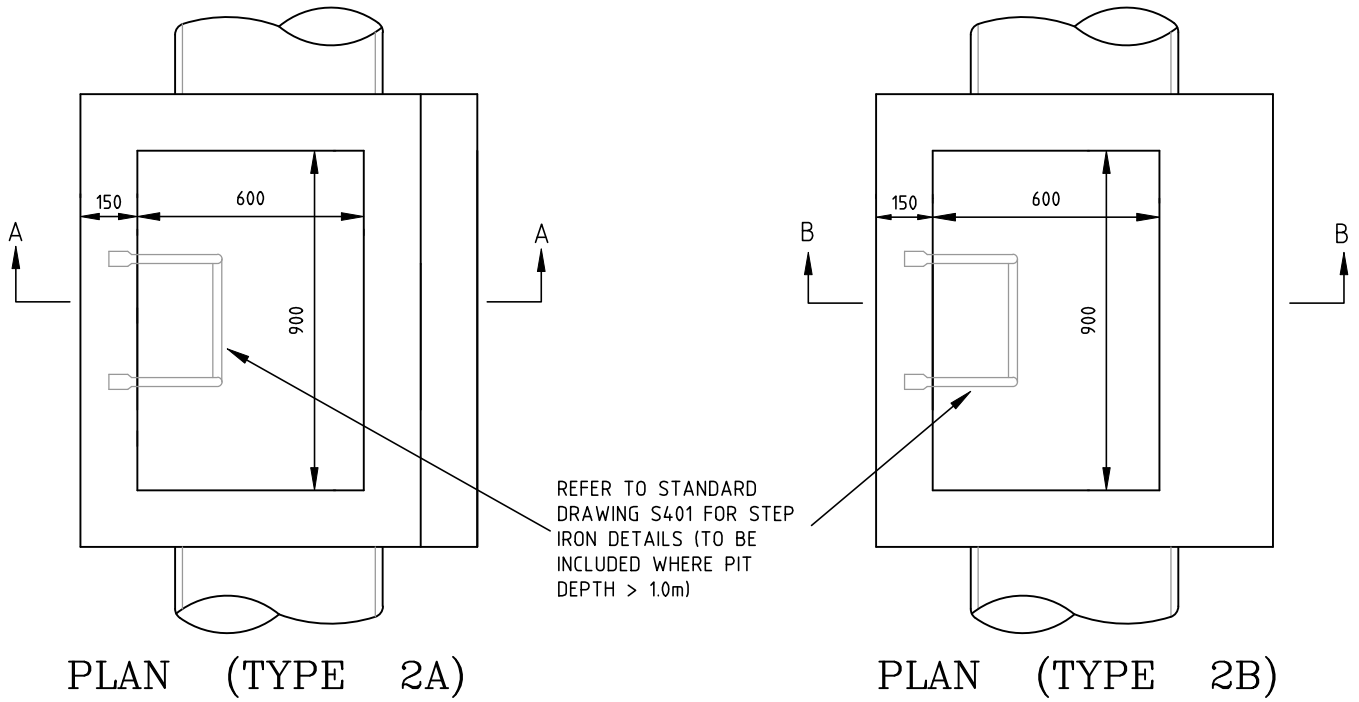
DRG. NO. S402

ISSUE DATE: 19/12/25

TYPE 1 JUNCTION PIT
LESS THAN 1.2m DEPTH FOR PIPE SIZES UP TO 450ϕ

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED





NOTES:

1. CONCRETE STRENGTH TO BE 32MPa AT 28 DAYS.
2. FLOORS AND WALLS OF PIT TO BE A MINIMUM OF 150mm THICK COMPACTED CONCRETE, REINFORCING TO HAVE 30mm COVER
3. PIT DIMENSIONS ARE INDICATIVE ONLY, REFER TO PIT SCHEDULE FOR PIT SIZES.
4. FOR PIT LID DETAILS REFER TO THE PIT SCHEDULE, RELEVANT STANDARD DRAWINGS AND THE CITY OF KINGSTON ROADS DRAINS DESIGN STANDARDS.
5. PIT WALLS ARE TO BE WATER TIGHT.

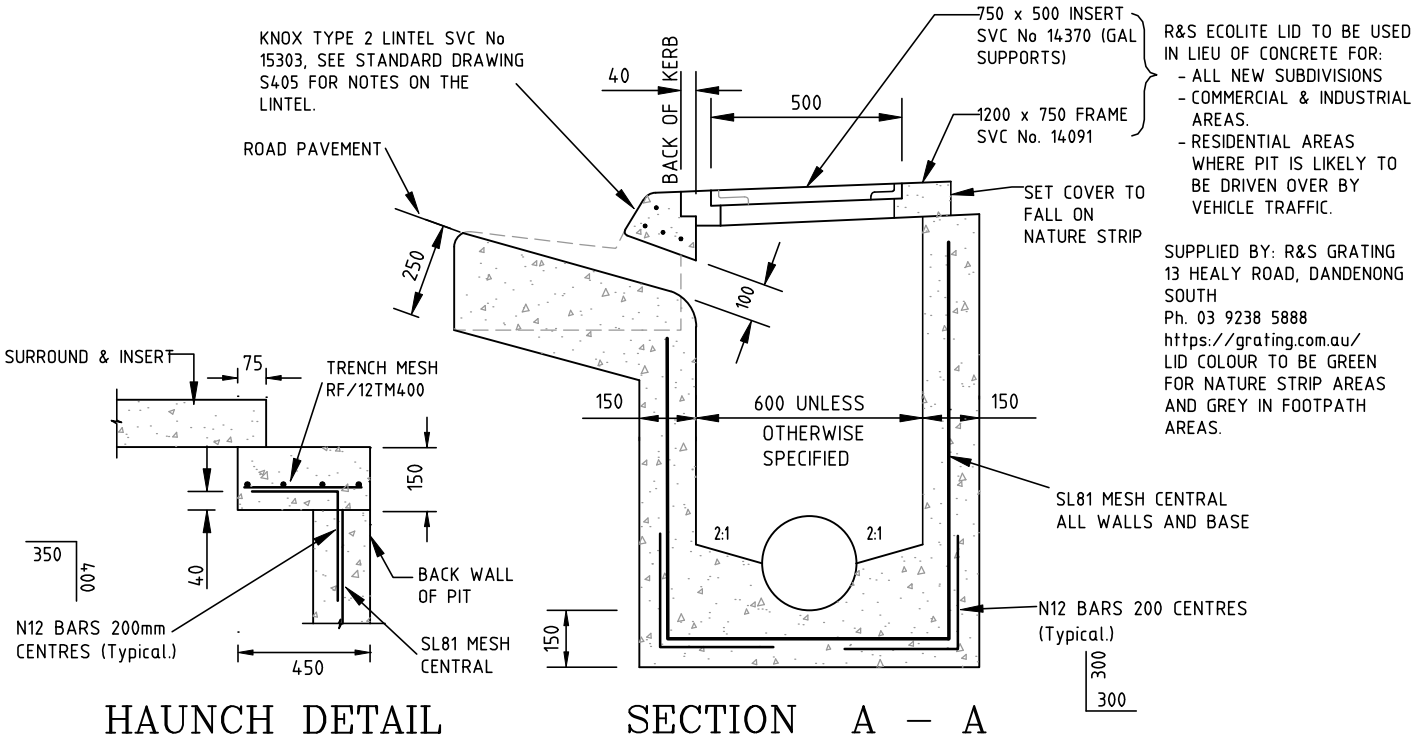
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S403

ISSUE DATE: 19/12/25

TYPE 2 JUNCTION PIT
GREATER THAN 1.2m DEPTH AND LESS THAN 2.4m DEPTH
FOR PIPES UP TO 675ϕ

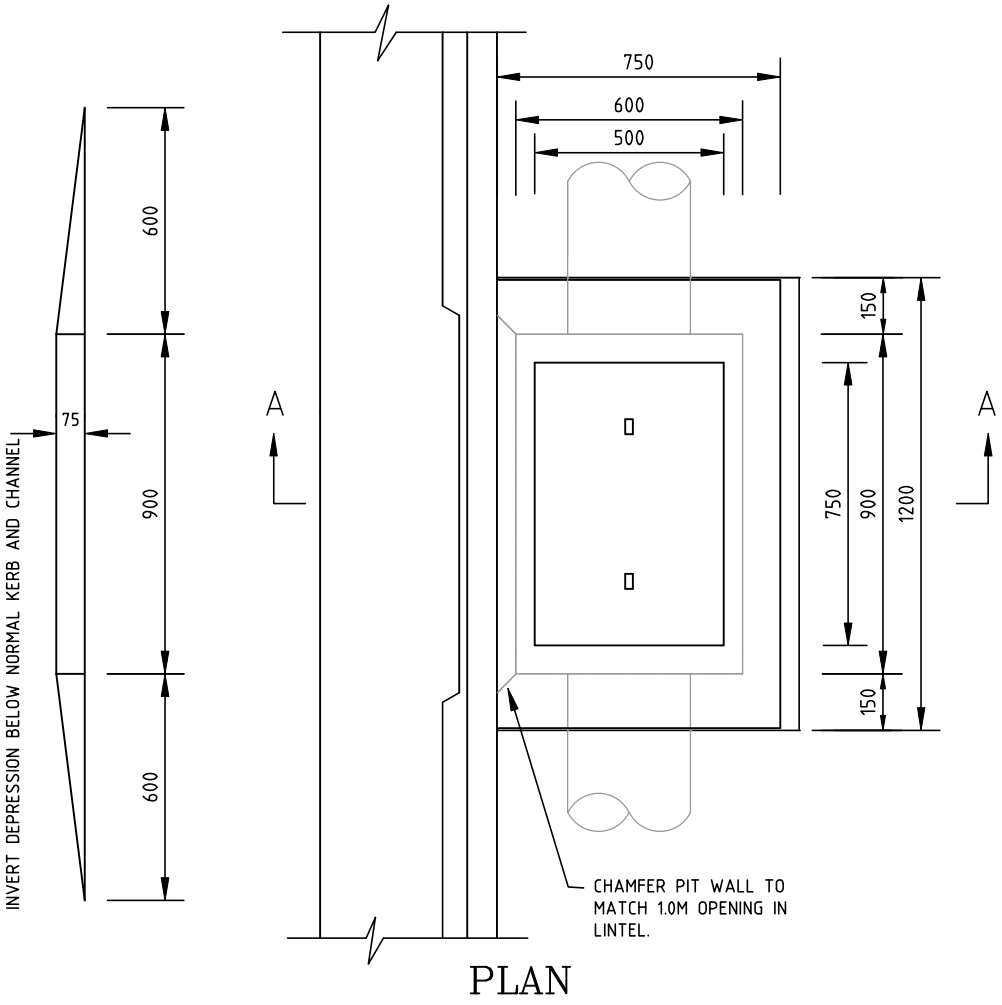
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



HAUNCH DETAIL

(FOR PIPES 525 - 750)

SECTION A – A



PLAN

NOTES:

1. CONCRETE STRENGTH TO BE 32 MPa.
2. STEP IRONS REQUIRED WHERE PIT DEPTH EXCEEDS 1.0m. REFER TO STEP IRONS STANDARD DRAWING S401 FOR DETAILS.
3. SURROUND AND INSERT SUPPLIED BY:
   S.V.C. PRODUCTS
   38 JAPPADDY ST MORDIALLOC
   Ph. 1300 287 782
   https://svc.com.au/
   R&S GRATING
   13 HEALY ROAD, DANDENONG SOUTH
   Ph. 03 9238 5888
   https://grating.com.au/
4. ALL PROPRIETARY COMPONENTS TO BE SUPPLIED BY MANUFACTURERS LISTED OR OTHERS IF APPROVED BY COUNCIL.
5. CONCRETE LINTEL, PIT LID AND SURROUND ARE TO BE BLACK COLOURED.
6. ALL NEW CONCRETE KERB AND CHANNEL TO BE BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.

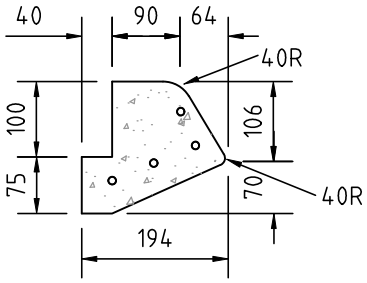
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S404

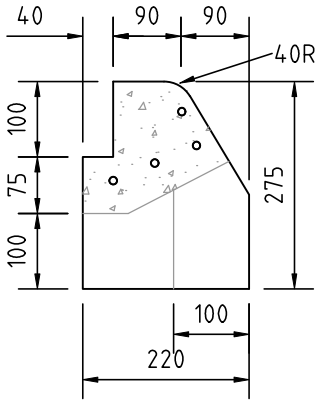
ISSUE DATE: 19/12/25

SINGLE SIDE ENTRY PIT DETAIL

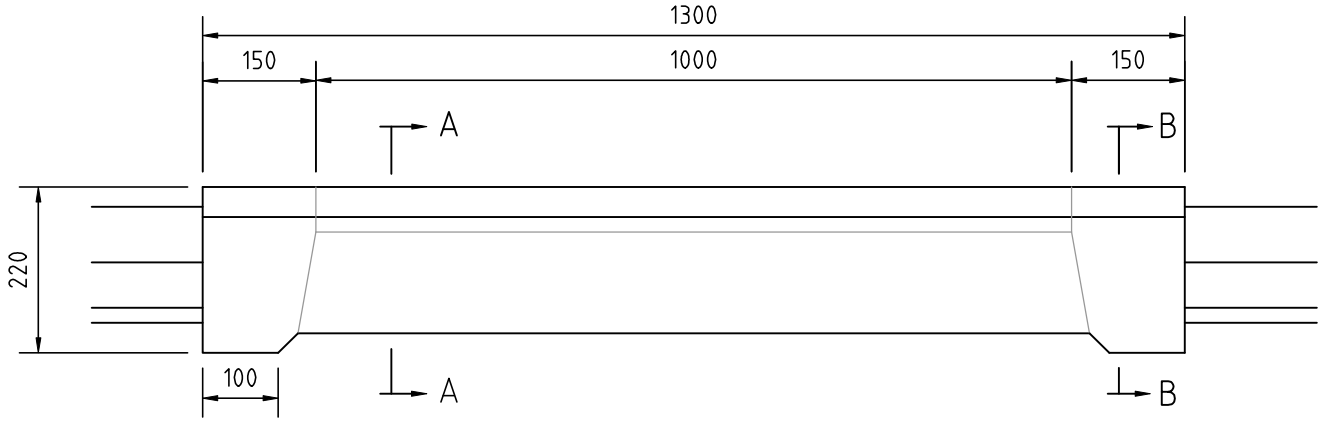
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



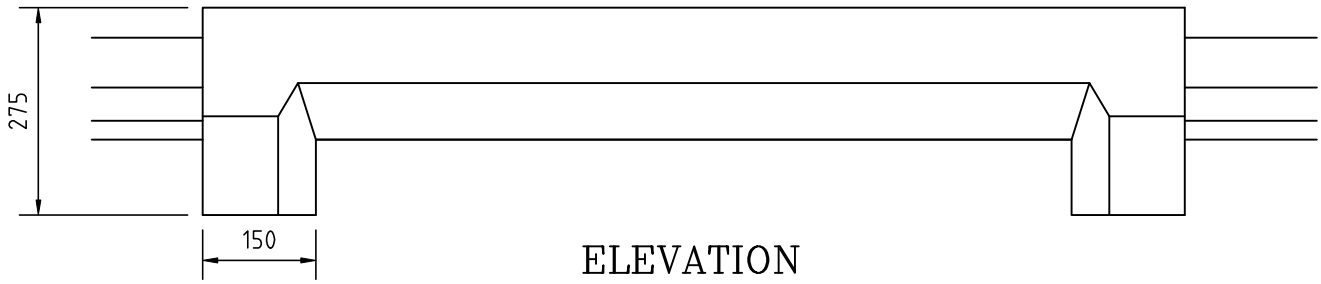
SECTION A – A



SECTION B – B



PLAN



ELEVATION

NOTES

1. THIS IS A CUSTOM MADE LINTEL AND THE CONTRACTOR IS TO CONFIRM ITS AVAILABILITY IMMEDIATELY ON RECIEVING THE COUNCIL ORDER FOR THE WORKS.
2. THE LINTEL IS TO BE A BLACK COLOUR. THE COLOUR IS TO BE ABILOX PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2%** IN GREY CEMENT
3. CORRECT SELECTION OF CONCRETE/MORTAR MIX BEDDING AREAS SUPPORTING LINTEL MUST BE ADHERED TO. COMPACTION OF THIS MATERIAL IS NECESSARY TO AVOID CRACKING OF LINTEL AND FAILURE OF ADJOINING KERB.

S.V.C. PRODUCTS PTY.LTD

CODE: 15.303

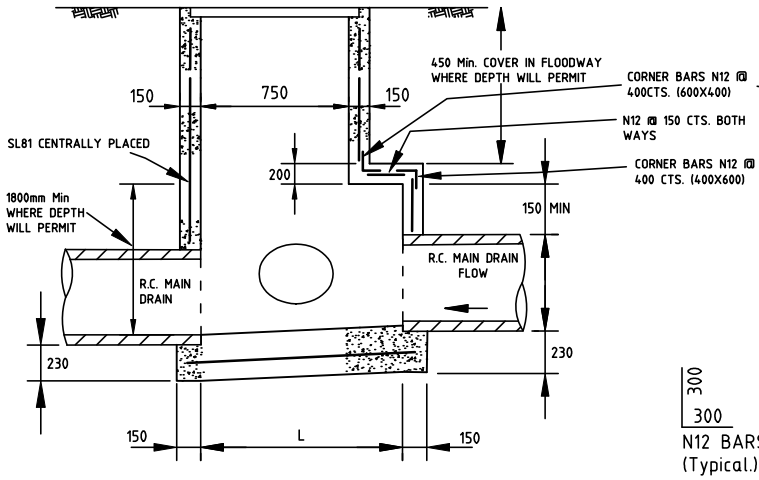
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S405

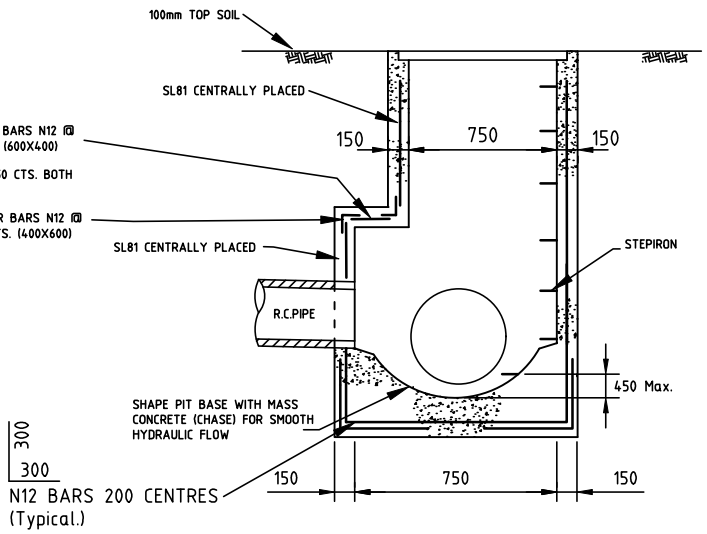
ISSUE DATE: 19/12/25

PRE-CAST LINTEL 'KNOX' TYPE 2 BLACK COLOURED

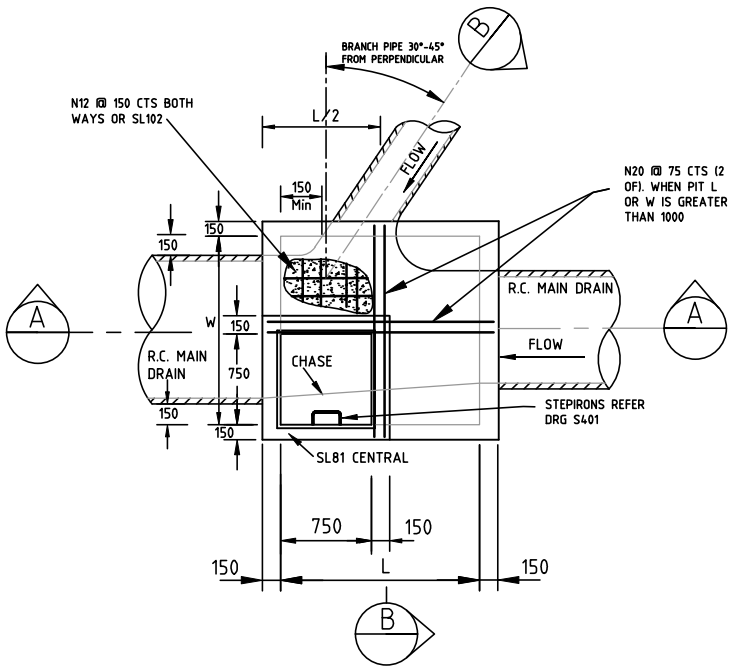
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



SECTION A



SECTION B



SECTIONAL PLAN

MAIN LINE DIA.	STANDARD DIMENSIONS		MAX. BRANCH DIA ϕ	VARIATIONS TO PIT LENGTH - 'L' DEPENDING ON BRANCH ANGLE					
				BRANCH ANGLE					
	L	W		0°	0°-10°	10°-20°	20°-30°	30°-40°	40°-45°
675	750	1050	225	750	750	750	750	750	750
675	750	1050	300	750	750	750	750	750	900
750	750	1050	300	750	750	750	750	750	900
750	750	1050	375	900	900	900	900	900	900
825	750	1050	375	900	900	900	900	900	900
900	750	1200	450	900	900	900	1050	1050	1050
975	750	1350	450	900	900	900	1050	1050	1050
1050	750	1350	525	900	1050	1050	1050	1200	1200
1125	750	1350	525	900	1050	1050	1050	1200	1200
1200	750	1500	600	1050	1050	1050	1200	1200	1350
1275	750	1650	600	1050	1050	1050	1200	1200	1350
1350	750	1650	675	1050	1050	1050	1200	1350	1500
1425	750	1800	675	1050	1050	1050	1200	1350	1500
1500	750	1800	750	1200	1200	1200	1350	1500	1500
1575	750	1950	750	1200	1200	1200	1350	1500	1500
1650	750	1950	825	1200	1350	1350	1350	1500	1650
1725	750	2100	825	1200	1350	1350	1350	1500	1650
1800	750	2100	900	1350	1350	1350	1500	1650	1800
1875	750	2250	900	1350	1350	1350	1500	1650	1800
1950	750	2250	975	1500	1500	1500	1650	1800	1950
2100	750	2400	1050	1500	1500	1650	1650	1950	2100
2400	750	2700	1200	1650	1650	1800	1950	2100	2250

NOTES:

1. IF THE TOP OF THE PIT NEEDS TO BE CORBELLED TO SUIT THE COVER, OR IF THE PIT IS SUFFICIENTLY DEEP TO WARRANT CORBELLING, THEN THE CORBEL MUST BE APPROPRIATELY REINFORCED AND BE A MINMUM OF 750X750mm.
2. PREFERENCE SHALL BE GIVEN TO ANGLING BRANCH DRAINS DOWNSTREAM AT 30° TO 45° FROM PERPENDICULAR.
3. BRANCH PIPES SHALL NOT BE CONNECTED TO CORBELLED SECTIONS AND SHALL BE CLEAR OF STEPIRONS. BRANCH PIPES SHALL NOT BE CONNECTED TO ANY PIT CORNERS- 150mm CLEARANCE IS GENERALLY REQUIRED BETWEEN A PIT CORNER & OUTSIDE FACE OF PIPE.
4. ALL CONCRETE SHALL BE 32MPa min.
5. FOR PIT LID DETAILS REFER TO THE PIT SCHEDULE, RELEVANT STANDARD DRAWINGS AND THE CITY OF KINGSTON ROADS DRAINS DESIGN STANDARDS.
6. GALVANISED STEP IRONS ARE TO BE PROVIDED AS PER STANDARD DRAWING S401 WHEN THE PIT DEPTH EXCEEDS 1.0m
7. ALL PROPRIETY COMPONENTS TO BE SUPPLIED BY MANUFACTURES LISTED OR OTHERS IF APPROVED BY COUNCIL.
8. DIMENSIONS ARE IN mm's.

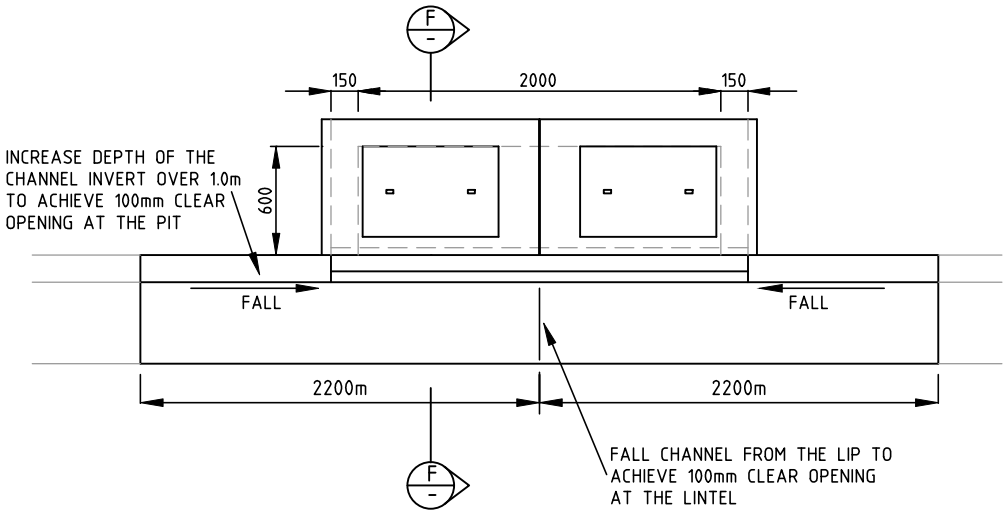
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S406

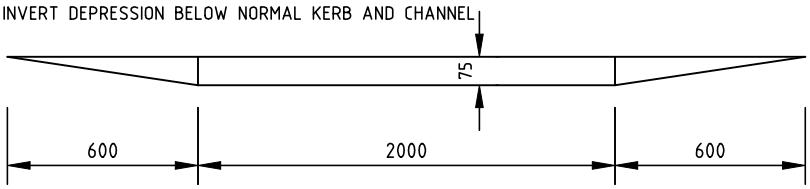
ISSUE DATE: 19/12/25

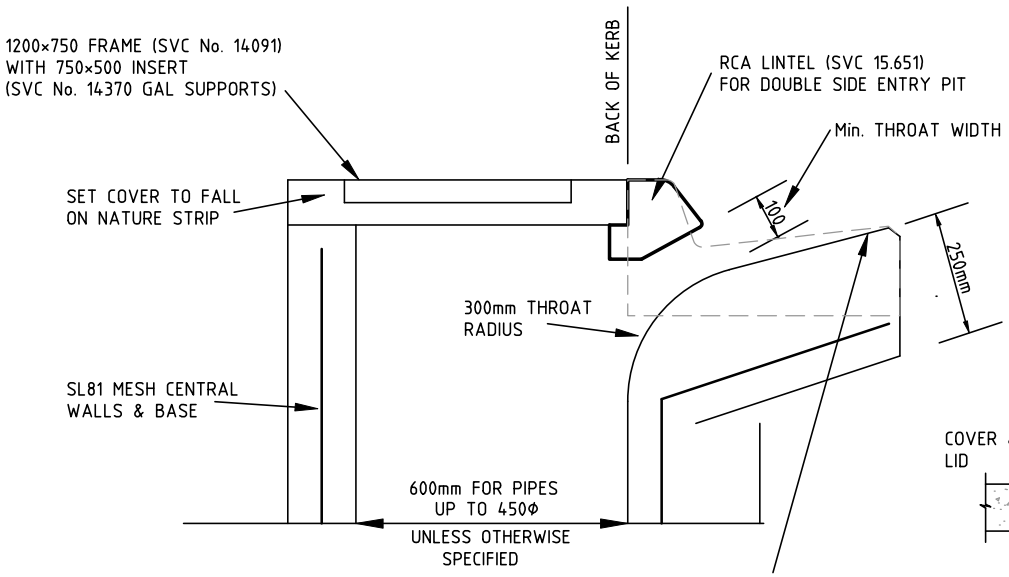
TYPE 3 JUNCTION PIT
DIMENSIONS AND CONSTRUCTION NOTES

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

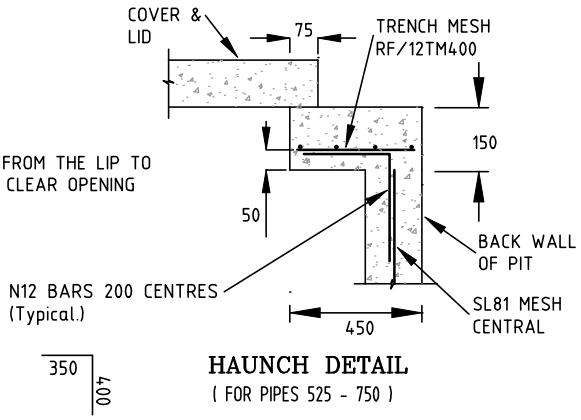


PLAN DOUBLE SIDE ENTRY PIT





SECTION F-F



HAUNCH DETAIL
(FOR PIPES 525 - 750)

NOTES:

1. CONCRETE STRENGTH TO BE 32 MPa.
2. STEP IRONS REQUIRED WHERE PITS EXCEED 1.0m. REFER TO STEP IRONS STANDARD DRAWING S401 FOR DETAILS.
3. SURROUND AND INSERT SUPPLIED BY S.V.C. PRODUCTS
   38 JAPPADDY ST BRAESIDE, 3195
   PH. 9580 6644
4. ALL PROPRIETARY COMPONENTS TO BE SUPPLIED BY MANUFACTURERS LISTED OR OTHERS IF APPROVED BY COUNCIL.
5. R&S ECOLITE LID TO BE USED IN LIEU OF CONCRETE LID FOR:
   - ALL NEW SUBDIVISIONS
   - COMMERCIAL AND INDUSTRIAL AREAS.
   - RESIDENTIAL AREAS WHERE PIT IS LIKELY TO BE DRIVEN OVER BY VEHICLE TRAFFIC

   SUPPLIED BY: R&S GRATING
   13 HEALY ROAD, DANDENONG SOUTH
   Ph. 03 9238 5888
   https://grating.com.au/
   LID COLOUR TO BE GREEN FOR NATURE STRIP AREAS AND GREY IN FOOTPATH AREAS.
6. CONCRETE LINTEL, PIT LIDS AND SURROUNDS ARE TO BE BLACK COLOURED.
7. ALL NEW CONCRETE KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.

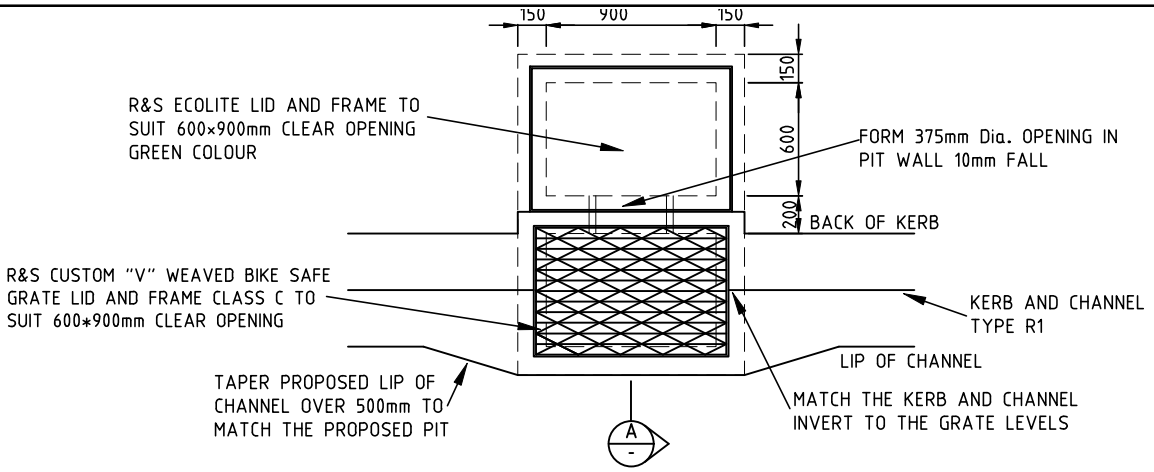
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S409

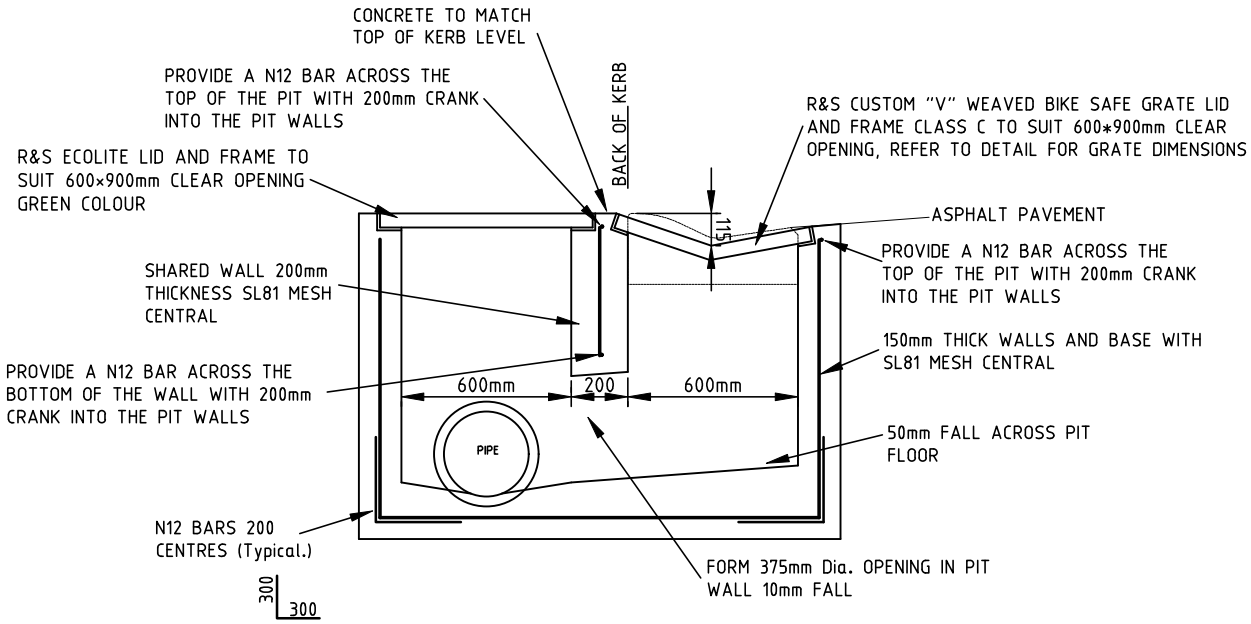
ISSUE DATE: 19/12/25

DOUBLE SIDE ENTRY PIT

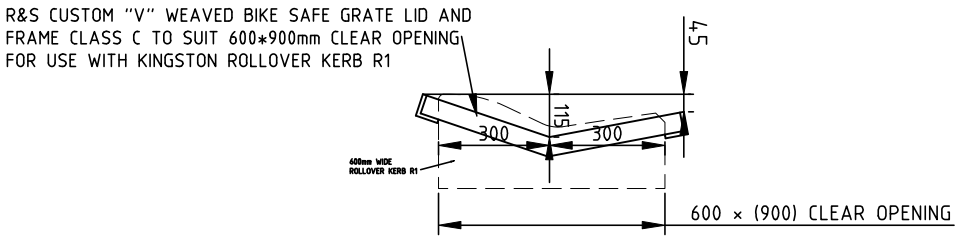
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PLAN



SECTION A-A



GRATE DIMENSIONS

NOTES

1. ALL CONCRETE TO BE 32MPa.
2. DIMENSIONS ARE IN mm's.
3. ALL NEW CONCRETE KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF 2.0% BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
4. GRATE TO BE A R&S CUSTOM "V" WEAVED BIKE SAFE GRATE LID AND FRAME CLASS C TO SUIT 600*900mm CLEAR OPENING FOR USE WITH KINGSTON ROLLOVER KERB R1
5. THE BACK WALL IS TO BE CORBELED FOR PIPES LARGER THAN 525mm Dia. RUNNING PARALLEL WITH THE KERB AND CHANNEL.
6. GALVANISED STEP IRONS ARE TO BE PROVIDED AS PER STANDARD DRAWING S401 WHEN THE PIT DEPTH EXCEEDS 1.0m
7. ALL PROPRIETY COMPONENTS TO BE SUPPLIED BY MANUFACTURES LISTED OR OTHERS IF APPROVED BY COUNCIL.
8. ALL GRATES TO HAVE A NON SLIP FINISH APPLIED TO THE TOP OF THE GRATE.

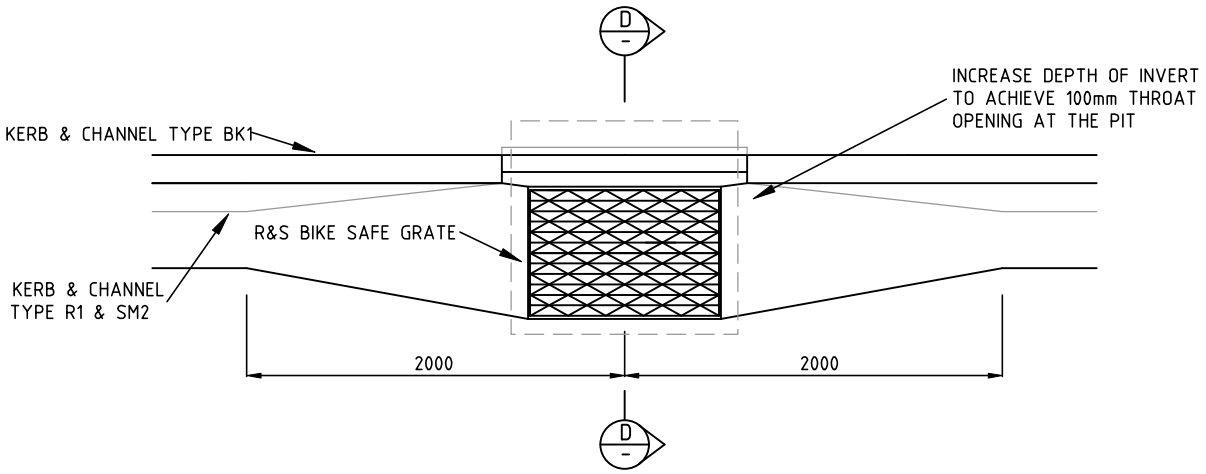
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S410

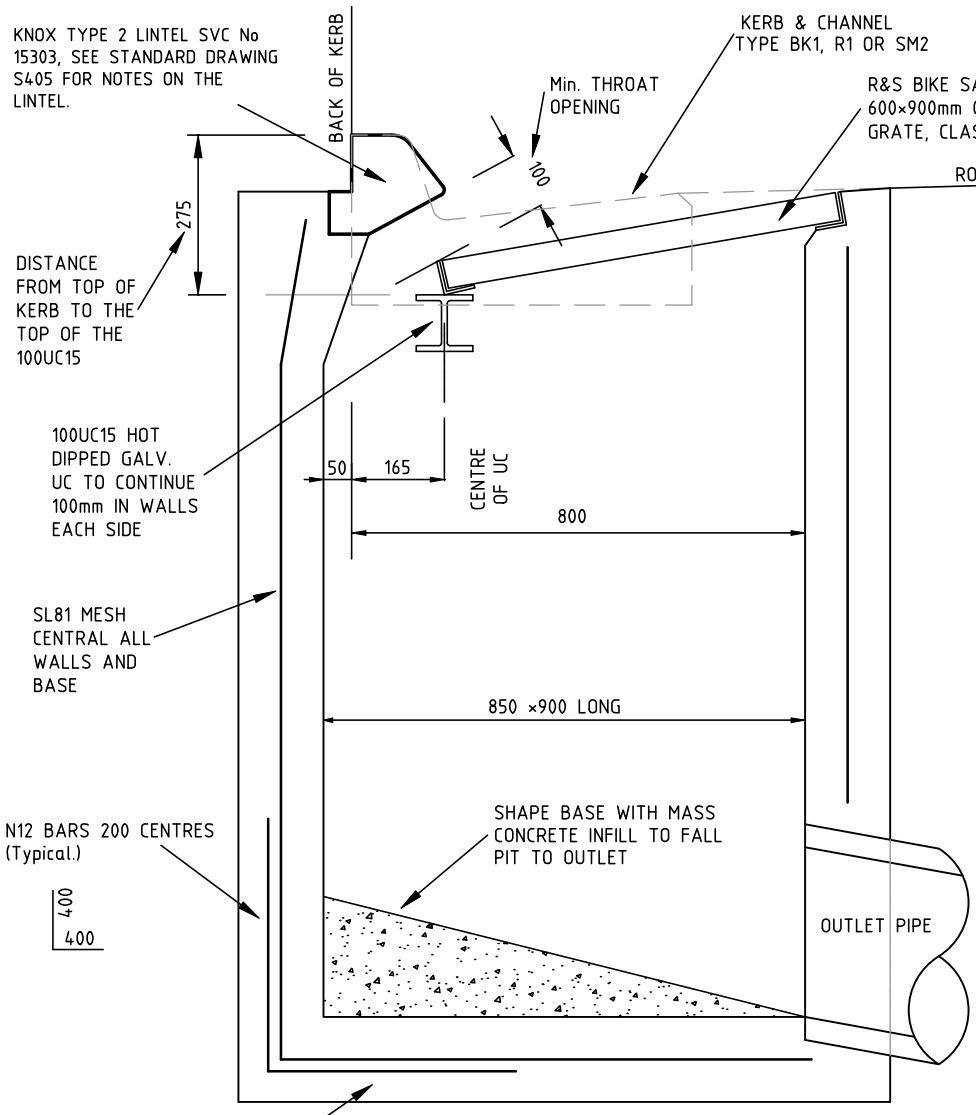
ISSUE DATE: 19/12/25

CHANNEL GRATE PIT
FOR ROLL OVER KERB AND CHANNEL TYPE R1

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PLAN UNDER CHANNEL GRATE PIT



SECTION D-D

NOTES

1. ALL CONCRETE TO BE 32MPa.
2. DIMENSIONS ARE IN mm's.
3. GRATE TO BE BIKE SAFE GRATE CLASS D, DIMENSIONS OF THE GRATE ARE FOR THE CLEAR OPENING. APPROVED SUPPLIERS AS OF FEBRUARY 2025
   R&S GRATING, BIKE SAFE GRATES
   13 HEALY ROAD DANDENONG SOUTH.
   Ph. 03 9238 5888
   https://grating.com.au/
4. THE FRONT WALL IS TO BE CORBELED FOR PIPES LARGER THAN 750mm Dia. RUNNING PARALLEL WITH THE KERB AND CHANNEL.
5. GALVANISED STEP IRONS ARE TO BE PROVIDED AS PER STANDARD DRAWING S401 WHEN THE PIT DEPTH EXCEEDS 1.0m
6. ALL PROPRIETY COMPONENTS TO BE SUPPLIED BY MANUFACTURES LISTED OR OTHERS IF APPROVED BY COUNCIL.
7. CONCRETE LINTEL IS TO BE BLACK COLOURED.
8. ALL NEW CONCRETE KERB AND CHANNEL TO BE BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
9. ALL GRATES TO HAVE A NON SLIP FINISH APPLIED TO THE TOP OF THE GRATE.

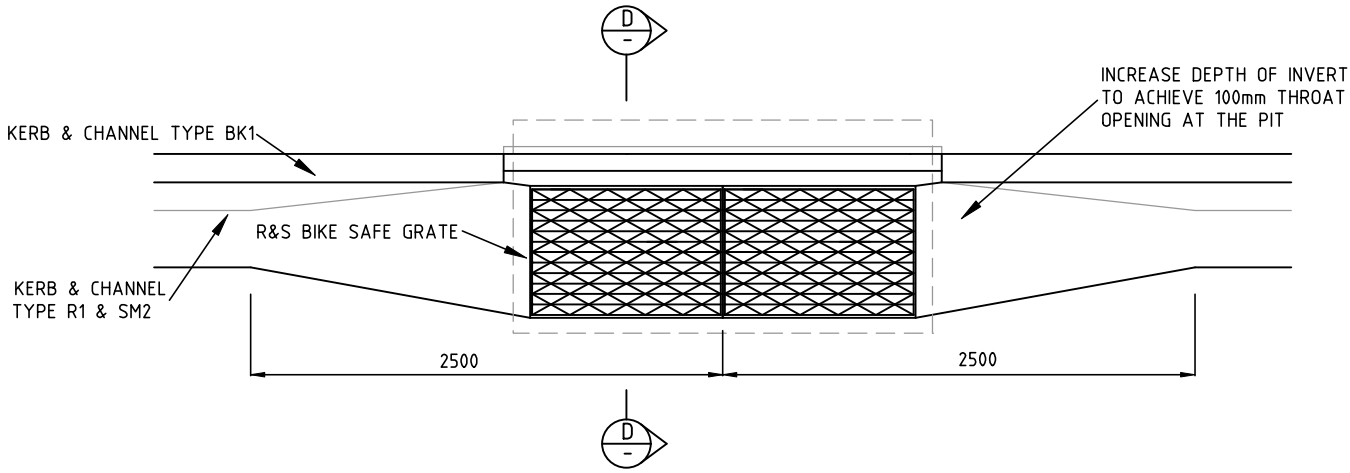
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S411

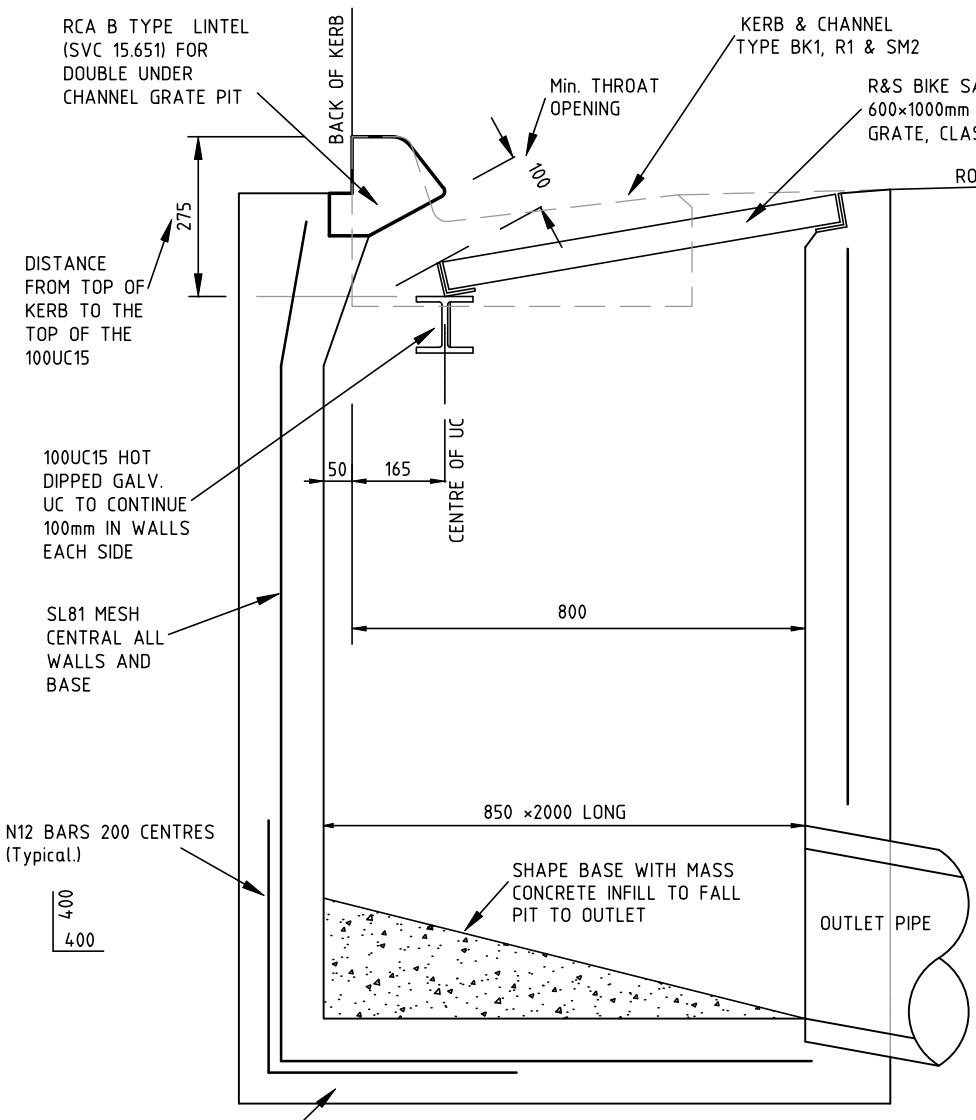
ISSUE DATE: 19/12/25

SINGLE UNDER CHANNEL GRATE PIT
FOR KERB AND CHANNEL TYPE BK1, R1, SM2

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PLAN DOUBLE UNDER CHANNEL GRATE PIT



SECTION D-D

NOTES

1. ALL CONCRETE TO BE 32MPa.
2. DIMENSIONS ARE IN mm's.
3. GRATE TO BE BIKE SAFE GRATE CLASS D, DIMENSIONS OF THE GRATE ARE FOR THE CLEAR OPENING. APPROVED SUPPLIERS AS OF FEBRUARY 2025.
   R&S GRATING, BIKE SAFE GRATES
   13 HEALY ROAD DANDENONG SOUTH.
   Ph. 03 9238 5888
   https://grating.com.au/
4. THE FRONT WALL IS TO BE CORBELED FOR PIPES LARGER THAN 750mm Dia. RUNNING PARALLEL WITH THE KERB AND CHANNEL.
5. GALVANISED STEP IRONS ARE TO BE PROVIDED AS PER STANDARD DRAWING S401 WHEN THE PIT DEPTH EXCEEDS 1.0m
6. ALL PROPRIETY COMPONENTS TO BE SUPPLIED BY MANUFACTURES LISTED OR OTHERS IF APPROVED BY COUNCIL.
7. CONCRETE LINTEL IS TO BE BLACK COLOURED.
8. ALL NEW CONCRETE KERB AND CHANNEL TO BE BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
9. ALL GRATES TO HAVE A NON SLIP FINISH APPLIED TO THE TOP OF THE GRATE.

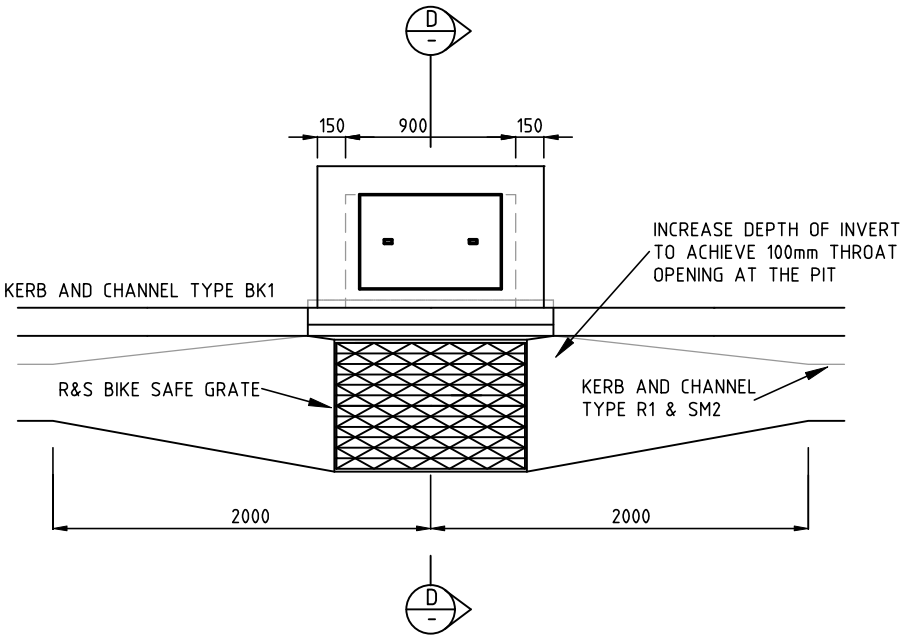
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S412

ISSUE DATE: 19/12/25

DOUBLE UNDER CHANNEL GRATE PIT
FOR KERB AND CHANNEL TYPE BK1, R1, SM2

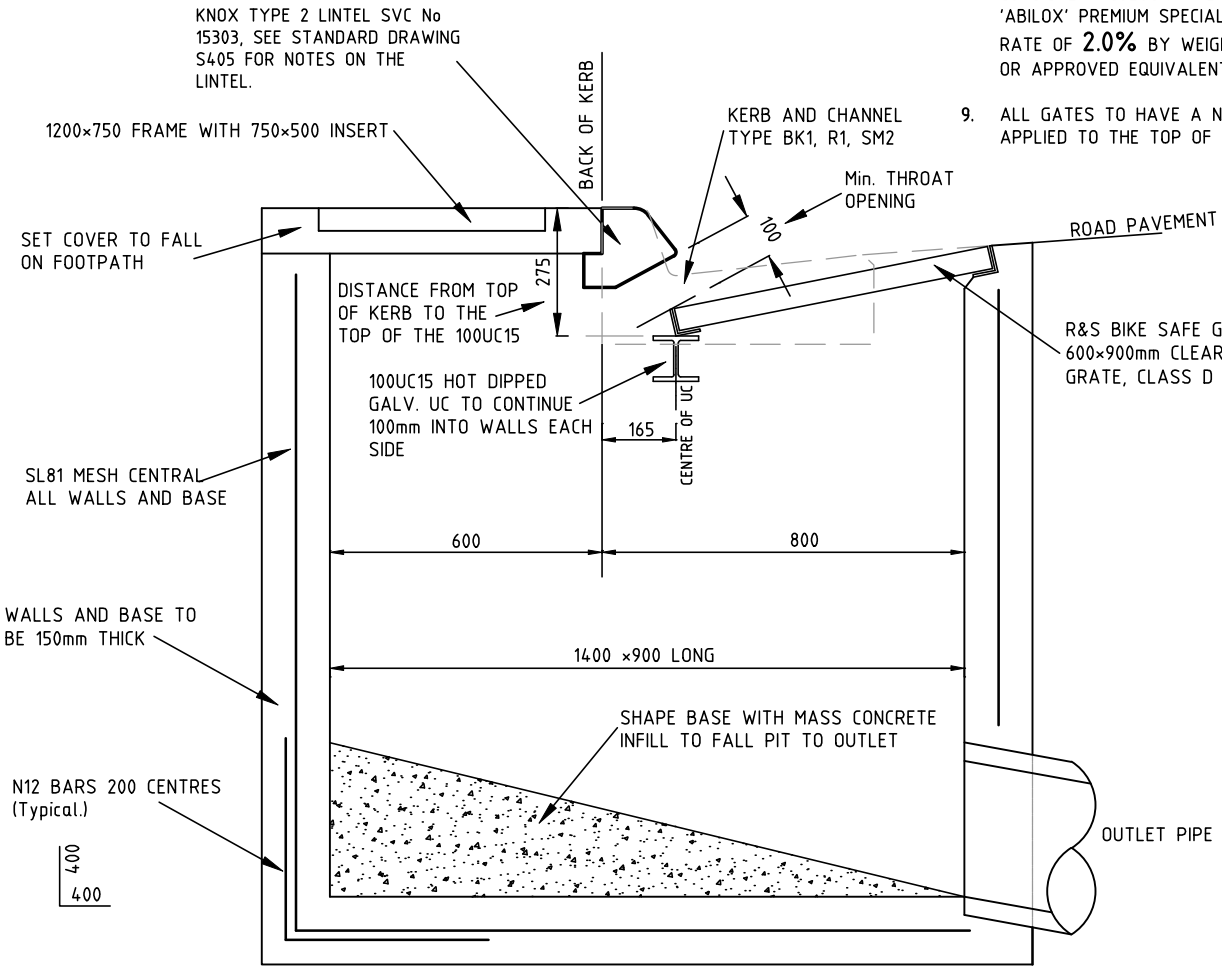
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PLAN SIDE ENTRY/GRATE PIT

NOTES

1. ALL CONCRETE TO BE 32MPa.
2. DIMENSIONS ARE IN mm's.
3. GRATE TO BE BIKE SAFE GRATE CLASS D, DIMENSIONS OF THE GRATE ARE FOR THE CLEAR OPENING.
   APPROVED SUPPLIERS AS OF FEBRUARY 2025
   R&S GRATING, BIKE SAFE GRATES
   13 HEALY ROAD DANDENONG SOUTH.
   Ph. 03 9238 5888
   https://grating.com.au/
4. THE FRONT WALL IS TO BE CORBELED FOR PIPES LARGER THAN 750mm Dia. RUNNING PARALLEL WITH THE KERB AND CHANNEL.
5. GALVANISED STEP IRONS ARE TO BE PROVIDED AS PER STANDARD DRAWING S401 WHEN THE PIT DEPTH EXCEEDS 1.0m
6. ALL PROPRIETY COMPONENTS TO BE SUPPLIED BY MANUFACTURES LISTED OR OTHERS IF APPROVED BY COUNCIL.
7. CONCRETE LINTEL, PIT LID AND SURROUND ARE TO BE BLACK COLOURED.
8. ALL NEW CONCRETE KERB AND CHANNEL TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
9. ALL GATES TO HAVE A NON SLIP FINISH APPLIED TO THE TOP OF THE GRATE.



SECTION D-D

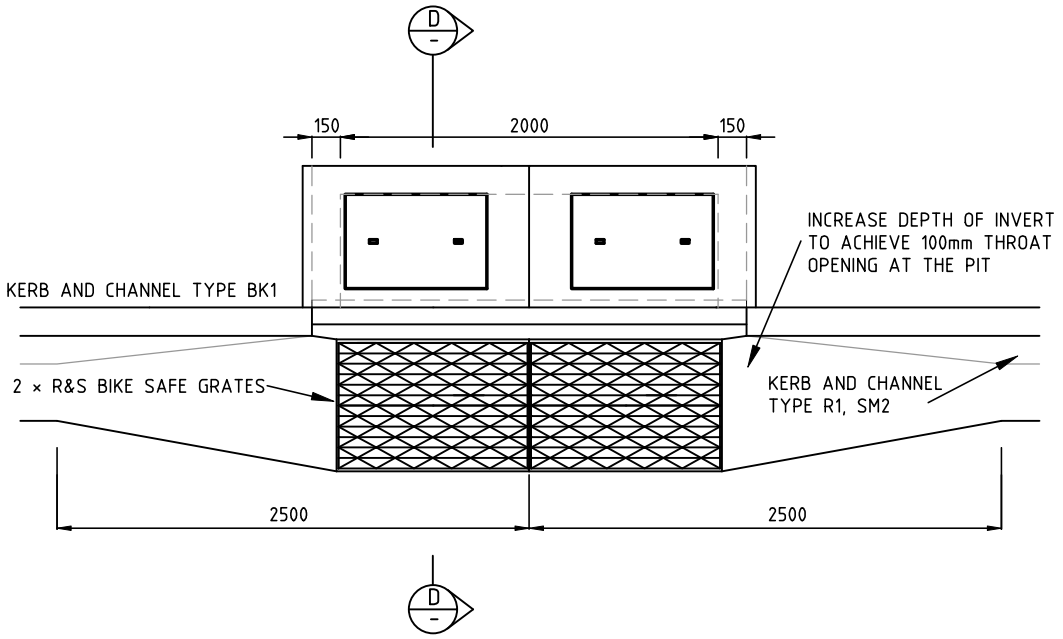
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S413

ISSUE DATE: 19/12/25

SIDE ENTRY GRATE PIT
FOR KERB AND CHANNEL TYPE BK1, R1, SM2

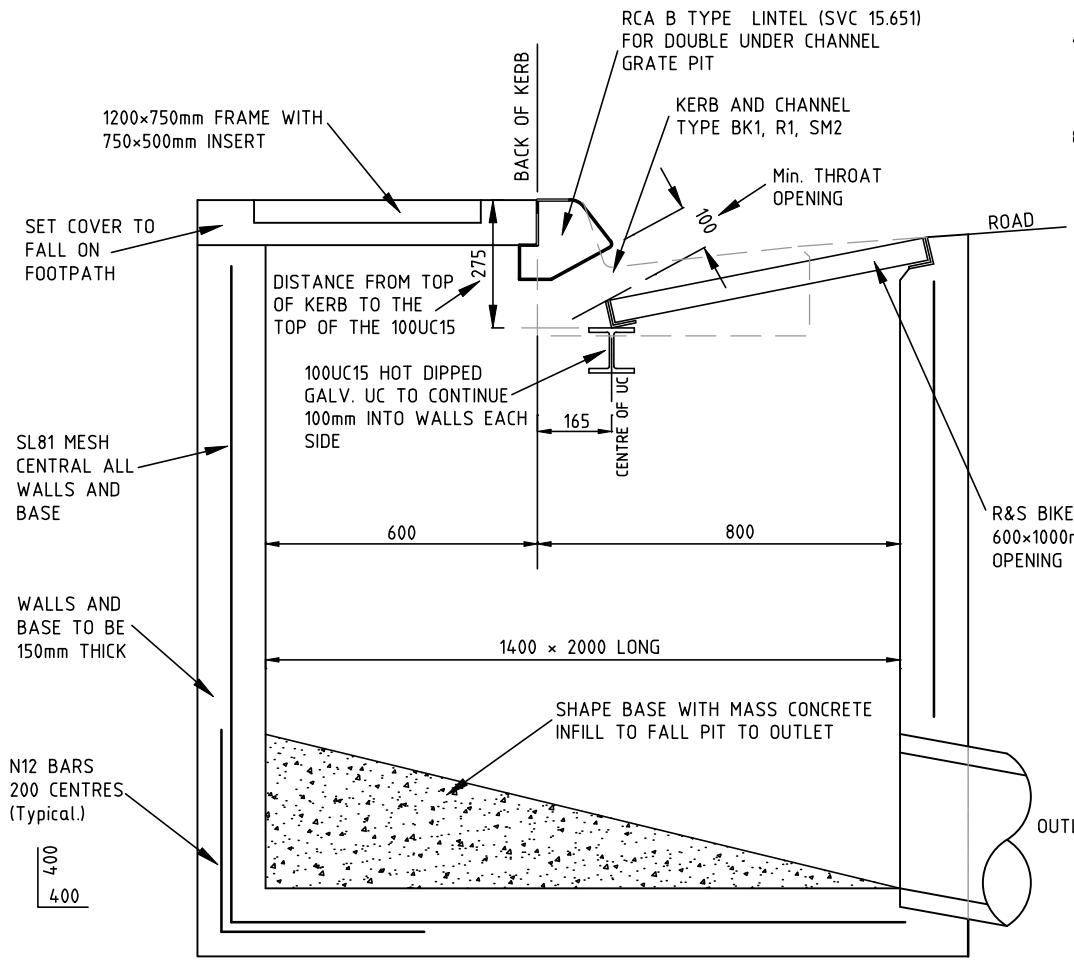
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PLAN DOUBLE SIDE ENTRY/GRATE PIT

NOTES

1. ALL CONCRETE TO BE 32MPa.
2. DIMENSIONS ARE IN mm's.
3. GRATE TO BE BIKE SAFE GRATE CLASS D, DIMENSIONS OF THE GRATE ARE FOR THE CLEAR OPENING.
   APPROVED SUPPLIERS AS OF FEBRUARY 2025
   R&S GRATING, BIKE SAFE GRATES
   13 HEALY ROAD DANDENONG SOUTH.
   Ph. 03 9238 5888
   https://grating.com.au/
4. THE FRONT WALL IS TO BE CORBELED FOR PIPES LARGER THAN 750mm Dia. RUNNING PARALLEL WITH THE KERB AND CHANNEL.
5. GALVANISED STEP IRONS ARE TO BE PROVIDED AS PER STANDARD DRAWING S401 WHEN THE PIT DEPTH EXCEEDS 1.0m
6. ALL PROPRIETY COMPONENTS TO BE SUPPLIED BY MANUFACTURES LISTED OR OTHERS IF APPROVED BY COUNCIL.
7. CONCRETE LINTEL, PIT LID AND SURROUND ARE TO BE BLACK COLOURED.
8. ALL NEW CONCRETE KERB AND CHANNEL TO BE BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
9. ALL GRATES TO HAVE A NON SLIP FINISH APPLIED TO THE TOP OF THE GRATE.



SECTION D-D

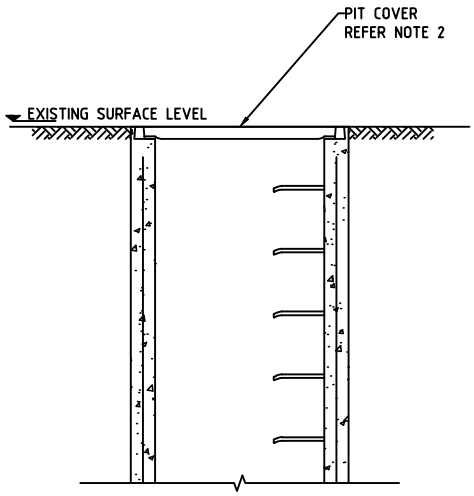
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S414

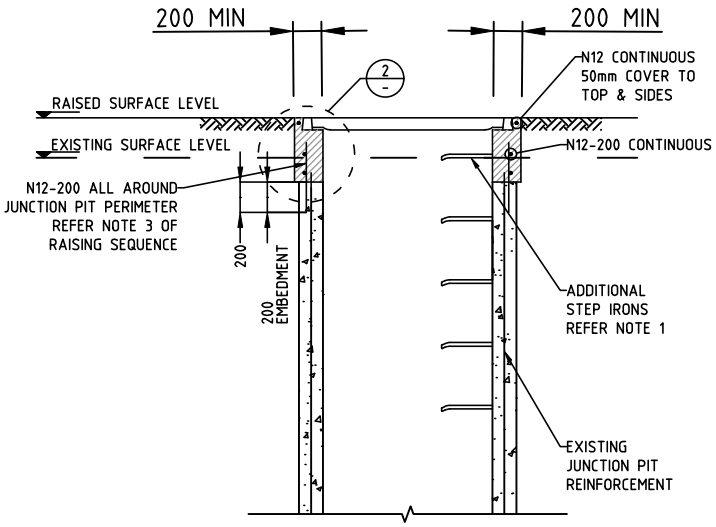
ISSUE DATE: 19/12/25

DOUBLE SIDE ENTRY GRATE PIT
FOR KERB AND CHANNEL TYPE BK1, R1, SM2

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



EXISTING



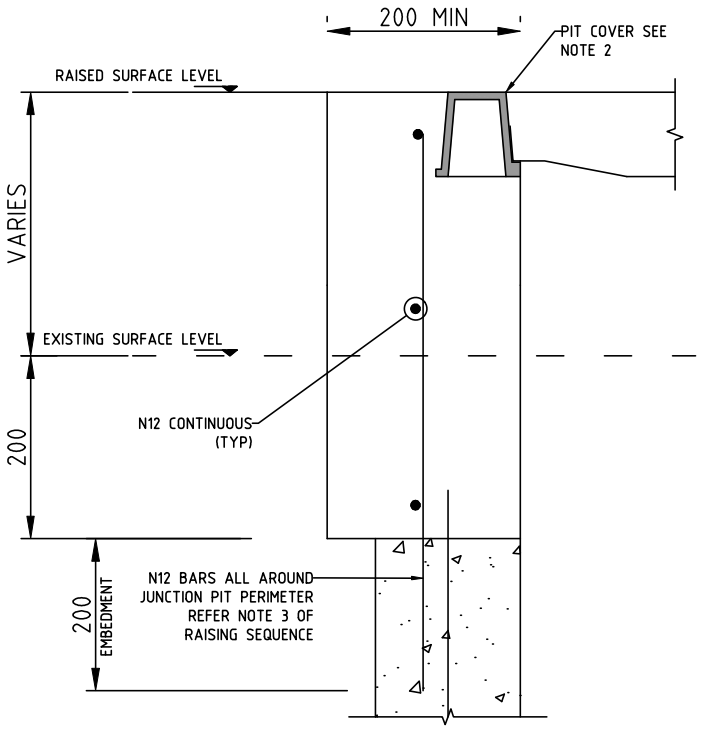
AFTER RAISING

RAISING EXISITING SHAFT SEQUENCE:

1. EXCAVATE SOIL TO 200 BELOW EXISTING LEVEL.
2. CUT WALLS OFF SHAFT AT UNDERSIDE OF COVER FRAME. EXPOSE EXISTING REINFORCING.
3. DRILL N12 AT 200mm EMBEDMENT INTO SHAFT WALL (150mm SPACING).
4. FORM UP WALL 200mm WIDE AT FINAL LEVEL AND CAST IN NEW COVER AND FRAME, REFER NOTE 2.
5. BACKFILL TO FINAL LEVEL.

NOTES

1. THE RESULTANT DISTANCE FROM TOP OF THE JUNCTION PIT TO THE FIRST STEPIRON TO BE EVALUATED & ADDITIONAL STEPIRONS TO BE PROVIDED.
2. PROVIDE NEW PIT COVER AS DETERMINED BY COUNCIL.
3. IF EXISTING PIT IS NOT STRUCTURALLY SOUND OR IF PIT IS CONSTRUCTED OF MATERIALS OTHER THAN CONCRETE, THIS TREATMENT IS NOT APPLICABLE.



DETAIL 2

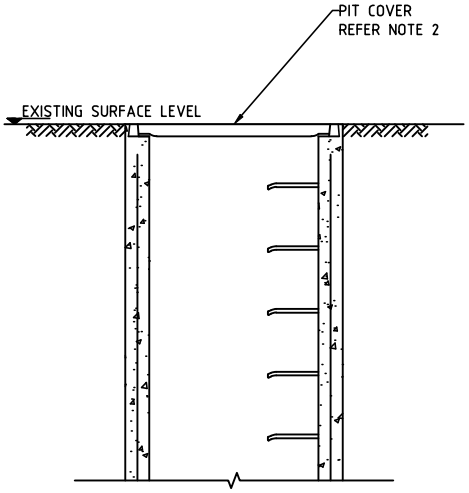
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S415

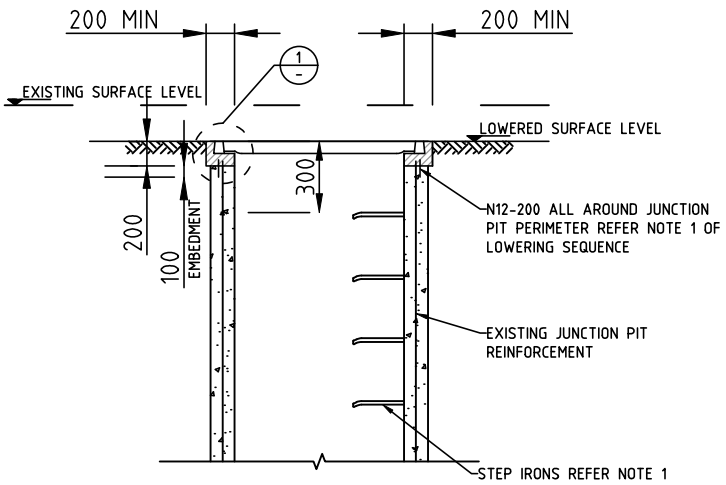
ISSUE DATE: 19/12/25

JUNCTION PIT COVER RISING

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



EXISTING



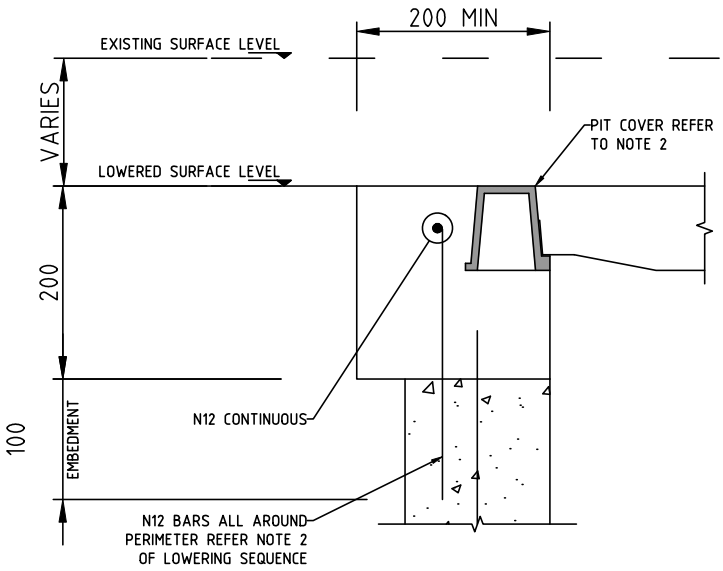
AFTER LOWERING

LOWERING EXISITING SHAFT SEQUENCE:

1. EXCAVATE TO 300 BELOW FINAL LEVEL.
2. REMOVE EXISITING SHAFT TO 200 BELOW FINAL LEVEL. LEAVE REINFORCEMENT PROJECTING AND DRILL AND ANCHOR N12 BARS 100mm EMBEDMENT INTO CONCRETE WALL (150mm SPACING).
3. CAST ON TOP OF WALLS TO FINAL LEVEL NEW COVER FRAME, REFER NOTE 2.
4. SMOOTH TROWEL CONCRETE.
5. BACKFILL TO FINAL LEVEL.

NOTES

1. THE RESULTANT DISTANCE FROM TOP OF THE JUNCTION PIT TO THE FIRST STEPIRON TO BE EVALUATED & ADDITIONAL STEPIRONS TO BE PROVIDED.
2. PROVIDE NEW PIT COVER AS DETERMINED BY COUNCIL.
3. IF EXISTING PIT IS NOT STRUCTURALLY SOUND OR IF PIT IS CONSTRUCTED OF MATERIALS OTHER THAN CONCRETE, THIS TREATMENT IS NOT APPLICABLE.



DETAIL 1

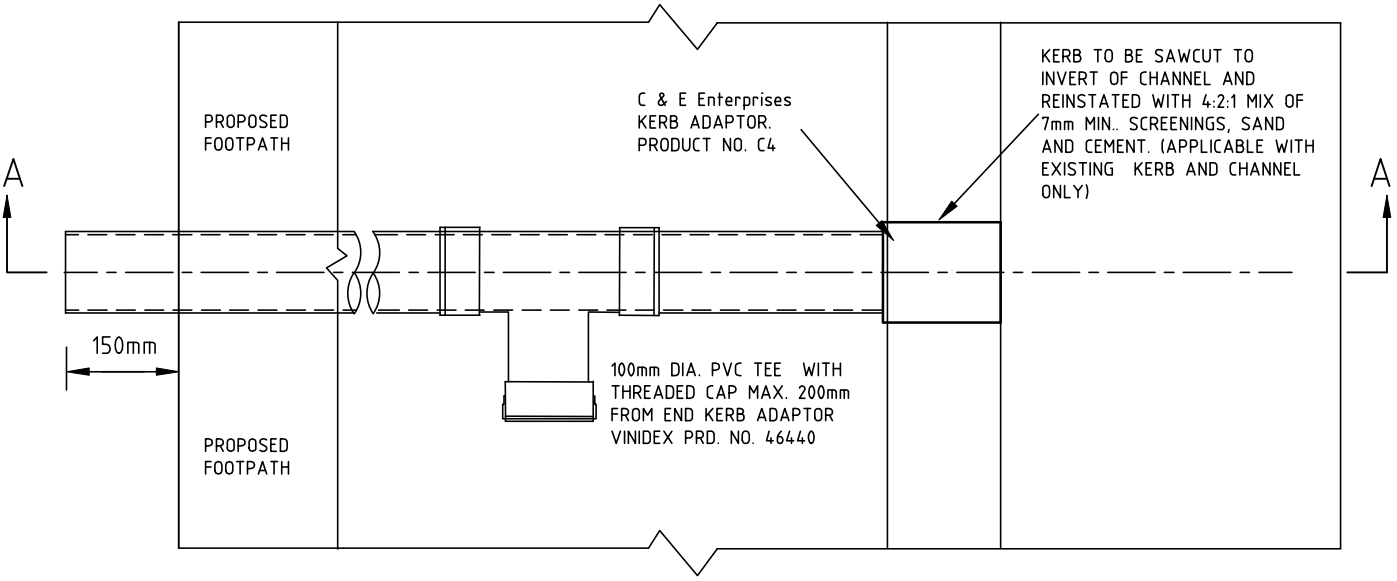
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S416

ISSUE DATE: 19/12/25

JUNCTION PIT COVER LOWERING

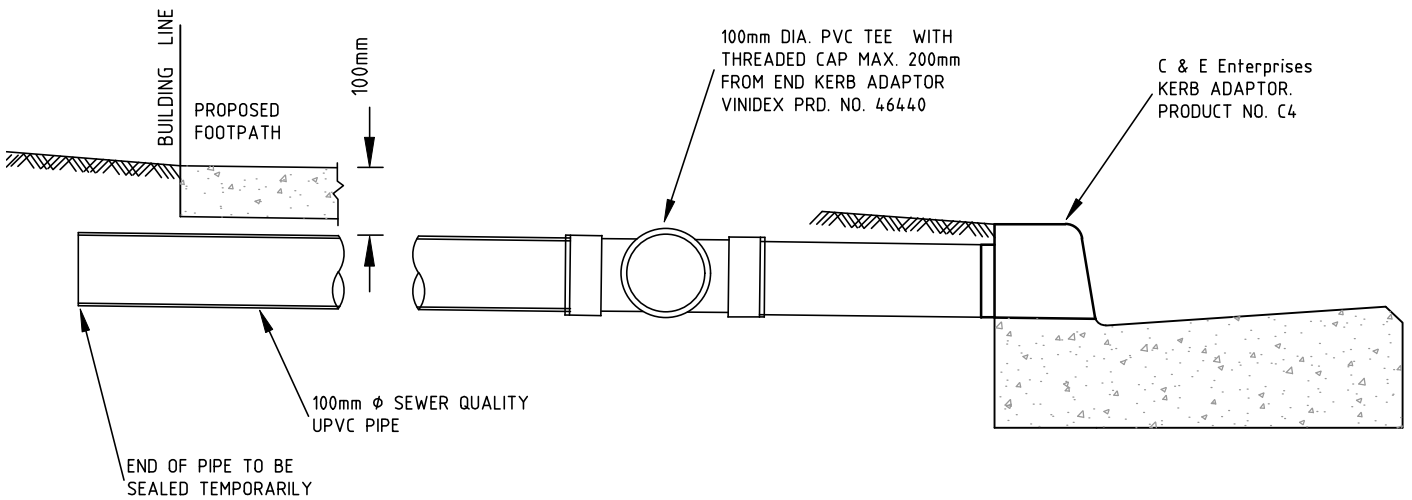
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PLAN

1. ALL PIPES, FITTINGS AND KERB ADAPTOR TO BE SEWER QUALITY UPVC.
2. ALL CONNECTIONS TO BE CARRIED OUT USING FABRICATED FITTINGS TO MANUFACTURERS SPECIFICATIONS.
3. HOUSE DRAIN TO BE LAID AT A MINIMUM GRADE OF 1 IN 100.
4. WHERE A CHANGE OF DIRECTION IN THE PIPE IS REQUIRED A SEWER QUALITY UPVC I.O. IS TO BE PROVIDED.
5. FOR NEW KERB AND CHANNEL, ADAPTORS MUST BE PLACED WITHIN 1 HOUR OF CONCRETE POUR.
6. ALL PROPRIETARY COMPONENTS TO BE SUPPLIED BY MANUFACTURERS LISTED OR OTHERS IF APPROVED BY COUNCIL.
7. KNOWN MELBOURNE RETAILERS OF C & E KERB ADAPTORS AS AT FEBRUARY 2025 ARE:

   R&S GRATING, 13 HEALY ROAD DANDENONG SOUTH.
   Ph. 9238 5888
   www.grating.com.au
8. 'C4' ADAPTOR SHOWN IS FOR BK1 KERB PROFILE. C. & E. ENTERPRISES STOCK DIFFERENT KERB ADAPTORS FOR OTHER KERB PROFILES.



SECTION A – A

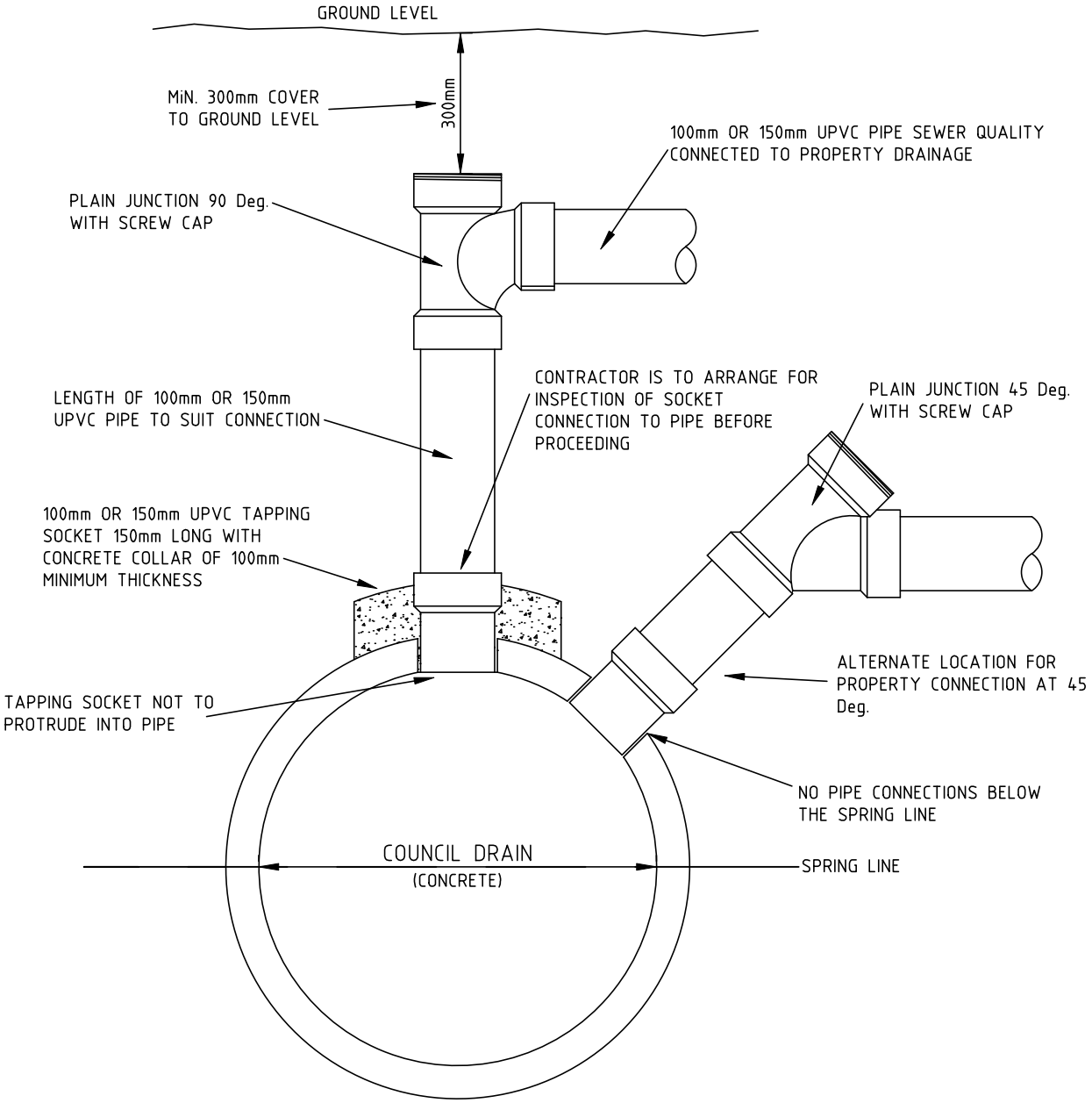
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S501

ISSUE DATE: 19/12/25

CONNECTION OF HOUSE STORMWATER DRAIN
TO KERB AND CHANNEL

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



NOTES

1. CONNECTION OPENING INTO COUNCIL DRAIN TO BE KEPT TO A MINIMUM.
2. REINFORCEMENT IN COUNCIL DRAIN TO BE CUT FLUSH WITH THE EDGE OF THE OPENING.
3. SOCKET MUST NOT PROTUDE INTO THE PIPE, MORTAR TO BE NEATLY RENDERED ON THE INSIDE OF THE PIPE.
4. CONTRACTOR MUST CONTACT COUNCIL TO INSPECT THE SOCKET CONNECTION TO THE PIPE NO FURTHER WORK IS TO PROCEED UNTIL THE SOCKET CONNECTION IS INSPECTED.
5. FOR NON CONCRETE STORMWATER PIPE, PROPRIETY FITTINGS ARE TO BE USED AS RECOMMENDED BY THE STORMWATER PIPE MANUFACTURER.

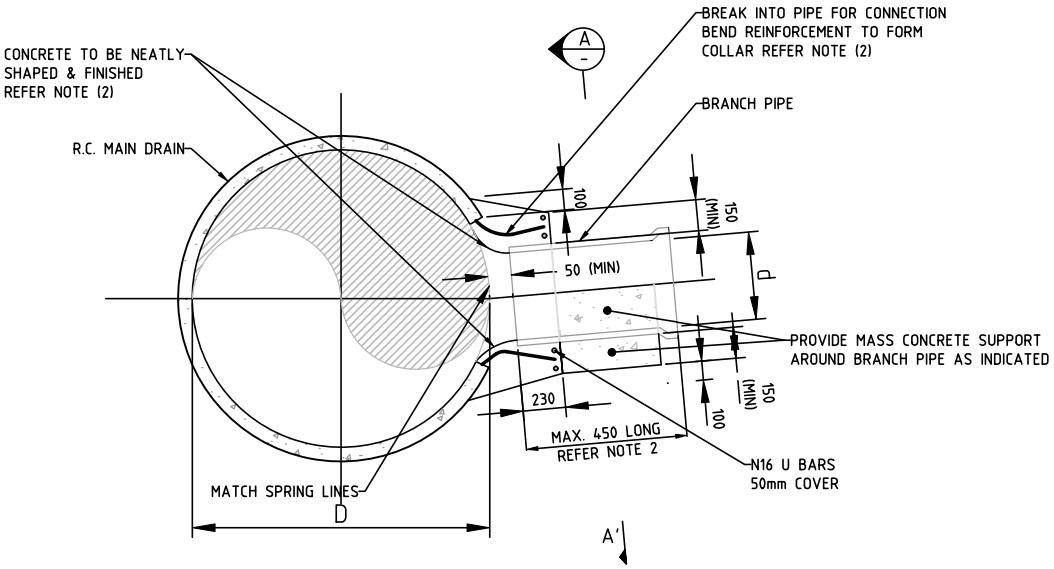
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S503

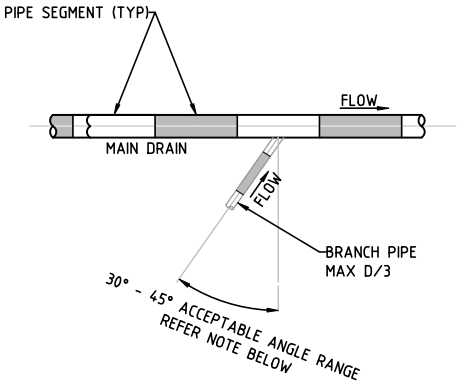
ISSUE DATE: 19/12/25

CONNECTION OF 100 OR 150mm DRAINAGE PIPES TO COUNCIL CONCRETE STORMWATER DRAIN

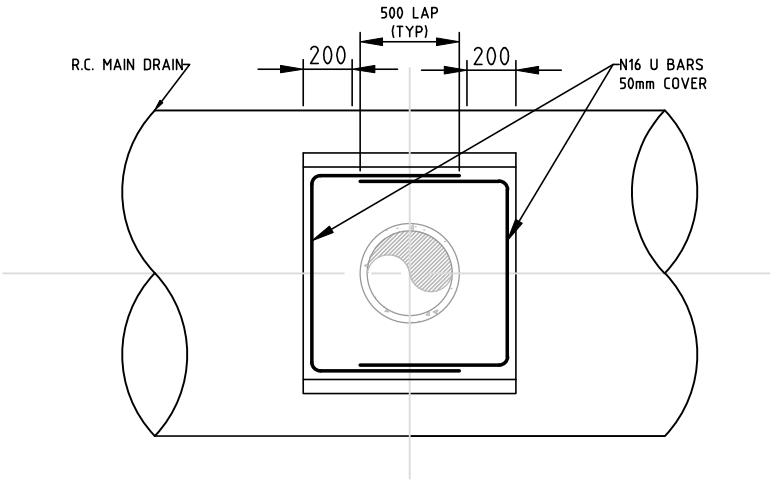
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



BRANCH PIPE UP TO ø 450 PROVIDED
'd' IS LESS THAN 'D/3'



PREFERRED BRANCH LOCATION ALONG MAIN PIPE PLAN



A–A' SECTION

NOTES:

1. PIPE CONNECTIONS TO BE LOCATED AT THE SPRING LINE OF MAIN PIPE. IF THIS IS NOT POSSIBLE THAN A JUNCTION PIT MUST BE CONSTRUCTED IN LIEU OF THIS DETAIL.
2. REINFORCEMENT IN MAIN PIPE TO BE CUT AND FORMED INTO COLLAR. CARE SHOULD BE TAKEN TO PREVENT DAMAGE TO PIPE AND TO ENSURE SIZE OPENING IS KEPT TO A MINIMUM.
3. WHERE CONNECTION CANNOT BE RENDERED FROM INSIDE MAIN PIPE, CONNECTION IS TO CONSIST OF A 300mm LONG STUB TO ALLOW RENDERING FROM OUTSIDE.
4. ANY CONNECTION WORKS PERFORMED FROM INSIDE A COUNCIL DRAIN MUST BE IN ACCORDANCE WITH 'CONFINED SPACE ENTRY' WORK PROCEDURES.
5. BONDING OF NEW TO OLD CONCRETE SHALL BE SUFFICIENT TO ENSURE NO CRACKS OR LEAKS FORM. CONCRETE SURFACES TO BE SCABBLED, CLEANED PRIMED WITH PARCHEM NITOBOND SBR. ALL REINFORCEMENT WITHOUT ADEQUATE COVER (IE. LESS THAN 30mm) TO BE EPOXY SEALED OR SIMILARLY TREATED.
6. MINIMUM CONCRETE COMPRESSIVE STRENGTH: 32 MPa.
7. WHEN 'd' IS GREATER THAN 'D/3' A NEW JUNCTION PIT MUST BE CONSTRUCTED.
8. FOR ALL CONNECTIONS GREATER THAN 150mm DIAMETER ONLY CONCRETE PIPES SHALL BE USED.
9. PREFERENCE SHOULD BE GIVEN TO ANGLING BRANCH PIPE AT 45° IN DOWN STREAM DIRECTION. WHEN 'd' < D/6, ANGLE OF BRANCH PIPE MAY BE INSTALLED PERPENDICULAR TO MAIN PIPE.

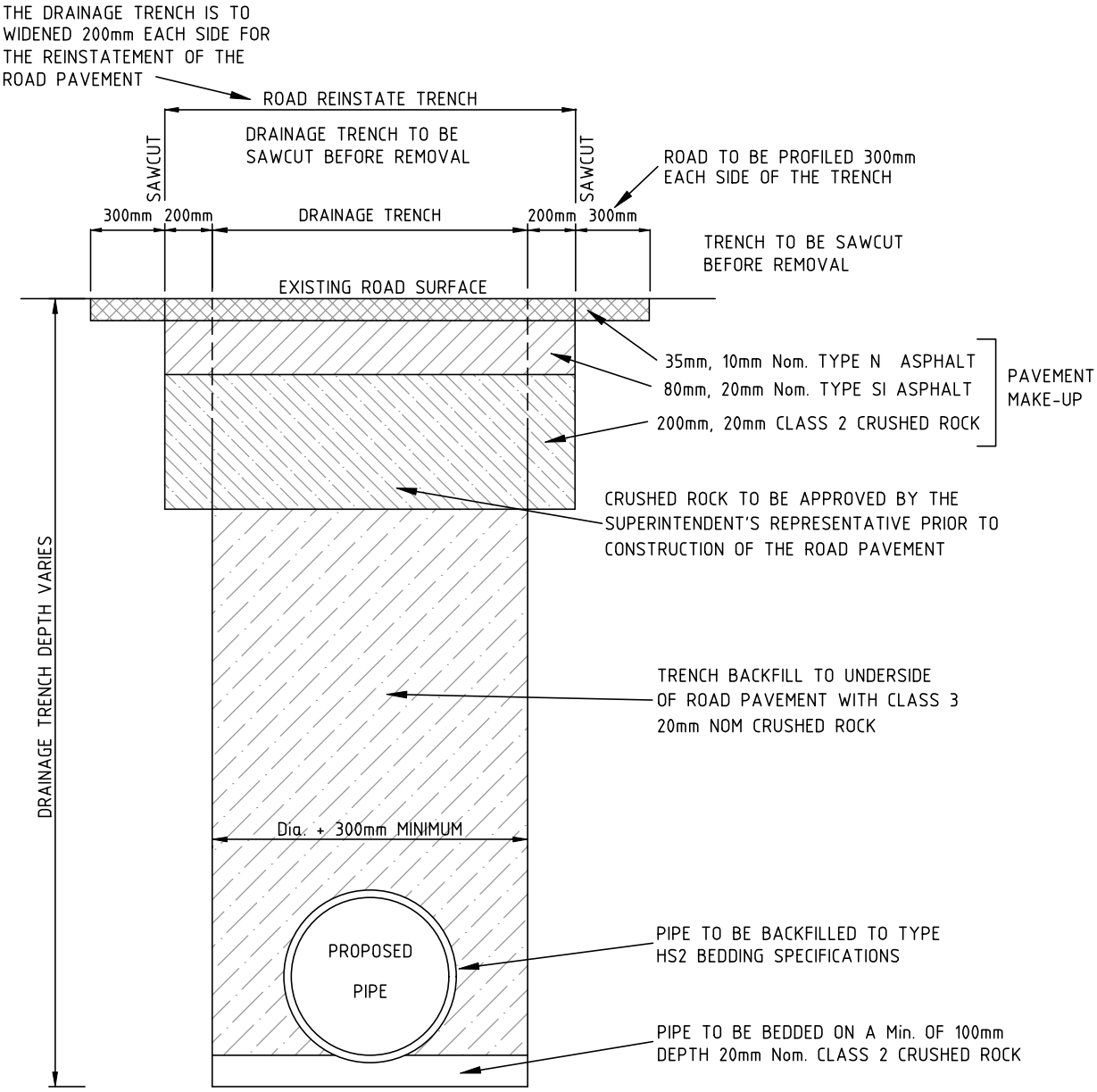
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S504

ISSUE DATE: 19/12/25

PIPE TO PIPE CONNECTION TO COUNCIL PIPES FOR BRANCH PIPES GREATER THAN 150mmø

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PIPE BACKFILL DETAILS
IN ASPHALT ROAD PAVEMENT

NOT TO SCALE

PIPE BACKFILL DETAILS

1. ALL PIPES LOCATED UNDER THE ROAD PAVEMENT TO HAVE THEIR TRENCHES NEATLY SAWCUT PRIOR TO REMOVAL WITH THE PIPES TO BE BACKFILLED AS PER THE DETAIL SHOWN ABOVE.
2. THE DRAINAGE TRENCH IS TO WIDENED AFTER THE DRAINAGE CONSTRUCTION TO ALLOW FOR THE REINSTATEMENT OF THE ROAD PAVEMENT AS SHOWN ABOVE. ANY WIDENING OF THE TRENCHES OVER THE DIMENSIONS AS SHOWN ABOVE IS TO BE AT THE CONTRACTORS EXPENSE.
3. FOR ALL OTHER ROAD TYPES PAVEMENT MAKEUP TO BE DIRECTED BY COUNCIL.

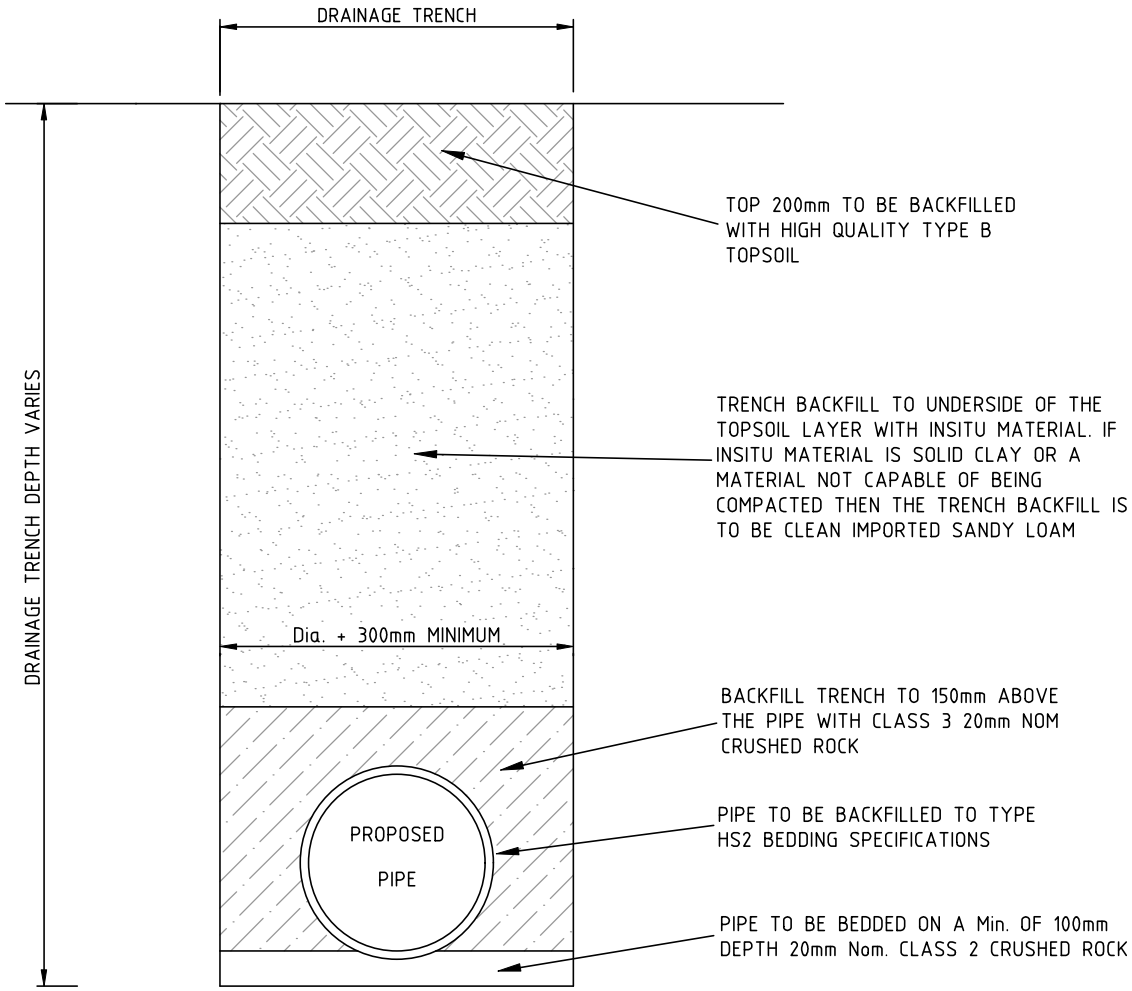
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S505

ISSUE DATE: 19/12/25

PIPE BACKFILL DETAIL RESIDENTIAL PAVEMENTS, UNDER KERB OR IN NATURESTRIPS WITHIN 1m OF BACK OF KERB.

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PIPE BACKFILL DETAILS
IN EASEMENTS
NOT TO SCALE

PIPE BACKFILL DETAILS

1. ALL DRAINAGE TRENCHES WITHIN 500mm OF PROPERTY BOUNDARIES, STRUCTURES SUCH AS SHEDS, GARAGES, Etc., DRIVEWAYS OR PAVED AREAS ARE TO BE BACKFILLED TO THE UNDERSIDE OF THE STRUCTURE OR PAVING WITH CLASS 3 20mm Nom. CRUSHED ROCK.

2. COMPACTION OF ALL PIPE BACKFILL ZONES TO BE TO COUNCILS STANDARD SPECIFICATIONS FOR ROAD AND DRAINAGE WORKS

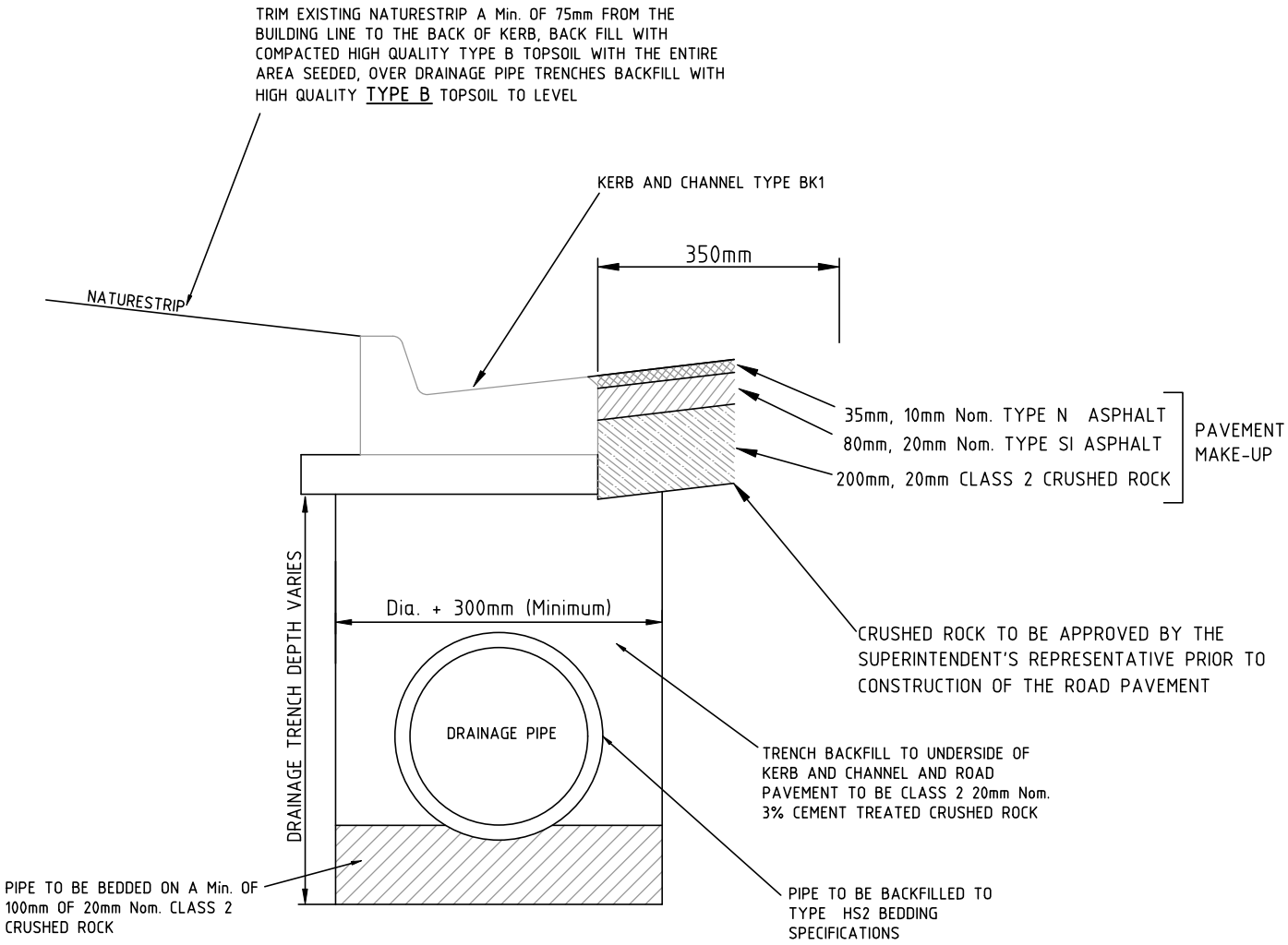
KINGSTON CITY COUNCIL
STANDARD DRAWING

PIPE BACKFILL DETAIL EASEMENTS

DRG. NO. S506

ISSUE DATE: 19/12/25

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



PIPE BACKFILL DETAILS FOR PIPE UNDER THE KERB AND CHANNEL OR IN NATURE STRIPS WITHIN 1m OF BACK OF KERB

NOT TO SCALE

PIPE BACKFILL DETAILS

1. ALL PIPES LOCATED UNDER THE ROAD PAVEMENT TO HAVE THEIR TRENCHES NEATLY SAWCUT PRIOR TO REMOVAL WITH THE PIPES TO BE BACKFILLED AS PER THE DETAIL SHOWN ABOVE.
2. THE DRAINAGE TRENCH IS TO WIDENED AFTER THE DRAINAGE CONSTRUCTION TO ALLOW FOR THE REINSTATEMENT OF THE ROAD PAVEMENT AS SHOWN ABOVE. ANY WIDENING OF THE TRENCHES OVER THE DIMENSIONS AS SHOWN ABOVE IS TO BE AT THE CONTRACTORS EXPENSE.
3. FOR ALL OTHER ROAD TYPES PAVEMENT MAKEUP TO BE DIRECTED BY COUNCIL.

KINGSTON CITY COUNCIL
STANDARD DRAWING

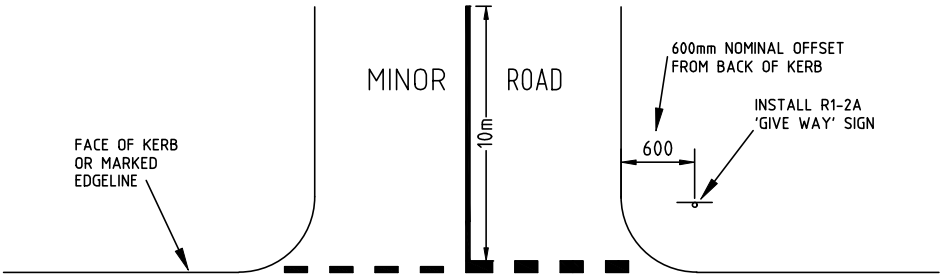
PIPE BACKFILL UNDER KERB AND CHANNEL

DRG. NO. S507

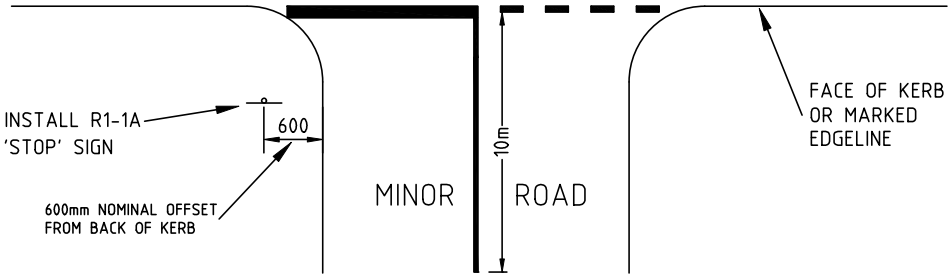
ISSUE DATE: 19/12/25

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

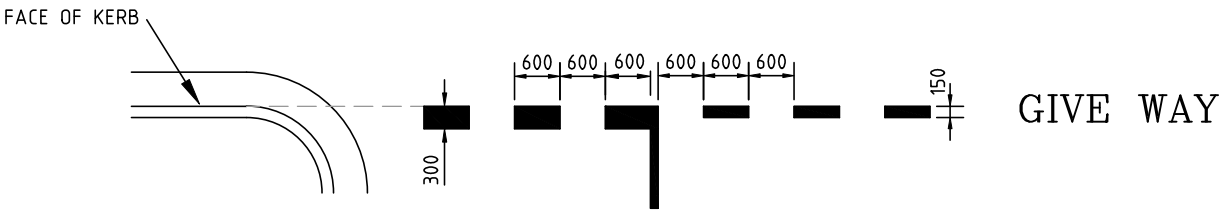
NOTE - SIZE OF SIGNS TO BE CONFIRMED BY SUPERINTENDANT'S REPRESENTATIVE PRIOR TO INSTALLATION

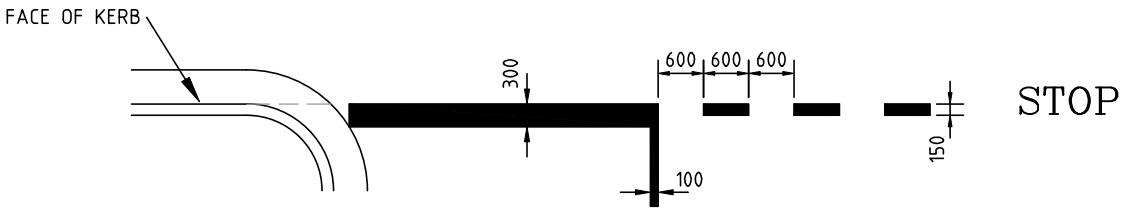


MAJOR ROAD



PLAN





DETAILS OF MARKINGS

LINEMARKING MATERIAL TO BE DETERMINED BY COUNCIL

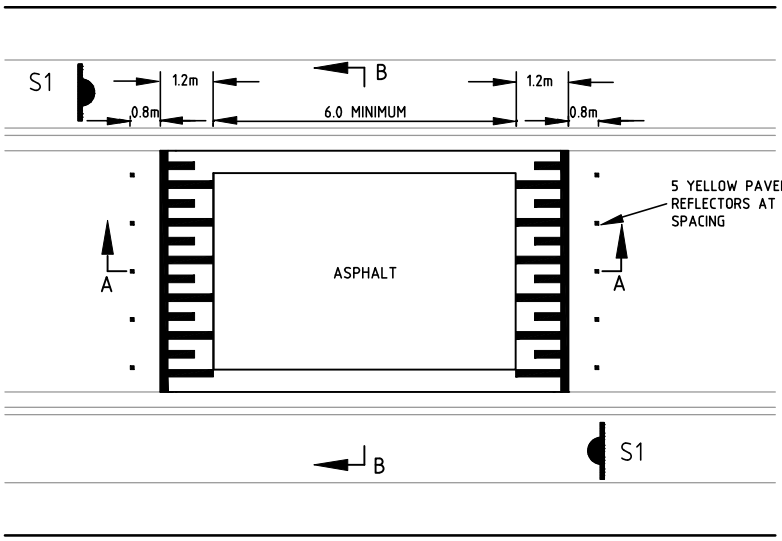
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S601

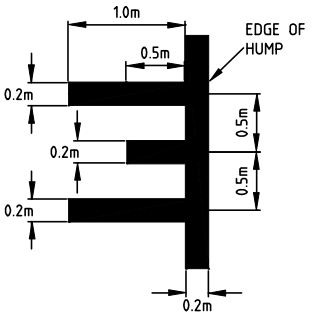
ISSUE DATE: 19/12/25

'GIVEWAY' AND 'STOP' LINEMARKING AND SIGNAGE
GENERAL URBAN AND RURAL USE

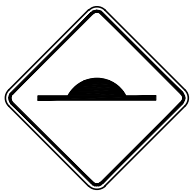
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



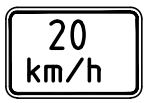
PLAN



LINE MARKING



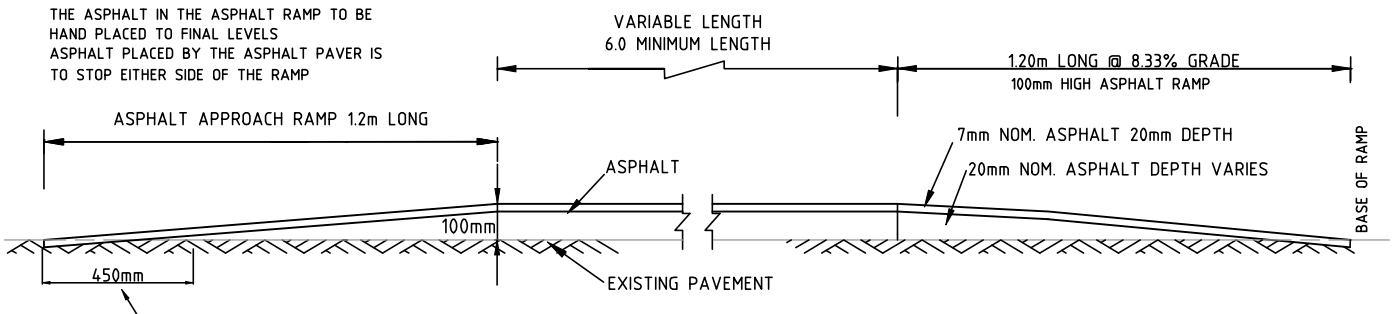
SIGN S1
ROAD HUMP W5-10A



SIGN S1
ADVISORY SPEED
SIGN...km/h W8-2A
600×400mm

SIGNAGE AND LINEMARKING NOTES

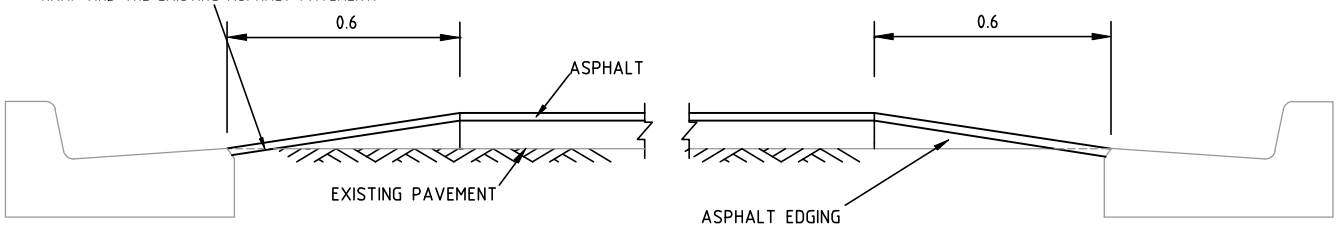
1. ALL SIGNAGE TO BE MANUFACTURED IN ACCORDANCE WITH AS1742 "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (CURRENT EDITION)
2. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL SIGNS, RRPM'S AND LINEMARKING TO BE INSTALLED IN ACCORDANCE WITH VICROADS TRAFFIC ENGINEERING MANUAL VOLUME 2, "SIGNS AND MARKINGS"
3. ALL LINEMARKING TO BE CARRIED OUT BY A CONTRACTOR APPROVED BY COUNCIL'S SUPERINTENDENT'S REPRESENTATIVE.
4. ALL LINEMARKING MATERIALS TO BE THERMOPLASTIC IN ACCORDANCE WITH VICROADS STANDARD SPECIFICATIONS FOR ROADWORKS AND BRIDGEWORKS SECTION 722 PART D: LONGLIFE PAVEMENT MARKINGS IN THERMOPLASTIC OR COLD-APPLIED PLASTIC WITH GLASS BEADS AND OTHER REQUIREMENTS.
5. FOR THE ASPHALT PAVEMENT; THE RAISED PLATFORM SHALL BE LAID AND ROLLED FIRST. A MINIMUM OF 2 HOURS LATER AFTER THE ASPHALT HAS GONE COLD, THE CONTRACTOR SHALL SAWCUT A STRAIGHT EDGE AND THEN HAND PLACE AND ROLL THE 1.2m LONG ASPHALT RAMPS TO ENSURE A SHARP CHANGE OF DIRECTION. THE ROLLING OF THE RAMPS IS TO BE PERPENDICULAR TO THE DIRECTION OF CAR TRAVEL TO ENSURE A UNIFORM RAMP.



SECTION A-A

AT THE BASE OF THE RAMP ON ALL 4 SIDES PROVIDE A 450mm LONG 0-40mm ASPHALT CHASE TO PROVIDE A SMOOTH TRANSITION BETWEEN THE RAMP AND THE EXISTING ASPHALT PAVEMENT.

FOR RAMPS IN THE EXISTING PAVEMENT SAWCUT THE EXISTING PAVEMENT AT THE BOTTOM OF THE RAMP TO A DEPTH OF 40mm PRIOR TO PROFILING THE EXISTING PAVEMENT TO PROVIDE A STRAIGHT NEAT LINE TO MATCH THE ASPHALT RAMP INTO.



SECTION B-B

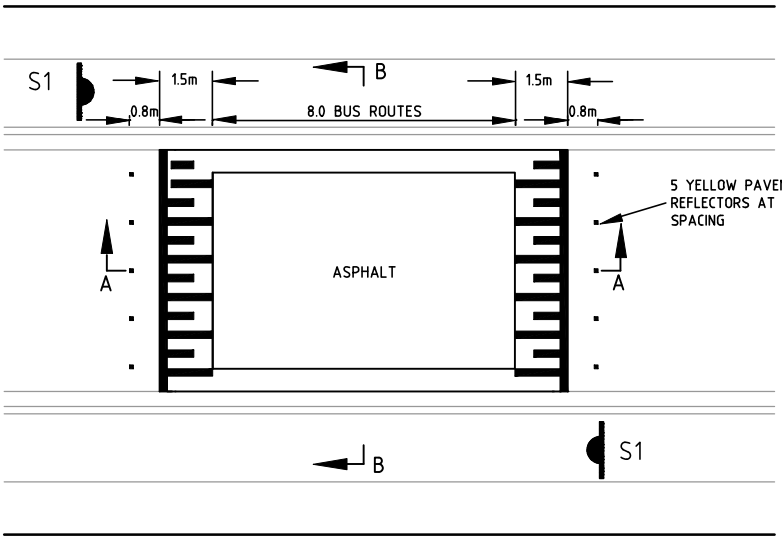
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S602

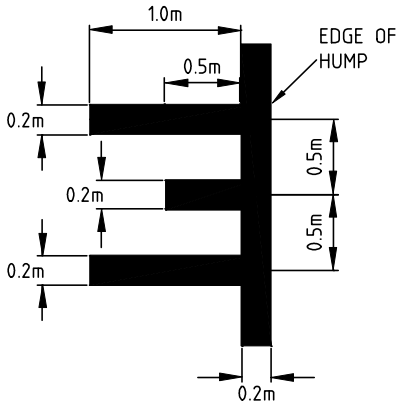
ISSUE DATE: 19/12/25

STANDARD RAISED PAVEMENT
NON BUS ROUTES (SEPARATE DRAWING FOR BUS ROUTES)

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



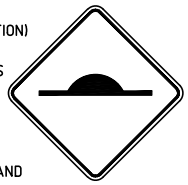
PLAN



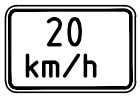
LINE MARKING

SIGNAGE AND LINEMARKING NOTES

1. ALL SIGNAGE TO BE MANUFACTURED IN ACCORDANCE WITH AS1742 "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (CURRENT EDITION)
2. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL SIGNS, RRPM'S AND LINEMARKING TO BE INSTALLED IN ACCORDANCE WITH VICROADS TRAFFIC ENGINEERING MANUAL VOLUME 2, "SIGNS AND MARKINGS"
3. ALL LINEMARKING TO BE CARRIED OUT BY A CONTRACTOR APPROVED BY COUNCIL'S SUPERINTENDENT'S REPRESENTATIVE.
4. ALL LINEMARKING MATERIALS TO BE THERMOPLASTIC IN ACCORDANCE WITH VICROADS STANDARD SPECIFICATIONS FOR ROADWORKS AND BRIDGEWORKS SECTION 722 PART D: LONGLIFE PAVEMENT MARKINGS IN THERMOPLASTIC OR COLD-APPLIED PLASTIC WITH GLASS BEADS AND OTHER REQUIREMENTS.
5. FOR THE ASPHALT PAVEMENT; THE RAISED PLATFORM SHALL BE LAID AND ROLLED FIRST. A MINIMUM OF 2 HOURS LATER AFTER THE ASPHALT HAS GONE COLD, THE CONTRACTOR SHALL SAWCUT A STRAIGHT EDGE AND THEN HAND PLACE AND ROLL THE 1.2m LONG ASPHALT RAMPS TO ENSURE A SHARP CHANGE OF DIRECTION. THE ROLLING OF THE RAMPS IS TO BE PERPENDICULAR TO THE DIRECTION OF CAR TRAVEL TO ENSURE A UNIFORM RAMP.

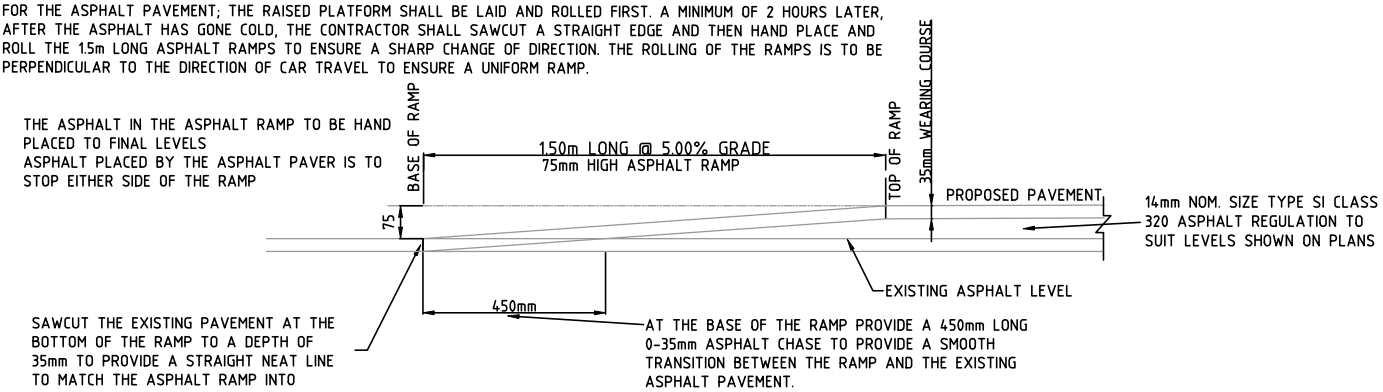


SIGN S1
ROAD HUMP W5-10A



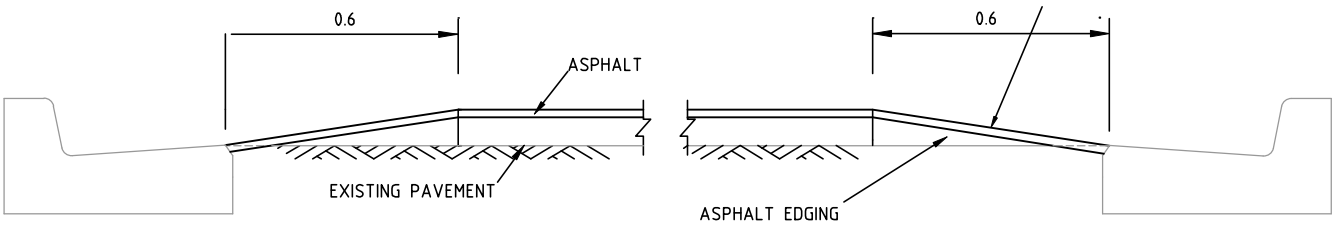
SIGN S1
ADVISORY SPEED
SIGN...km/h W8-2A
600×400mm

FOR THE ASPHALT PAVEMENT; THE RAISED PLATFORM SHALL BE LAID AND ROLLED FIRST. A MINIMUM OF 2 HOURS LATER, AFTER THE ASPHALT HAS GONE COLD, THE CONTRACTOR SHALL SAWCUT A STRAIGHT EDGE AND THEN HAND PLACE AND ROLL THE 1.5m LONG ASPHALT RAMPS TO ENSURE A SHARP CHANGE OF DIRECTION. THE ROLLING OF THE RAMPS IS TO BE PERPENDICULAR TO THE DIRECTION OF CAR TRAVEL TO ENSURE A UNIFORM RAMP.



RAISED PLATFORM RAMP SECTION
NOT TO SCALE

AT THE BASE OF THE RAMP ON ALL 4 SIDES PROVIDE A 450mm LONG 0-40mm ASPHALT CHASE TO PROVIDE A SMOOTH TRANSITION BETWEEN THE RAMP AND THE EXISTING ASPHALT PAVEMENT.



SECTION B-B

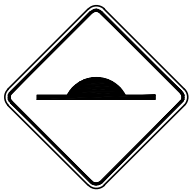
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S603

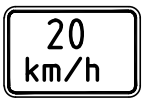
ISSUE DATE: 19/12/25

STANDARD RAISED PAVEMENT
FOR BUS ROUTES

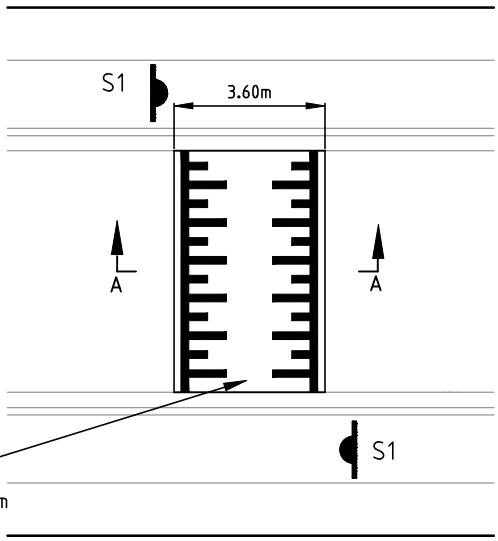
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



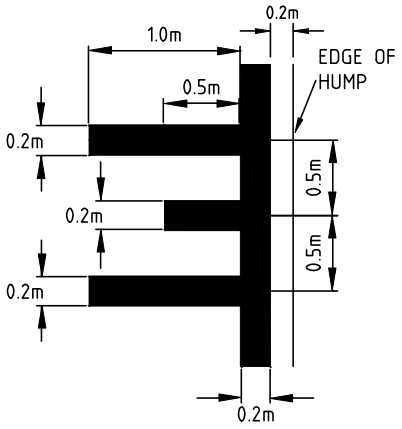
SIGN S1
ROAD HUMP
W5-10A



SIGN S1
ADVISORY
SPEED SIGN.
20km/h W8-2A
600×400

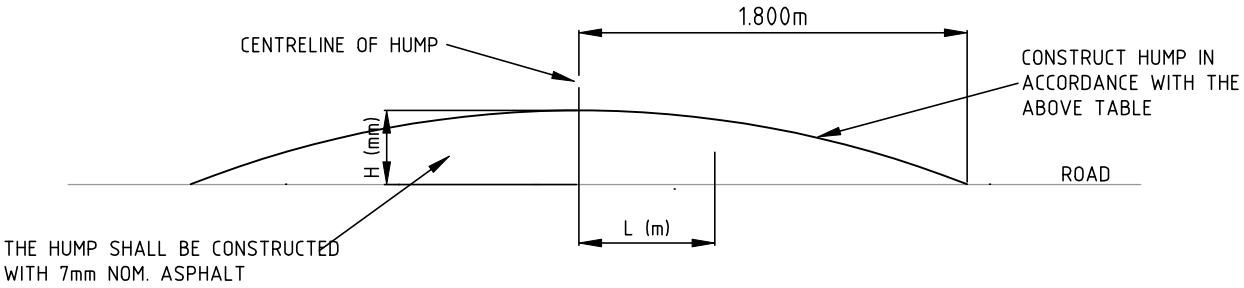


PLAN



LINE MARKING

WATTS PROFILE SPEEDHUMP PROFILE																			
L (m)	0.0	0.1	0.2	0.3	0.4	0.5	0.6	0.7	0.8	0.9	1.0	1.1	1.2	1.3	1.4	1.5	1.6	1.7	1.8
H (mm)	100	100	99	97	95	93	90	86	81	76	71	65	58	51	43	34	25	16	5



SECTION A-A

SIGNAGE AND LINEMARKING NOTES

1. ALL SIGNAGE TO BE MANUFACTURED IN ACCORDANCE WITH AS1742 "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (CURRENT EDITION)

2. UNLESS OTHERWISE SHOWN ON THE PLANS, ALL SIGNS, RRPM'S AND LINEMARKING TO BE INSTALLED IN ACCORDANCE WITH VICROADS TRAFFIC ENGINEERING MANUAL VOLUME 2, "SIGNS AND MARKINGS"

3. ALL LINEMARKING TO BE CARRIED OUT BY A CONTRACTOR APPROVED BY COUNCIL'S SUPERINTENDENT'S REPRESENTATIVE.

4. ALL LINEMARKING MATERIALS TO BE THERMOPLASTIC IN ACCORDANCE WITH VICROADS STANDARD SPECIFICATIONS FOR ROADWORKS AND BRIDGEWORKS SECTION 722 PART D: LONGLIFE PAVEMENT MARKINGS IN THERMOPLASTIC OR COLD-APPLIED PLASTIC WITH GLASS BEADS AND OTHER REQUIREMENTS.

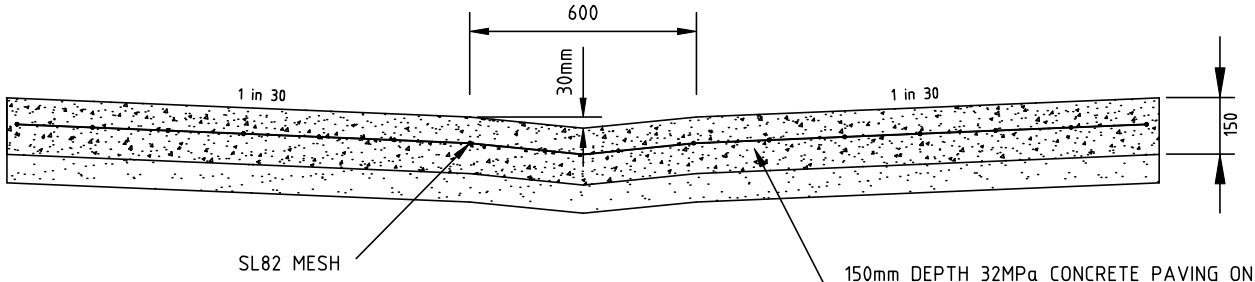
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S604

ISSUE DATE: 19/12/25

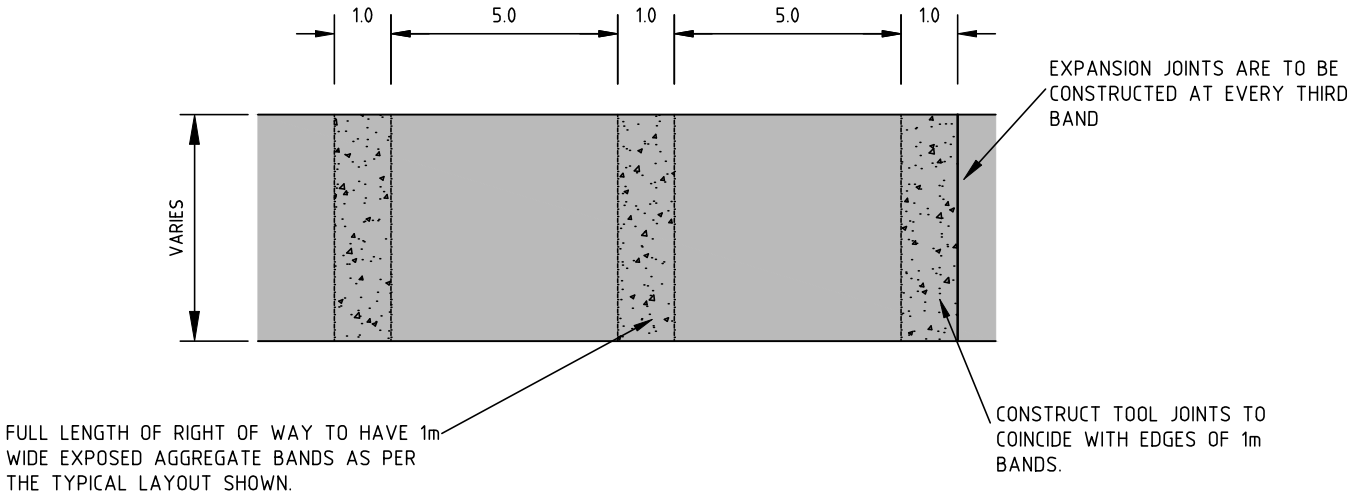
'WATTS' PROFILE SPEED HUMP

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



TYPICAL CROSS SECTION

NOT TO SCALE



TYPICAL LAYOUT PLAN

NOT TO SCALE

NOTES:

1. CONCRETE STRENGTH TO BE 32MPa AT 28 DAYS
2. PAVEMENT DEPTH AND REINFORCING SHOWN IS FOR RIGHT OF WAYS ABBUTTING RESIDENTIAL PROPERTIES WITH SUBGRADES OF A MINIMUM CBR OF 10. FOR LOCATIONS INVOLVING LOWER CBR's OR WHERE COMMERCIAL TRAFFIC CAN BE ANTICIPATED PAVEMENT DEPTH AND REINFORCING STEELS IS TO BE DESIGNED ON A SITE SPECIFIC BASIS.
3. EXPOSED BANDS ARE TO BE POURED MONOLITHICALLY WITH ADJOINING COLOURED CONCRETE.
4. EXPANSION JOINTS ARE TO BE CONSTRUCTED AT EVERY THIRD BAND USING A 'CONNOLLY EXPANSION JOINT'. CONTACT 'CONNOLLY KEY JOINT' ON 1800 335 215 FOR DETAILS.
5. THERE ARE MULTIPLE CHEMICAL RETARDENTS READILY AVAILABLE TO PROVIDE THE EXPOSED AGGREGATE FINISH.
6. THE RETARDER IS TO BE APPLIED IN A UNIFORM APPLICATION TO THE FRESHLY PLACED CONCRETE SURFACE PRIOR TO THE INITIAL SET.
7. ONCE THE CONCRETE HAS SET THE SOFT CEMENT PASTE MAY BE BRUSHED OFF. EXACT TIMING OF THE CHEMICAL APPLICATION AND CEMENT PASTE REMOVAL IS TO BE IN ACCORDANCE WITH THE MANUFACTURERS DETAILS.

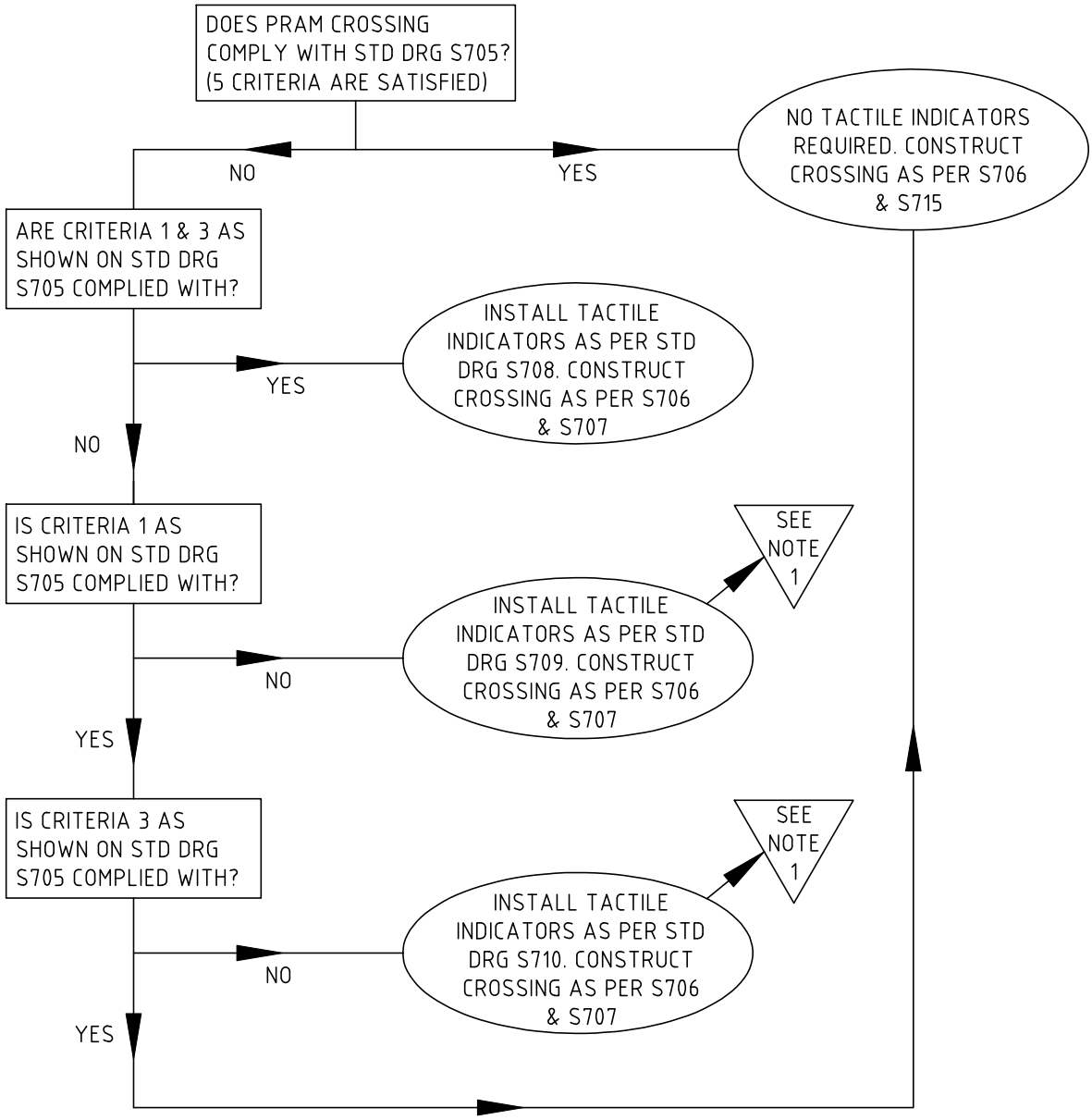
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S605

ISSUE DATE: 19/12/25

STANDARD RIGHT OF WAY
CONSTRUCTION DETAILS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



THE FLOW CHART ABOVE REFERS DIRECTLY OR INDIRECTLY TO THE FOLLOWING STANDARD DRAWINGS

S702 WARNING TACTILE INDICATORS
S703 DIRECTIONAL TACTILE INDICATORS
S704 PATH OF TRAVEL FOR SIGHT IMPAIRED
S705 CRITERIA SUCH THAT TACTILE INDICATORS ARE NOT REQUIRED
S706 PRAM CROSSING LAYOUT
S707 PRAM RAMP DIMENSIONS FOR CROSSINGS WITH TACTILE INDICATORS.
S708 TACTILE INDICATOR LAYOUT IF CRITERIA 2, 4 OR 5 ARE NOT COMPLIED WITH
S709 EXAMPLE TACTILE INDICATOR LAYOUT IF CRITERIA 1 IS NOT COMPLIED WITH
S710 EXAMPLE TACTILE INDICATOR LAYOUT IF CRITERIA 3 IS NOT COMPLIED WITH
S711 CHANGE OF GRADE BETWEEN APPROACH & RAMP SO THAT TACTILE INDICATORS ARE NOT REQUIRED
S712 SPLITTER ISLAND EXAMPLE TACTILE INDICATOR LAYOUT
S713 MID BLOCK CROSSINGS TACTILE INDICATOR LAYOUT
S714 BUS STOP TACTILE INDICATOR LAYOUT
S715 PRAM RAMP DIMENSIONS FOR CROSSINGS WITHOUT TACTILE INDICATORS.

NOTE 1. IF THE EXAMPLE PATH CONFIGURATIONS DO NOT REPRESENT THE ON SITE SITUATION THEN CONTACT COUNCIL TO NOMINATE THE REQUIRED TACTILE INDICATOR LAYOUT

KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S701

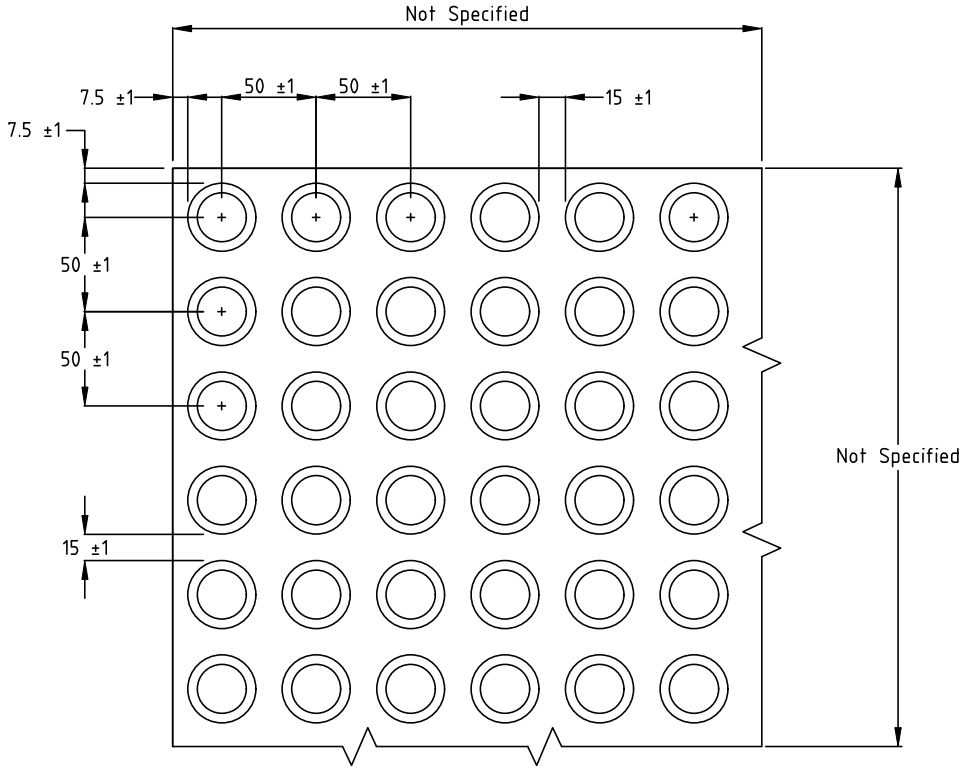
ISSUE DATE: 19/12/25

TACTILE INDICATOR REQUIREMENTS FOR PRAM CROSSINGS
FLOW CHART

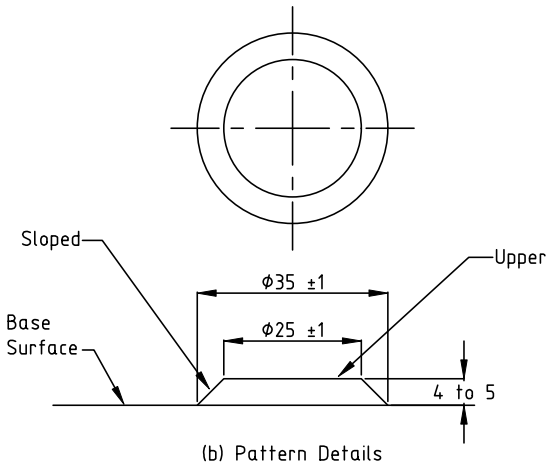
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

2.2.3.3 Design requirements

The design of warning indicators shall comply with Figure 1.



(a) Top view



(b) Pattern Details

DIMENSIONS IN MILLIMETRES

FIGURE 1 TYPICAL WARNING INDICATOR PATTERN

NOTES

REFER TO SECTION 2.2.3.3 OF AS1428.4 :2002 (PAGE 12) FOR THE DESIGN REQUIREMENTS OF WARNING INDICATORS.

REFER TO SECTION 2.1(b) OF AS1428.4 :2002 (PAGE 10) FOR LUMINANCE CONTRAST CRITERIA FOR TACTILE INDICATORS.

NOTE THAT COUNCIL HAS ADOPTED THE USE OF TACTILE INDICATOR LAYOUTS PLACED IN MULTIPLES OF 300mm (EG WARNING INDICATOR PADS ON PRAM CROSSINGS ARE GENERALLY 900mm x 600mm)

KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S702

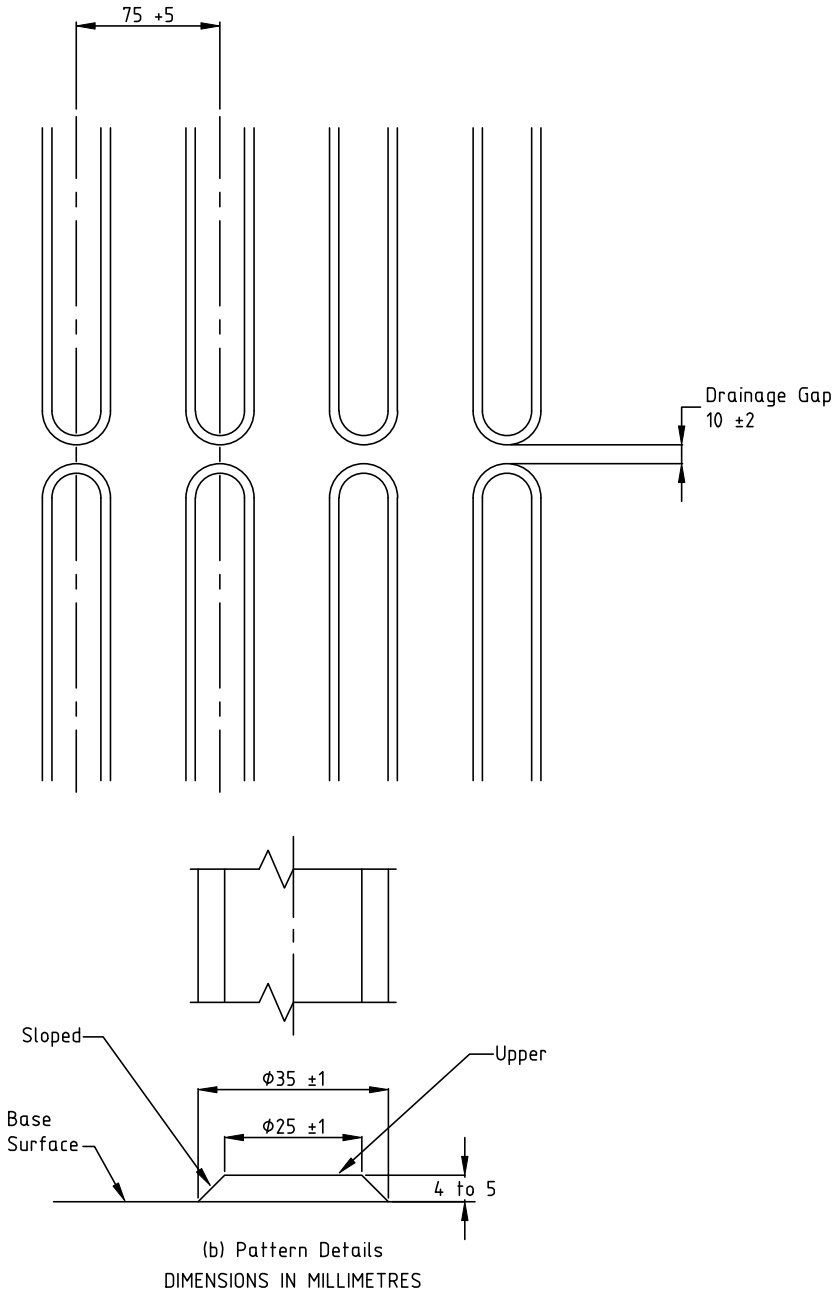
ISSUE DATE: 19/12/25

WARNING TACTILE INDICATORS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

2.243.3 Design requirements

The design of directional indicators shall comply with Figure 2.



(b) Pattern Details

DIMENSIONS IN MILLIMETRES

FIGURE 2 TYPICAL DIRECTIONAL INDICATOR PATTERN

NOTES

REFER TO SECTION 2.2.4.3 OF AS 1428.4 :2002 (PAGES 13 & 14) FOR THE DESIGN REQUIREMENTS OF DIRECTIONAL INDICATORS.

REFER TO SECTION 2.1(b) OF AS1428.4 :2002 (PAGE 10) FOR LUMINANCE CONTRAST CRITERIA FOR TACTILE INDICATORS.

NOTE THAT COUNCIL HAS ADOPTED THE USE OF TACTILE INDICATOR LAYOUTS PLACED IN MULTIPLES OF 300mm (EG WARNING INDICATOR PADS ON PRAM CROSSINGS ARE GENERALLY 900mm x 600mm)

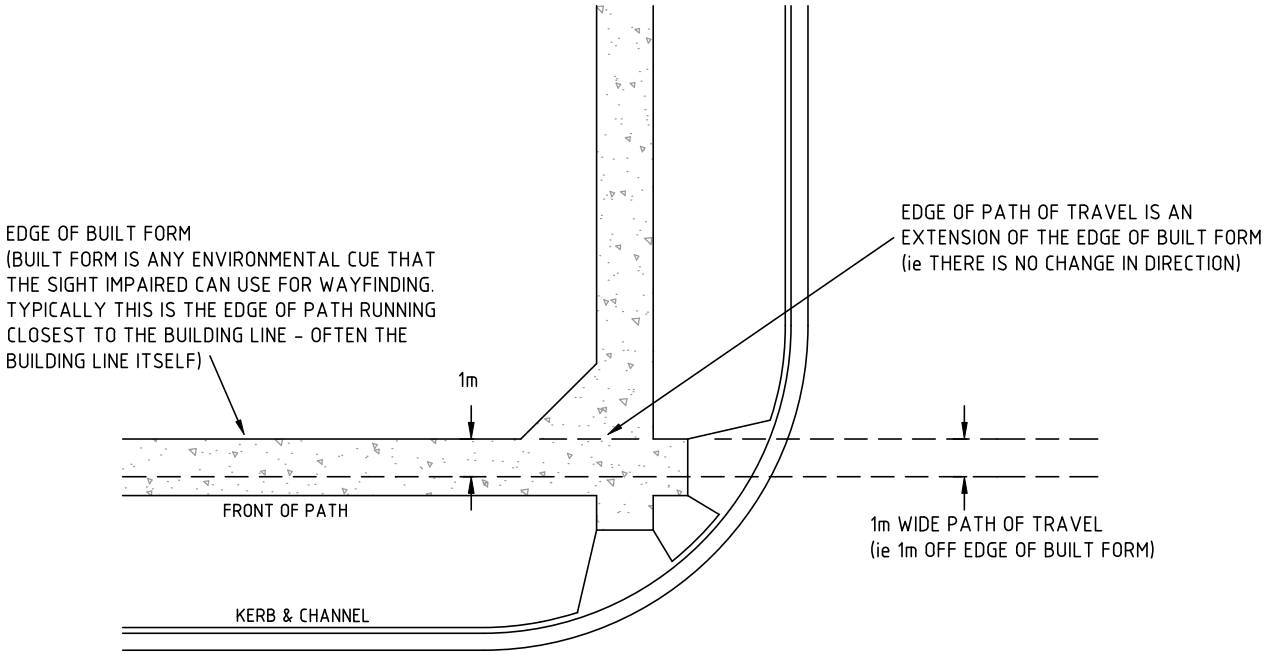
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S703

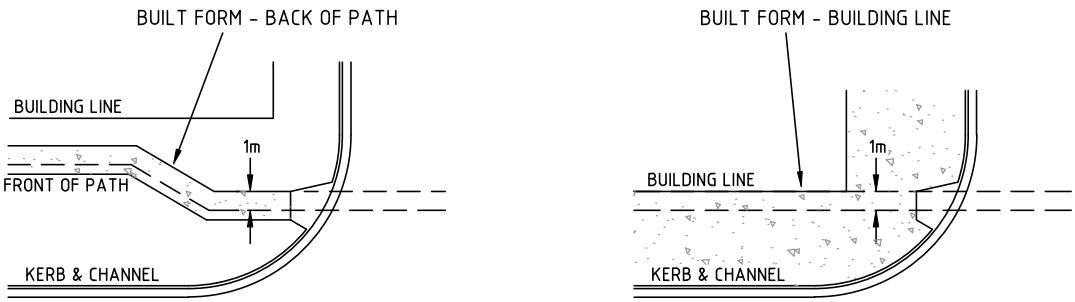
ISSUE DATE: 19/12/25

DIRECTIONAL TACTILE INDICATORS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



EXAMPLES



THIS DRAWING IS INTENDED TO CLARIFY COUNCIL'S DEFINITION OF THE TRAVEL PATH FOR SITE IMPAIRED PEDESTRIANS. THIS DEFINITION IS BASED ON COUNCIL'S UNDERSTANDING OF AS1428.4 :2002

KINGSTON CITY COUNCIL
STANDARD DRAWING

PATH OF TRAVEL FOR SIGHT IMPAIRED

DRG. NO. S704

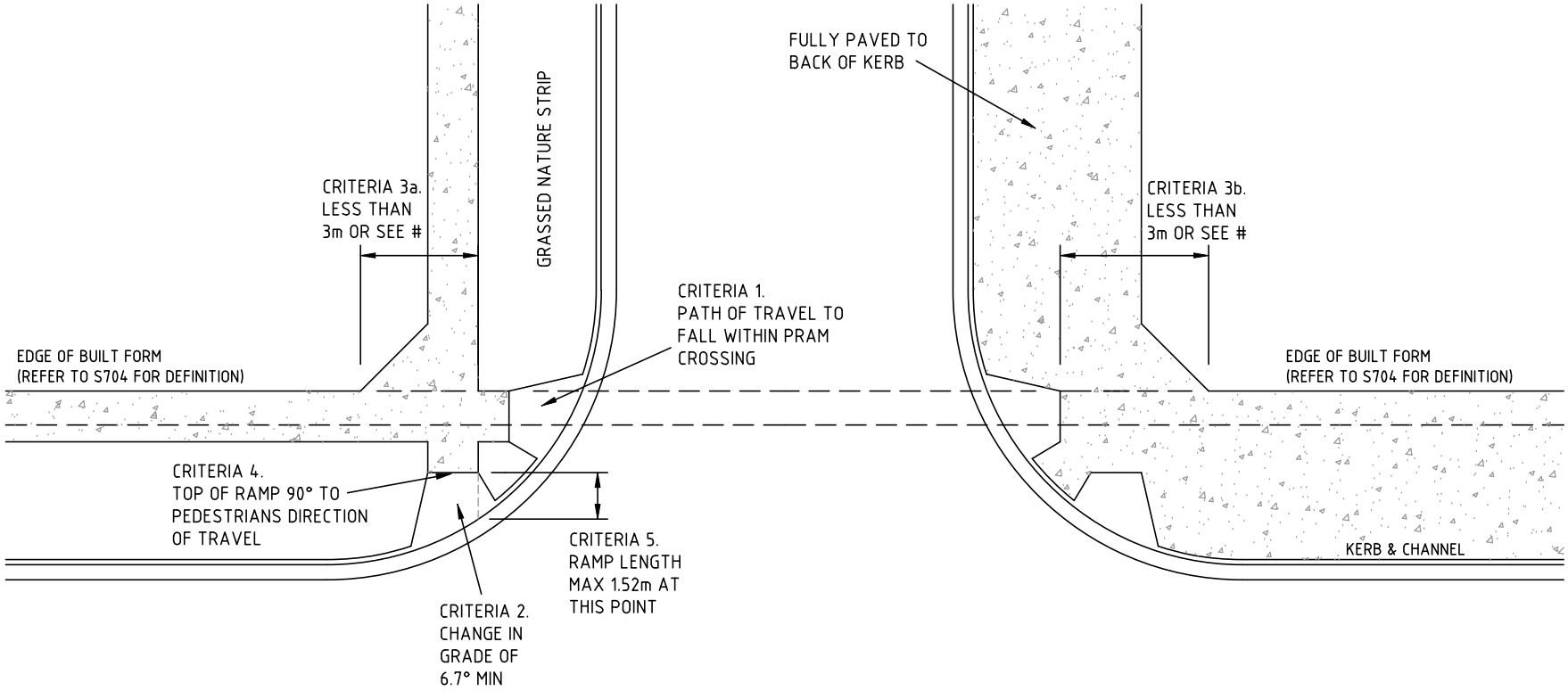
ISSUE DATE: 19/12/25

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

KINGSTON CITY COUNCIL

STANDARD DRAWING

CRITERIA SUCH THAT TACTILE INDICATORS ARE NOT REQUIRED



5 BASIC CRITERIA TO BE MET SUCH THAT TACTILE INDICATORS ARE NOT REQUIRED

1. 1m WIDE PATH OF TRAVEL (REFER TO S704 FOR DEFINITION) TO FALL WITHIN PRAM CROSSINGS
2. CHANGE IN GRADE BETWEEN RAMP AND PATH TO BE 6.7° MIN (REFER TO STD DRG S711)
3. a) EDGE OF NATURE STRIP TO BE WITHIN 3m (SEE #) OF CORNER OF BUILT FORM WHEN NATURE STRIP PRESENT
   b) BACK OF PRAM RAMP TO BE WITHIN 3m (SEE #) OF CORNER OF BUILT FORM WHEN AREA FULLY PAVED
4. BACK OF PRAM RAMP TO BE 90° TO THE PEDESTRIAN'S DIRECTION OF TRAVEL
5. THE SHORT SIDE OF THE PRAM RAMP TO BE A MAXIMUM OF 1.52m

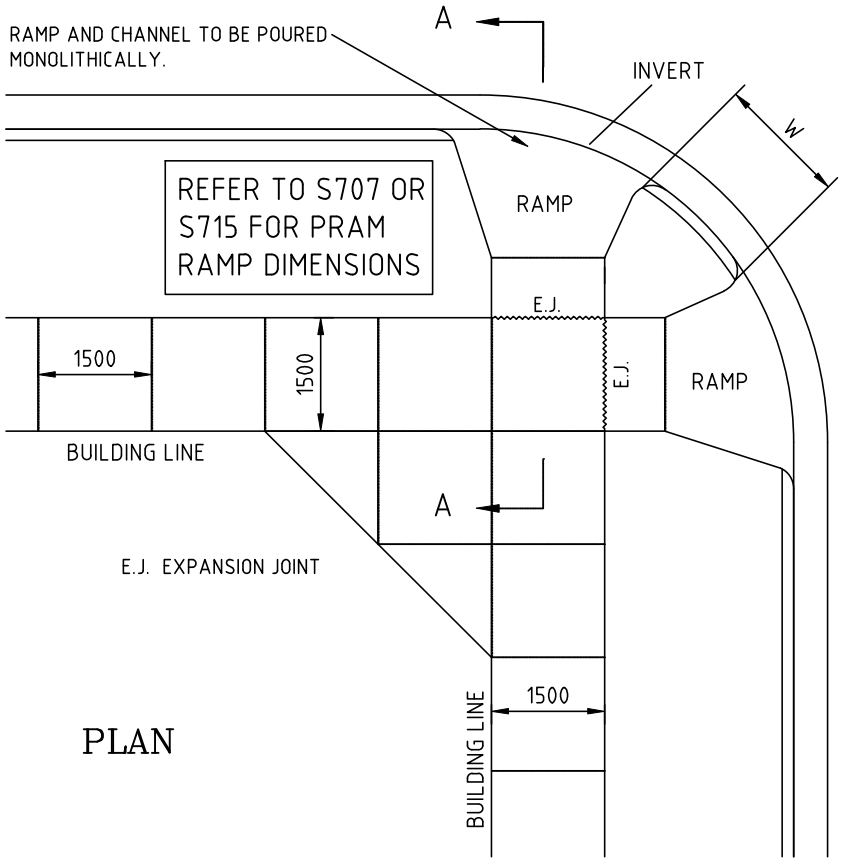
IF THERE IS A REASONABLE EXPECTATION THAT A VISION IMPAIRED PEDESTRIAN WILL BE ABLE TO RE-ESTABLISH THE CORRECT TRAVEL PATH WITHOUT ENDANGERING THEMSELVES THEN THE 3m LIMIT CAN BE INCREASED TO 8m.

NOTE - THESE CRITERIA (EXCLUDING THE NOTE MARKED AS #) ARE COUNCIL'S INTERPRETATION OF AS1428.4 :2002

DRG. NO. S705

ISSUE DATE: 19/12/25

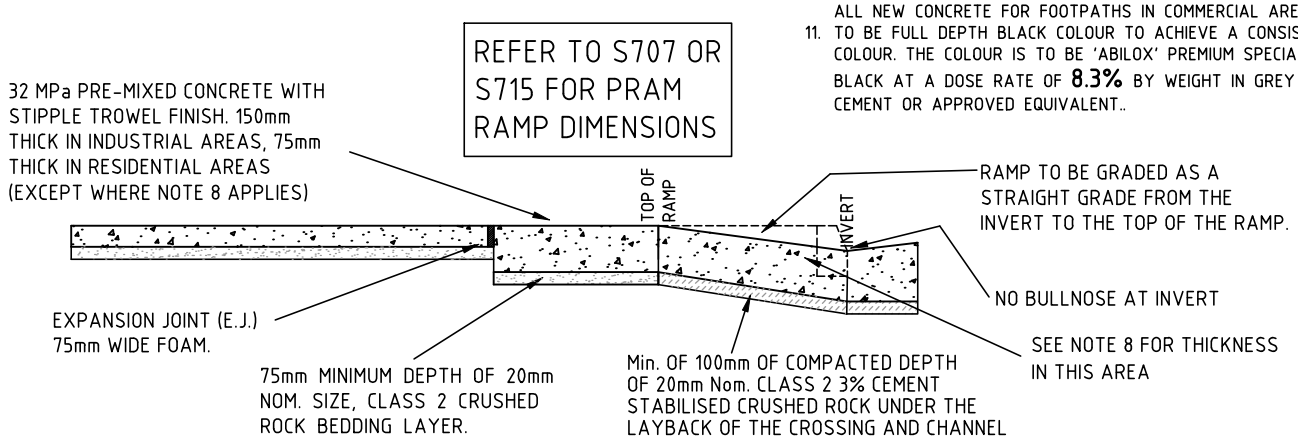
STANDARD DRAWING ISSUE
CURRENT AT TIME OF
CONSTRUCTION MUST BE USED



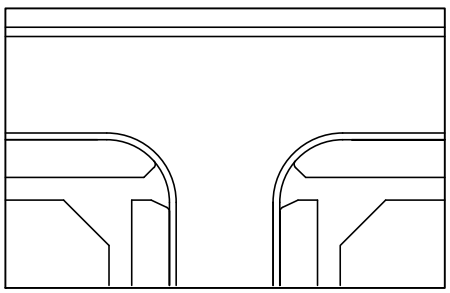
PLAN

NOTES:

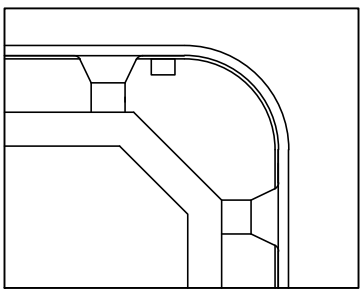
1. PRAM CROSSINGS AT 'T' INTERSECTIONS SHALL BE CONSTRUCTED AS PER DETAIL 1, ALL OTHER INFORMATION REMAINS THE SAME.
2. REFER TO DIMENSION 'W' ON THE PLAN, WHERE:
   'W' > 1m, AREA BETWEEN CROSSINGS SHALL BE GRASSED.
   1.0m >'W' >0.3m, AREA BETWEEN CROSSINGS SHALL BE CONCRETED. FULL HEIGHT KERB MUST BE MAINTAINED BETWEEN CROSSINGS.
   'W' <0.3m, REFER TO DETAIL 2 & CONTACT COUNCIL FOR APPROVED LOCATION.
3. THE SITE SHALL BE MAINTAINED SAFE AT ALL TIMES IN ACCORDANCE WITH A.S.1742.3
4. ALL PRAM CROSSINGS SHALL ALIGN WITH OPPOSING CROSSINGS & GAPS THROUGH CENTRAL MEDIANS.
5. KERB & CHANNEL SHALL BE REMOVED BETWEEN EXISTING JOINTS. WHERE 'W' IS < 1.2m THIS SECTION OF KERB & CHANNEL SHALL BE REPLACED AT THE SAME TIME.
6. ALL INVERTS SHALL BE CONSTRUCTED SO THAT NO WATER PONDS IN THE CHANNEL.
7. EXPANSION JOINTS TO BE 10mm WIDE & FILLED WITH CORK OR BITUMINOUS IMPREGNATED PARTICLEBOARD FOR FULL WIDTH & DEPTH OF EDGE.
8. ALL PAVING WITHIN 1200mm OF THE BACK OF KERB TO HAVE A THICKNESS OF 150mm.
9. ALL CROSSINGS SHOULD COMPLY WITH STANDARD DRAWINGS S701 TO S710 WHICH INCORPORATE COUNCIL'S INTERPETATION OF 'AS1428.4 : 2002 DESIGN FOR ACCESS AND MOBILITY – TACTILE INDICATORS' WITH SOME VARIATIONS.
10. ALL NEW CONCRETE FOR FOOTPATHS IN RESIDENTIAL AND INDUSTRIAL AREAS TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **2.0%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT.
11. ALL NEW CONCRETE FOR FOOTPATHS IN COMMERCIAL AREAS TO BE FULL DEPTH BLACK COLOUR TO ACHIEVE A CONSISTENT COLOUR. THE COLOUR IS TO BE 'ABILOX' PREMIUM SPECIAL BLACK AT A DOSE RATE OF **8.3%** BY WEIGHT IN GREY CEMENT OR APPROVED EQUIVALENT..



SECTION A – A



DETAIL 1



DETAIL 2

WHERE 'W' IS LESS THAN 0.3m, CROSSINGS CANNOT BE CONSTRUCTED IN LINE WITH THE FOOTPATH AS SHOWN ABOVE. CROSSINGS TO BE OFFSET SIMILAR TO DETAIL 2. CONTACT COUNCIL FOR APPROVED LOCATION.

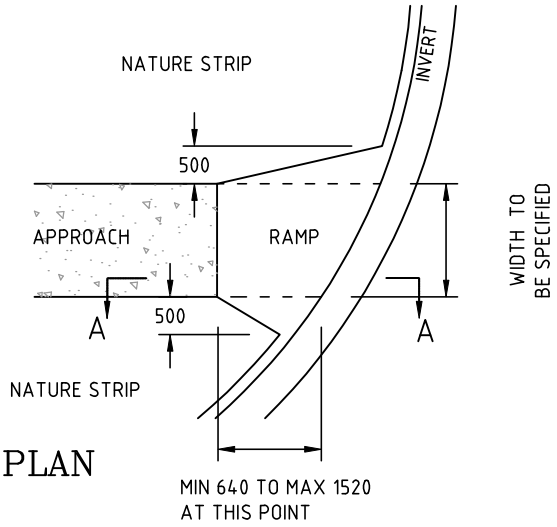
KINGSTON CITY COUNCIL
STANDARD DRAWING

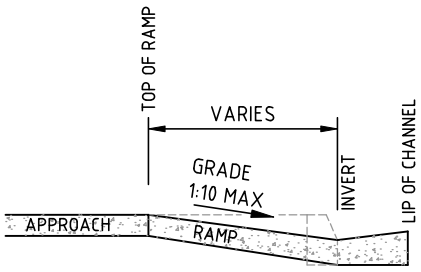
PRAM CROSSING LAYOUT

DRG. NO. S706

ISSUE DATE: 19/12/25

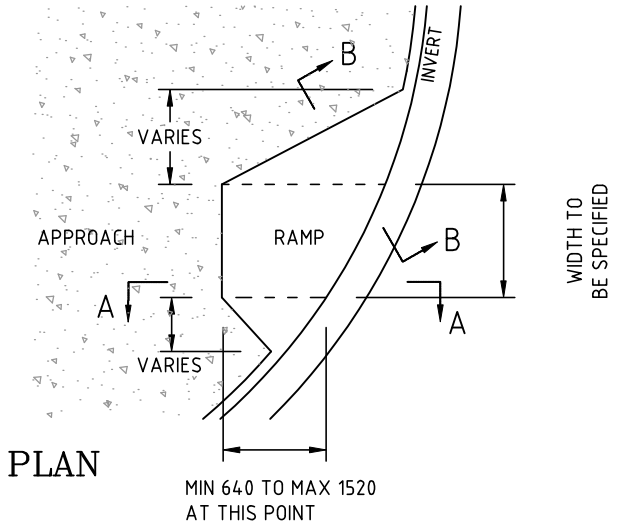
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

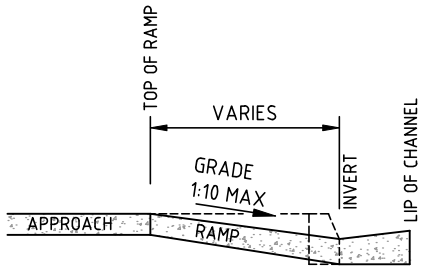




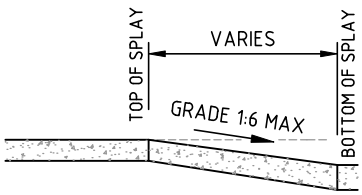
SECTION A – A

PRAM CROSSINGS BORDERING NATURE STRIP





SECTION A – A



SECTION B – B

PRAM CROSSINGS IN FULLY PAVED AREAS

LEGEND

SHARP CHANGE IN GRADE

PAVED SURFACE BEHIND PRAM RAMP

REFER TO S706 FOR PRAM CROSSING LOCATION AND CONSTRUCTION PARAMETERS

KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S707

ISSUE DATE: 19/12/25

PRAM RAMP DIMENSIONS
FOR PRAM CROSSINGS WITH TACTILE INDICATORS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

KINGSTON CITY COUNCIL

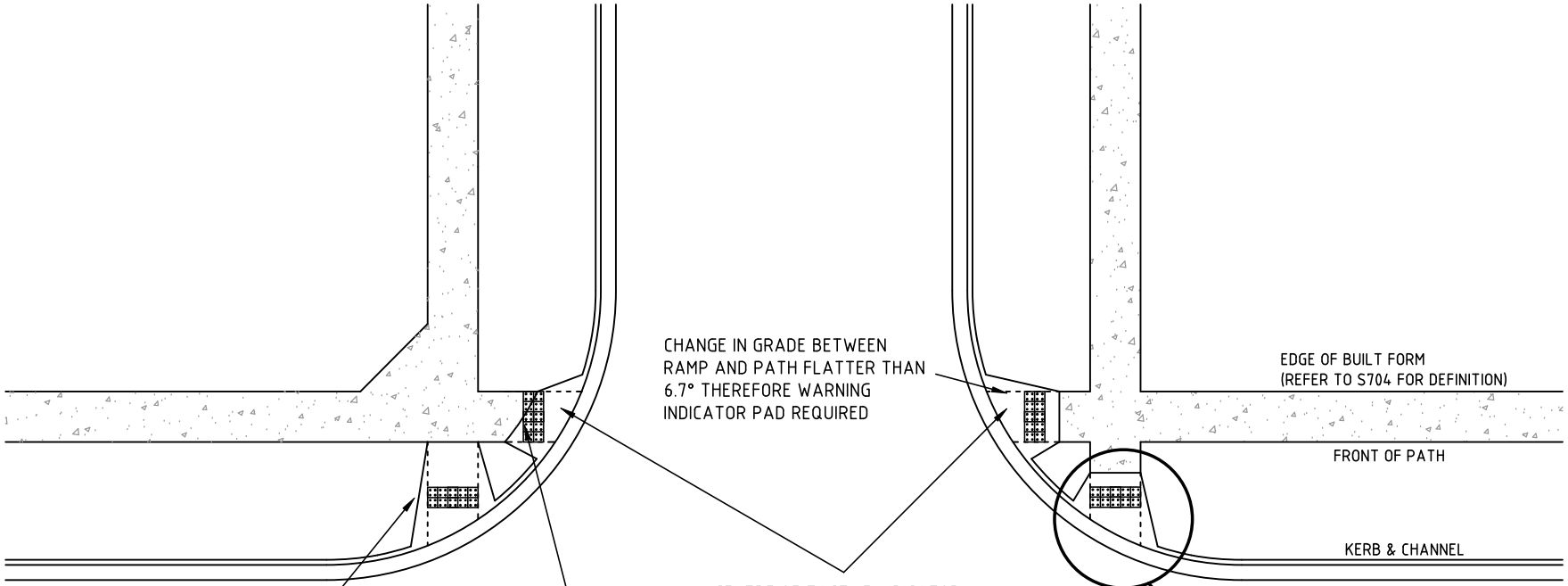
STANDARD DRAWING

TACTILE INDICATOR LAYOUT
FOR FLAT/ANGLED/LONG PRAM RAMP

DRG. NO. S708

ISSUE DATE: 19/12/25

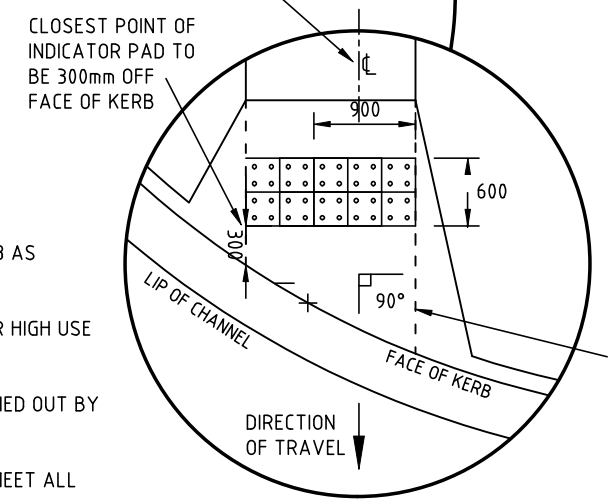
STANDARD DRAWING ISSUE
CURRENT AT TIME OF
CONSTRUCTION MUST BE USED



LEGEND

FOOTPATH

WARNING TACTILE INDICATOR PAD MADE UP OF 5 TILES x 2 TILES. REFER TO STD DRG S702 FOR DEFINITION.



NOTES

THE WARNING TACTILE INDICATOR LAYOUT SHOWN ON THIS PLAN CAN ONLY BE USED IF CRITERIA 1 & 3 AS OUTLINED ON STD DRG S705 HAVE BEEN COMPLIED WITH.

NOTE - THE WIDTH OF PRAM CROSSINGS IS 1.5m IN ALL AREAS EXCEPT SHOPPING CENTRES AND OTHER HIGH USE AREAS.

THESE GUIDELINES DO NOT OVERRIDE CONCLUSIONS CONTAINED WITHIN AN ACCESSIBILITY AUDIT CARRIED OUT BY A QUALIFIED ACCESSIBILITY CONSULTANT.

THESE DRAWINGS CANNOT TAKE INTO ACCOUNT ALL LOCAL CONDITIONS AND MUST BE ADJUSTED TO MEET ALL APPROPRIATE STANDARDS.

NOTES

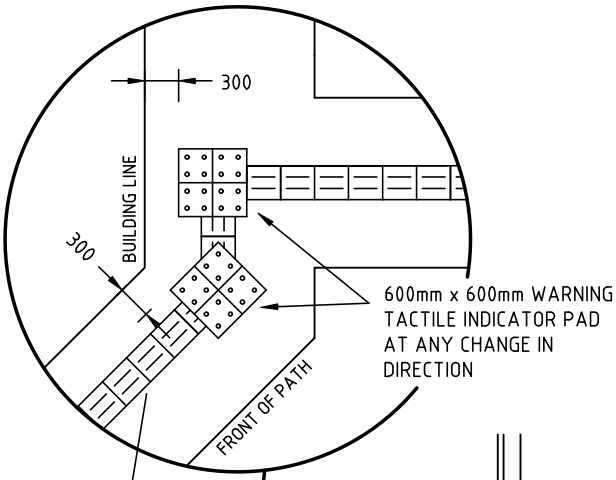
THE TACTILE INDICATOR LAYOUT SHOWN ON THIS PLAN ARE EXAMPLES THAT CAN BE USED IF CRITERIA 1 AS OUTLINED ON STD DRG S705 IS NOT COMPLIED WITH.

COUNCIL'S INTERPRETATION OF AS 1428.4 :2002 IS THAT WARNING TACTILE INDICATORS ON THE PRAM RAMPS ARE ONLY REQUIRED IF CRITERIA 2, 4 OR 5 ARE NOT SATISFIED FOR THAT RAMP.

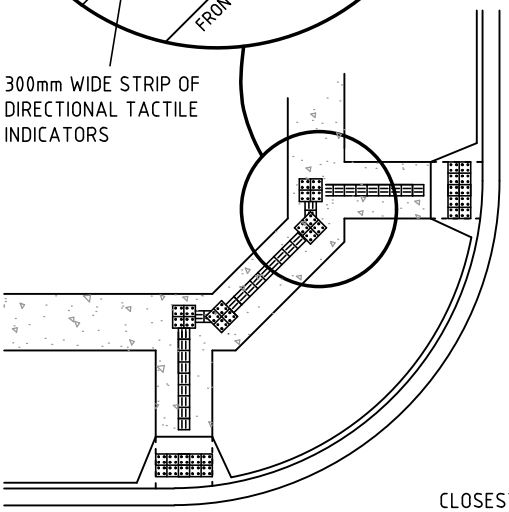
NOTE - THE WIDTH OF PRAM CROSSINGS IS 1.5m IN ALL AREAS EXCEPT SHOPPING CENTRES AND OTHER HIGH USE AREAS.

THESE GUIDELINES DO NOT OVERRIDE CONCLUSIONS CONTAINED WITHIN AN ACCESSIBILITY AUDIT CARRIED OUT BY A QUALIFIED ACCESSIBILITY CONSULTANT.

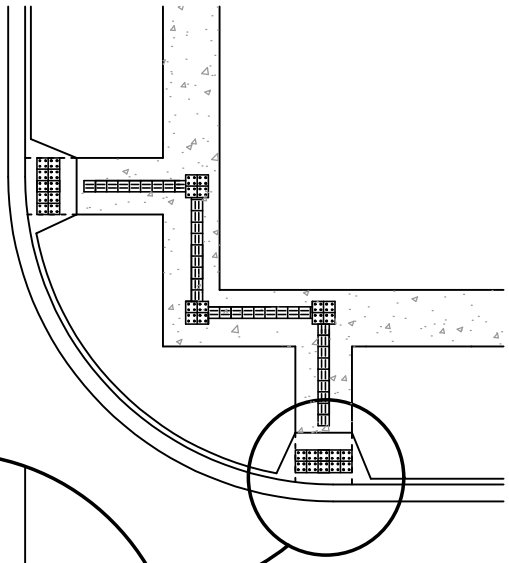
THESE DRAWINGS CANNOT TAKE INTO ACCOUNT ALL LOCAL CONDITIONS AND MUST BE ADJUSTED TO MEET ALL APPROPRIATE STANDARDS.



300mm WIDE STRIP OF DIRECTIONAL TACTILE INDICATORS

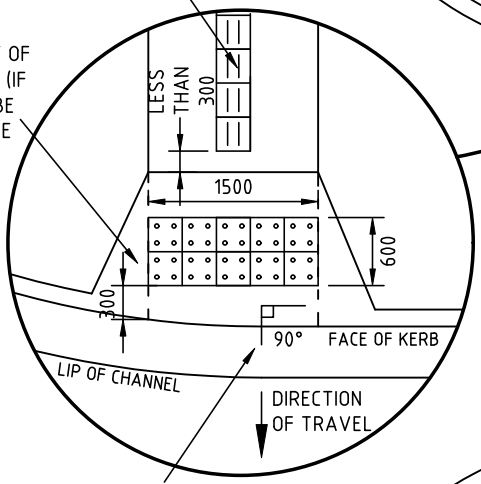


CENTRE OF DIRECTIONAL TACTILE INDICATOR STRIP TO LINE UP WITH CENTRE OF WARNING TACTILE INDICATOR PAD (TYPICAL)



300mm WIDE STRIP OF DIRECTIONAL TACTILE INDICATORS TO STOP WITHIN 300mm OF TOP OF PRAM RAMP

CLOSEST POINT OF INDICATOR PAD (IF REQUIRED) TO BE 300mm OFF FACE OF KERB



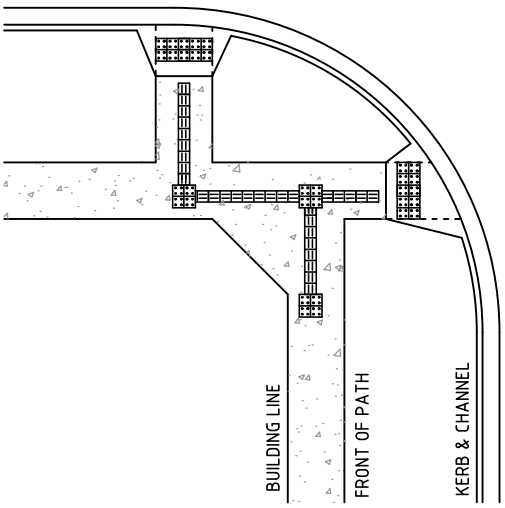
WARNING TACTILE INDICATOR PAD (IF REQUIRED) TO BE PLACED AT 90° TO THE DIRECTION OF TRAVEL

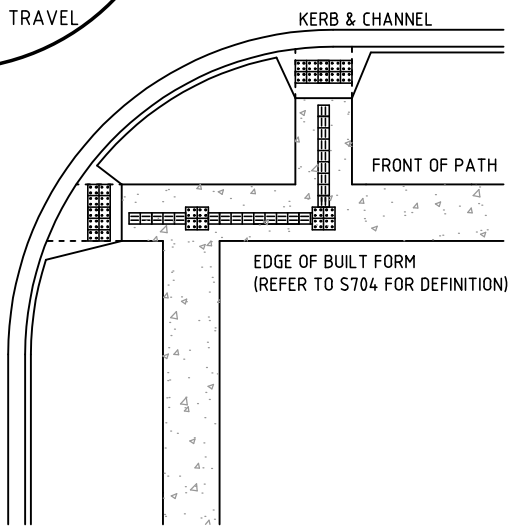
LEGEND

FOOTPATH

WARNING TACTILE INDICATOR PADS MADE UP OF 2 TILES x 2 TILES OR 5 TILES x 2 TILES. REFER TO STD DRG S702 FOR DEFINITION.

300mm WIDE STRIP OF DIRECTIONAL TACTILE INDICATORS (ie. 1 TILE WIDE). REFER TO STD DRG S703 FOR DEFINITION





KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S709

ISSUE DATE: 19/12/25

EXAMPLE TACTILE INDICATOR LAYOUT IF CRITERIA 1 IS NOT COMPLIED WITH

(CRITERIA 1 - PATH OF TRAVEL TO BE WITHIN PRAM CROSSING)

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

NOTES

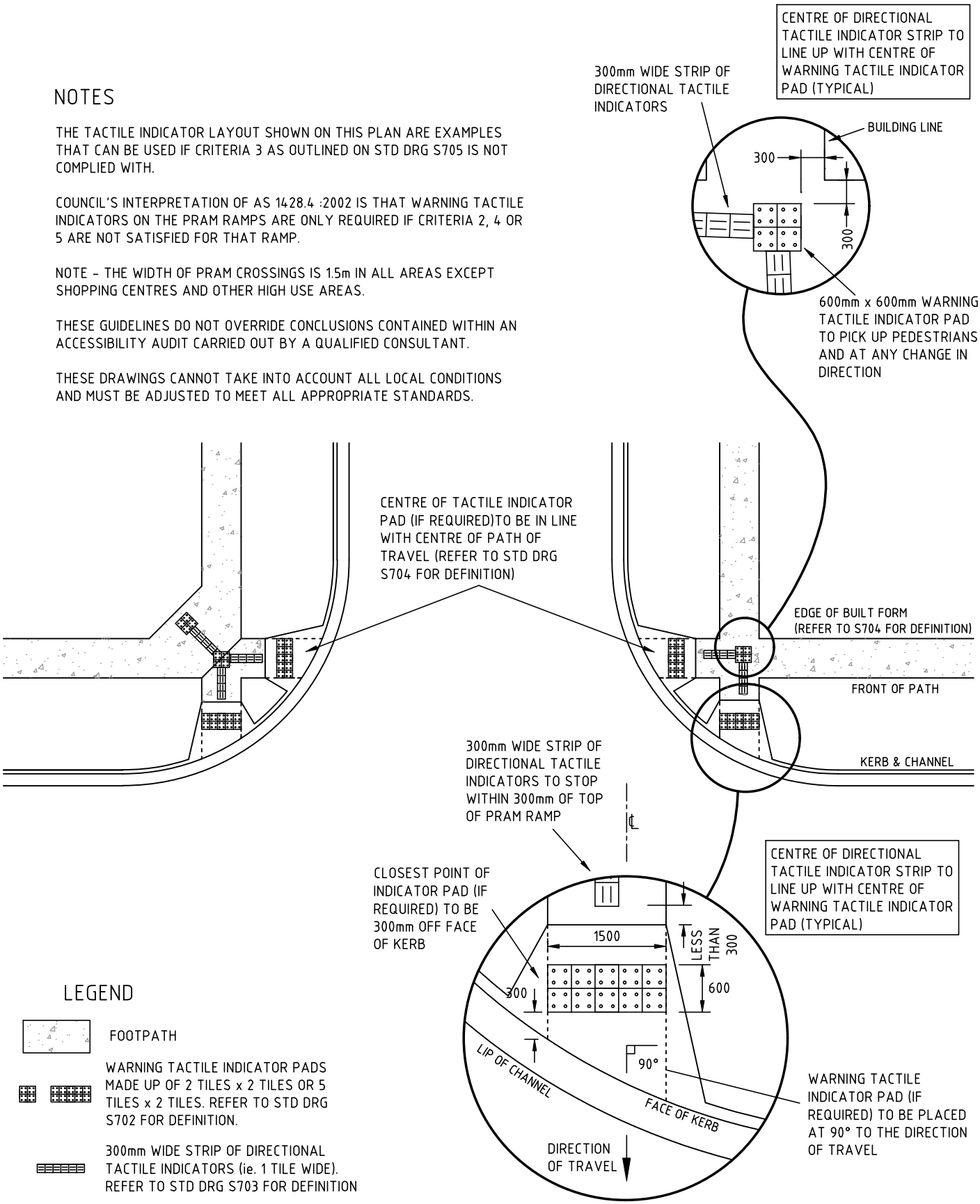
THE TACTILE INDICATOR LAYOUT SHOWN ON THIS PLAN ARE EXAMPLES THAT CAN BE USED IF CRITERIA 3 AS OUTLINED ON STD DRG S705 IS NOT COMPLIED WITH.

COUNCIL'S INTERPRETATION OF AS 1428.4 :2002 IS THAT WARNING TACTILE INDICATORS ON THE PRAM RAMPS ARE ONLY REQUIRED IF CRITERIA 2, 4 OR 5 ARE NOT SATISFIED FOR THAT RAMP.

NOTE - THE WIDTH OF PRAM CROSSINGS IS 1.5m IN ALL AREAS EXCEPT SHOPPING CENTRES AND OTHER HIGH USE AREAS.

THESE GUIDELINES DO NOT OVERRIDE CONCLUSIONS CONTAINED WITHIN AN ACCESSIBILITY AUDIT CARRIED OUT BY A QUALIFIED CONSULTANT.

THESE DRAWINGS CANNOT TAKE INTO ACCOUNT ALL LOCAL CONDITIONS AND MUST BE ADJUSTED TO MEET ALL APPROPRIATE STANDARDS.



LEGEND

FOOTPATH

WARNING TACTILE INDICATOR PADS MADE UP OF 2 TILES x 2 TILES OR 5 TILES x 2 TILES. REFER TO STD DRG S702 FOR DEFINITION.

300mm WIDE STRIP OF DIRECTIONAL TACTILE INDICATORS (ie. 1 TILE WIDE). REFER TO STD DRG S703 FOR DEFINITION

KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S710

ISSUE DATE: 19/12/25

EXAMPLE TACTILE INDICATOR LAYOUT IF CRITERIA 3 IS NOT COMPLIED WITH

(CRITERIA 3 - RAMP OR NATURE STRIP [AS REQUIRED] NOT WITHIN 3m OF BUILDING LINE)

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

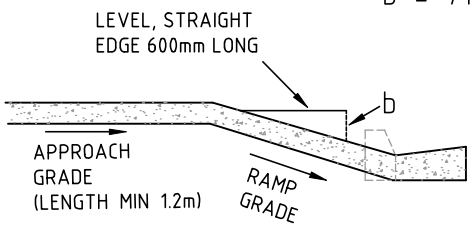
TABLES AND NOTES ON THIS PAGE ARE BASED ON COUNCIL'S INTERPRETATION OF AS1428.4.
IF WARNING TACTILE INDICATORS ON A PRAM RAMP ARE NOT USED THEN THIS STANDARD REQUIRES THAT A CHANGE IN GRADE BETWEEN THE PRAM RAMP AND THE FOOTPATH BEHIND THE PRAM RAMP (OVER A LENGTH OF MIN 1.2m) IS TO BE A MINIMUM OF 6.7° (EQUIVALENT TO 1 IN 8.5)

SETOUT OF PRAM CROSSING CAN ONLY BE CARRIED OUT IF INVERT LEVEL OF CHANNEL AT CENTRE OF PRAM CROSSING IS KNOWN. NOTE THAT THE MAIN DESIGN CRITERIA FOR PRAM CROSSING LAYOUT IS NOW GRADE AND NOT SET LEVEL DIFFERENCES, WITH ONLY A SMALL RANGE OF ALLOWABLE GRADES TO WORK WITH.
TO ACHIEVE THE REQUIRED GRADES AND TO MATCH IN WITH SURROUNDING FOOTPATH, THE SIZE OF INDIVIDUAL PRAM CROSSINGS WILL NEED TO BE VARIED, SUBJECT TO VARIOUS LIMITING FACTORS.

NOTE THAT DUE TO MAXIMUM GRADES ALLOWABLE TO CONFORM WITH WHEELCHAIR ACCESS REQUIREMENTS AND THE REQUIREMENT OF THE VISUALLY IMPAIRED TO HAVE A DISTINCT CHANGE IN GRADE IF TACTILE INDICATORS ARE TO BE AVOIDED, FOOTPATH APPROACHING A PRAM CROSSING CAN NOT HAVE FALL TOWARDS THE PRAM CROSSING IF TACTILE INDICATORS ARE NOT TO BE USED . IE APPROACHING FOOTPATH MUST EITHER HAVE ZERO LONGITUDINAL FALL OR FALL AWAY FROM THE CROSSING.

FLAT PATH APPROACHING RAMP

b = 71mm (+/- 5mm)



HEIGHT 'b' IS TO BE MEASURED IN LINE WITH THE CENTRE OF THE PRAM RAMP.
'b' must be 71mm(+/- 5mm)

FOR MAXIMUM AND MINIMUM RAMP LENGTHS SEE DRAWING S715.

PRAM CROSSING WITH FLAT APPROACHING RAMP

STEP 1 - IF 'b' IS MORE THAN 71mm THE PRAM RAMP NEEDS TO BE RECONSTRUCTED AT A FLATTER GRADE
STEP 2 - IF 'b' IS LESS THAN 71mm TACTILE INDICATORS ARE REQUIRED ON THE PRAM RAMP
STEP 3 - IF APPROACH HAS FALL TOWARDS CHANNEL TACTILE INDICATORS MUST BE USED

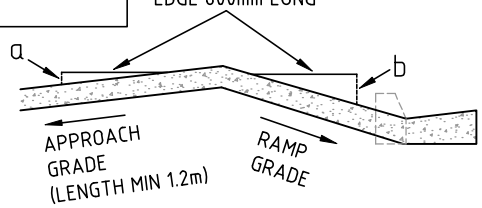
OR

RECONSTRUCT PRAM RAMP AND/OR APPROACH PATH TO COMPLY WITH ABOVE DIMENSIONS

PATH FALLING AWAY FROM RAMP

a + b > 71mm

HEIGHTS 'a' AND 'b' ARE TO BE MEASURED IN LINE WITH THE CENTRE OF THE PRAM RAMP



PRAM CROSSING WITH PATH FALLING AWAY FROM RAMP

STEP 1 - IF 'b' IS 76mm OR MORE THEN THE PRAM RAMP NEEDS TO BE RECONSTRUCTED AT A FLATTER GRADE
STEP 2 - IF TABLES 1 OR 2 ARE NOT COMPLIED WITH THEN TACTILE INDICATORS ARE REQUIRED ON THE PRAM RAMP

OR

RECONSTRUCT PRAM RAMP AND/OR APPROACH PATH TO COMPLY WITH TABLES 1 & 2

TABLE 1 (ALL MEASUREMENTS IN mm)

IF 'b' EQUALS	75	73	71	69	67	65	63	61	59	57	55	53	51	49	47	45	43	41
THEN 'a' MUST BE GREATER THAN OR EQUAL TO	0	0	0	2	4	6	8	10	12	14	16	18	20	22	24	26	28	30

TABLE 2 (ALL MEASUREMENTS IN mm)

IF 'a' EQUALS	0	2	4	6	8	10	12	14	16	18	20	22	24	26	28	30
THEN 'b' MUST BE GREATER THAN OR EQUAL TO	71	69	67	65	63	61	59	57	55	53	51	49	47	45	43	41

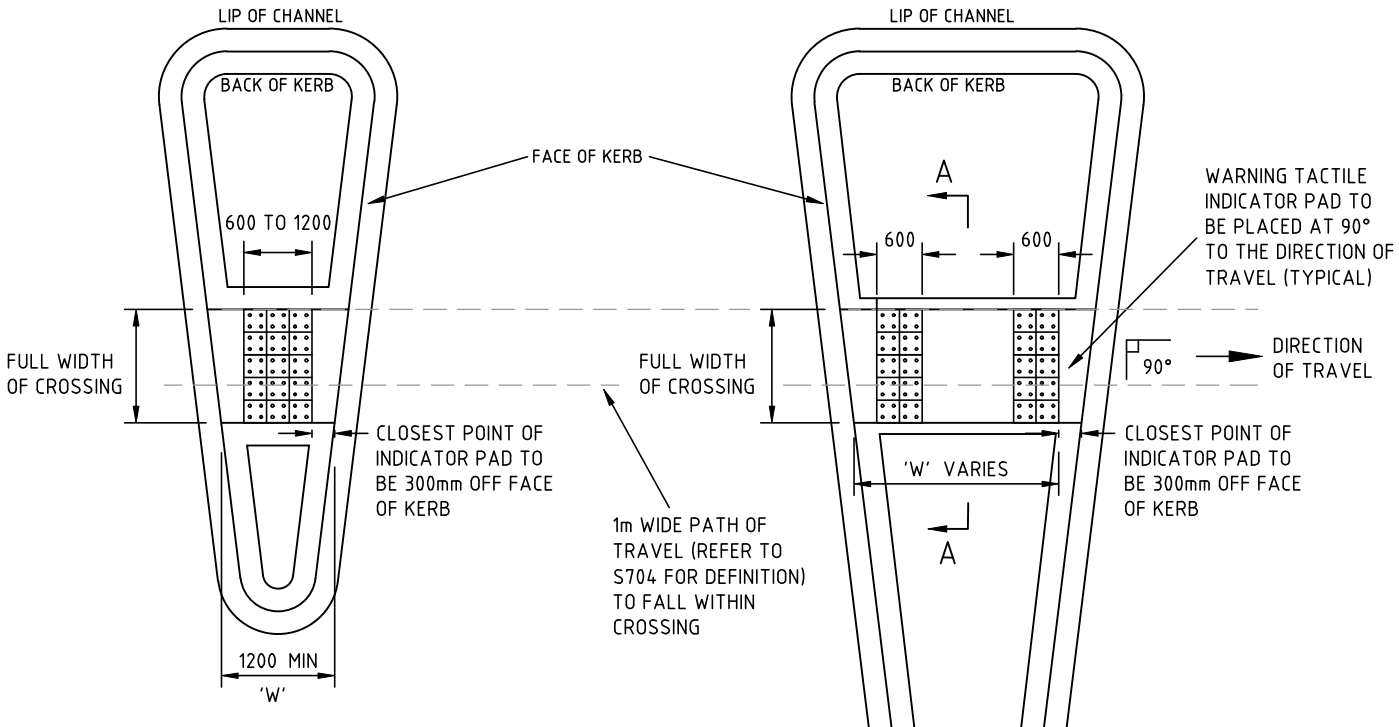
KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S711

ISSUE DATE: 19/12/25

CHANGE OF GRADE BETWEEN APPROACH AND RAMP SO THAT TACTILE INDICATORS ARE NOT REQUIRED

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

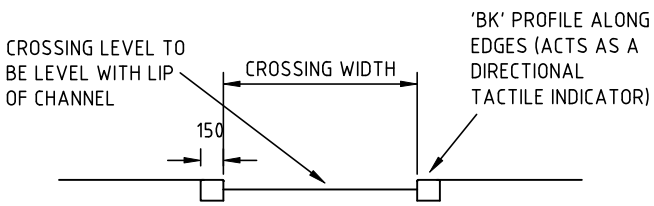


SINGLE INDICATOR PAD
1200 < W < 1800

TWO INDICATOR PADS
W > 1800

LEGEND

WARNING TACTILE INDICATOR PADS. REFER TO STD DRG S702 FOR DEFINITION.



SECTION A – A

NOTES

THE TACTILE INDICATOR LAYOUTS ABOVE ASSUME CROSSINGS ARE CUT THROUGH (REFER TO SECTION A-A). REFER TO VICROADS STD DRG SD2032 FOR AN EXAMPLE OF AN ISLAND WITH PRAM RAMPS.

COUNCIL'S POLICY IS TO PLACE WARNING TACTILES IN ALL SPLITTER ISLANDS (AS1428.4 DOES NOT REQUIRE WARNING TACTILES IN SPLITTER ISLANDS WHERE PEDESTRIANS ARE ONLY CROSSING NARROW ROADS).

NOTE - COUNCIL'S POLICY IS TO LIMIT THE WIDTH OF PRAM CROSSINGS TO 1.5m IN ALL AREAS EXCEPT SHOPPING CENTRES AND OTHER HIGH USE AREAS.

THESE GUIDELINES DO NOT OVERRIDE CONCLUSIONS CONTAINED WITHIN AN ACCESSIBILITY AUDIT CARRIED OUT BY A QUALIFIED ACCESSIBILITY CONSULTANT.

THESE DRAWINGS CANNOT TAKE INTO ACCOUNT ALL LOCAL CONDITIONS AND MUST BE ADJUSTED TO MEET ALL APPROPRIATE STANDARDS.

KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S712

ISSUE DATE: 19/12/25

SPLITTER ISLAND
EXAMPLE TACTILE INDICATOR LAYOUT

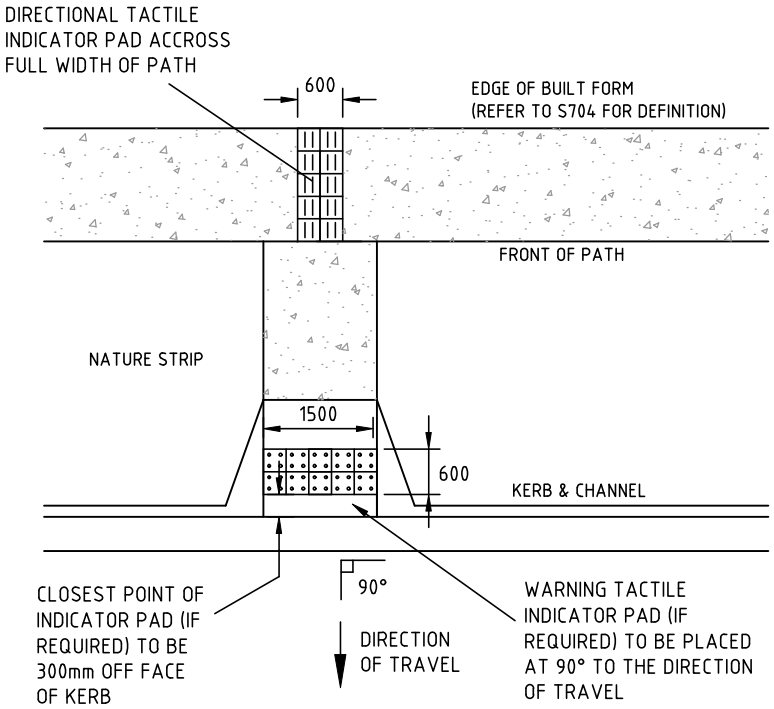
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED

NOTES

COUNCIL'S INTERPRETATION OF AS 1428.4 :2002 IS THAT ALL MID BLOCK CROSSINGS REQUIRE DIRECTIONALTACTILE INDICATORS. WARNING TACTILE INDICATORS ON THE PRAM RAMP ARE ONLY REQUIRED IF CRITERIA 2, 4 OR 5 AS SPECIFIED ON STD DRG S705 ARE NOT SATISFIED.

NOTE – COUNCIL'S POLICY IS TO LIMIT THE WIDTH OF PRAM CROSSINGS TO 1.5m IN ALL AREAS EXCEPT SHOPPING CENTRES AND OTHER HIGH USE AREAS.

CENTRE OF DIRECTIONAL TACTILE INDICATOR STRIP TO LINE UP WITH CENTRE OF WARNING TACTILE INDICATOR PAD (TYPICAL)



WITH NATURE STRIP

LEGEND

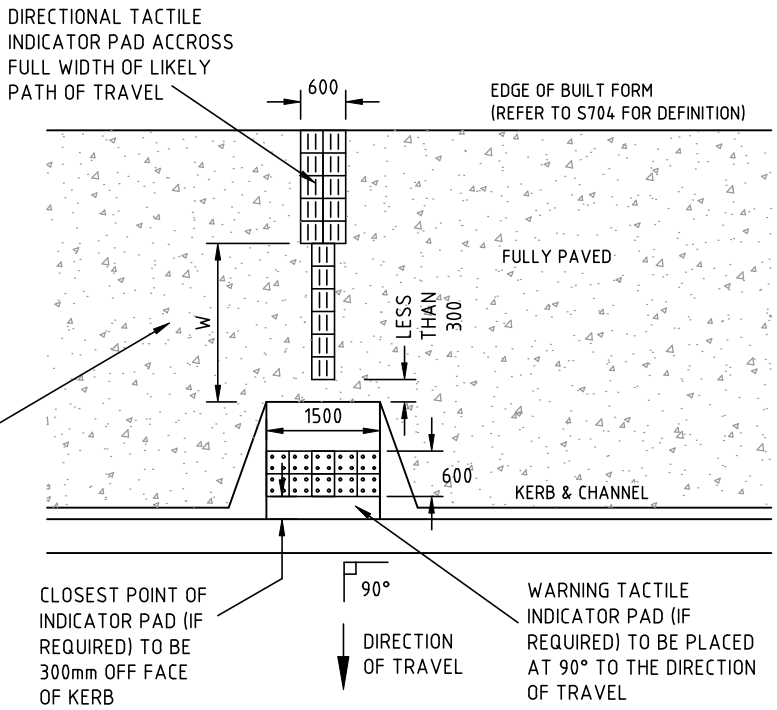
FOOTPATH

WARNING TACTILE INDICATOR PADS MADE UP OF 5 TILES x 2 TILES. REFER TO STD DRG S702 FOR DEFINITION.

300mm WIDE STRIP OF DIRECTIONAL TACTILE INDICATORS (ie. 1 TILE WIDE). REFER TO STD DRG S703 FOR DEFINITION

IF 'W' < 3m THEN TACTILE INDICATORS ARE NOT REQUIRED IN THIS AREA.

IF 'W' > 3m THEN CENTRED 300mm WIDE STRIP OF DIRECTIONAL TACTILE INDICATORS REQUIRED TO WITHIN 300mm OF TOP OF PRAM RAMP.



FULLY PAVED

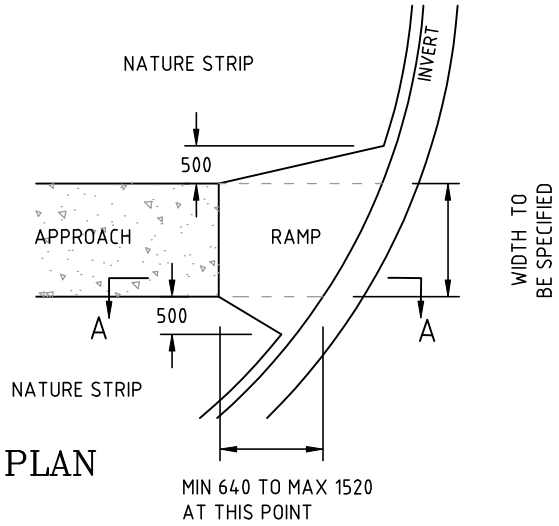
KINGSTON CITY COUNCIL
STANDARD DRAWING

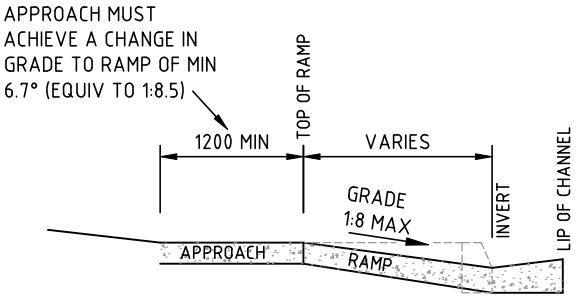
DRG. NO. S713

ISSUE DATE: 19/12/25

MID BLOCK CROSSINGS
TACTILE INDICATOR LAYOUT

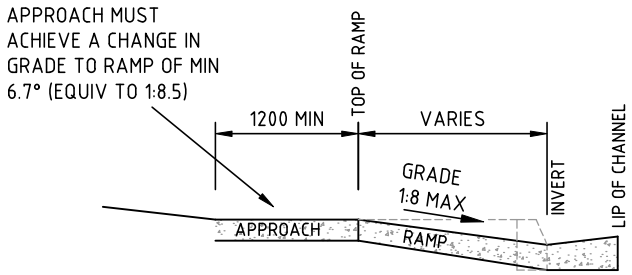
STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED



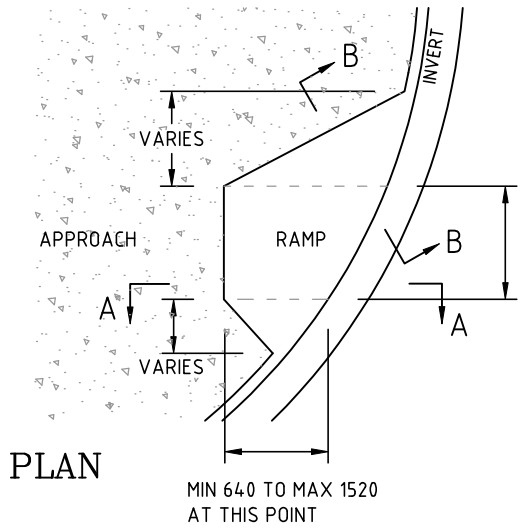


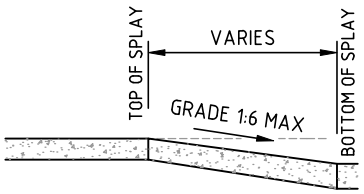
SECTION A – A

PRAM CROSSINGS BORDERING NATURE STRIP



SECTION A – A





SECTION B – B

PRAM CROSSINGS IN FULLY PAVED AREAS

LEGEND

SHARP CHANGE IN GRADE

PAVED SURFACE BEHIND PRAM RAMP

REFER TO S706 FOR PRAM CROSSING LOCATION AND CONSTRUCTION PARAMETERS

KINGSTON CITY COUNCIL
STANDARD DRAWING

DRG. NO. S715

ISSUE DATE: 19/12/25

PRAM RAMP DIMENSIONS
FOR CROSSINGS WITHOUT TACTILE INDICATORS

STANDARD DRAWING ISSUE CURRENT AT TIME OF CONSTRUCTION MUST BE USED