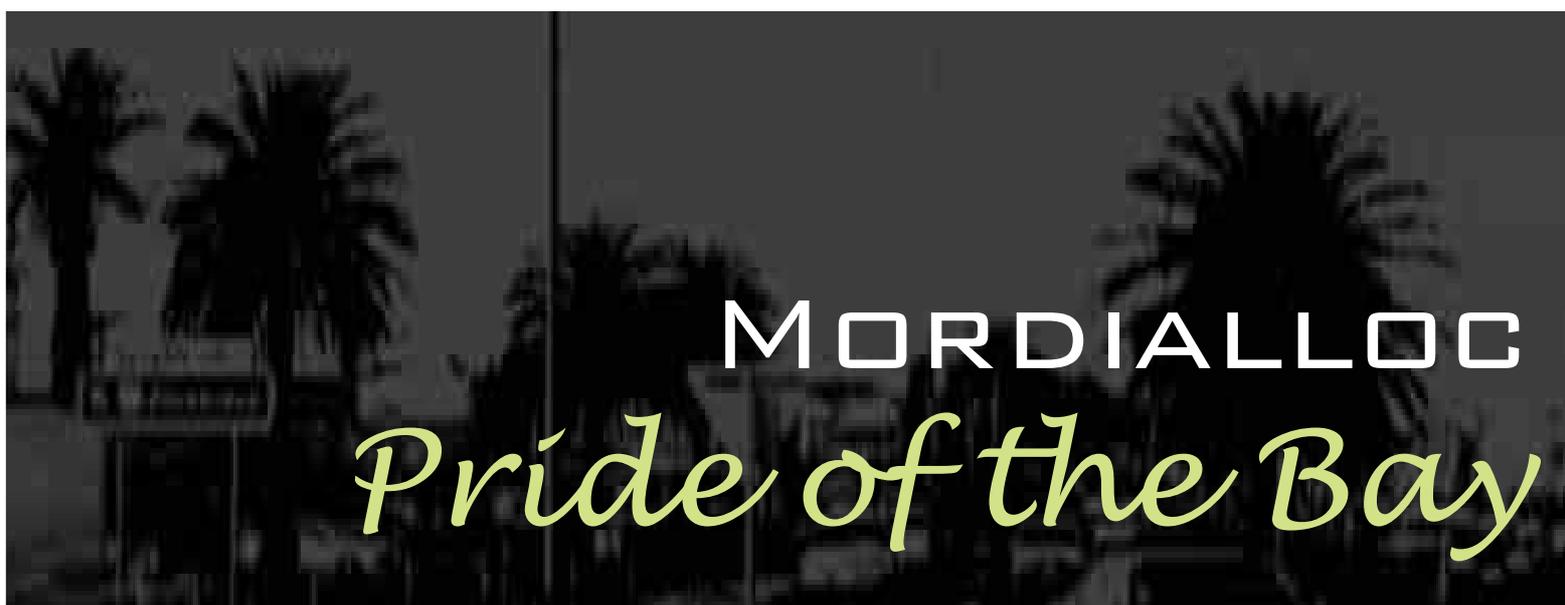




City of  
**KINGSTON**



**MORDIALLOC**  
*Pride of the Bay*

**Nov** 2004



A STRUCTURE PLAN FOR THE FUTURE OF MORDIALLOC

FINAL REPORT





## Acknowledgements

The *Mordialloc: Pride of the Bay* Structure Plan has been principally prepared by the City of Kingston Strategic Planning Department. The structure plan is a collective of contributions and recommendations from a range of groups. Appreciation is shown to:-

- the community, interest groups and key stakeholders, who have been actively involved, have responded to public discussions, provided written submissions, or that have given direction on specific aspects of the plan;
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  - Bryce Raworth Pty Ltd, for components of the '*Kingston Heritage Study*' relevant to the *Pride of the Bay* study area; and
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Council acknowledges work culminating in the Draft *Mordialloc: Pride of the Bay* Structure Plan (March 2001) prepared by Gutteridge Haskins & Davey Pty Ltd in Association with Mark McWha Landscape Architects, Essential Economic, and Andrew O'Brien & Associates.





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## 1.0 Introduction

The purpose of the Mordialloc Structure Plan is to “*guide the future development of the Mordialloc precinct and to provide guidance on its future planning and enhancement*” (Project Brief, 2000). Upon the initial commencement of the study, the aim of the project encompassed:-

- a detailed analysis of the study area;
- the creation of development options and their implications;
- development and review of possible implementation mechanisms;
- preparation of a land use framework; and
- an implementation plan.

Building on the initial release of the *Draft Mordialloc : Pride of the Bay* document exhibited for public comment during March 2002, the *Final Mordialloc: Pride of the Bay* (Nov 2004) plan provides a holistic framework for the future of Mordialloc. The Mordialloc : *Pride of the Bay* Structure Plan now incorporates the analysis, recommendations and findings of a series of ongoing studies and reviews that followed the outcomes of the public exhibition of the *Draft* documents. The structure plan further responds to community and key stakeholder consultations, in particular the need to address issues relating to heritage, built form and height attributes of the Centre.

Subsequently, the *Pride of the Bay* strategy is based on comprehensive studies of existing urban structure, form and function, and pressures for change. These include:-

- understanding the historic development of the centre and identified cultural values;
- the identification and enhancement of established characteristics which make Mordialloc a highly attractive and unique coastal village centre;
- established landscapes, physical form and existing building height;
- the established local economy and identified opportunities and needs for future retail & commercial growth;
- recreational use, including the existence of substantial boating activities;
- transport and access;
- demographic change; and
- future housing needs.

The documents seeks to sensitively balance within a realistic response these drivers for change, including the State Government’s planning policy context.

## Purpose

The *Pride of the Bay* Structure Plan sets a *vision* for the future of Mordialloc over the next 20 years. It establishes a sustainable long term future role for Mordialloc directed towards preserving what is highly valued and unique within the centre, whilst capitalising on identified opportunities to achieve wider community benefit.

The Structure Plan importantly outlines a *holistic* framework for future development and works in both the public and private realm. This will ensure future *public and private sector investment is coordinated*, and will overtime, achieve ongoing improvement to the liveability, amenity and function of this evolving coastal village.

More specifically, the ‘Mordialloc: Pride of the Bay’ Structure Plan;

- identifies a preferred role and vision for the study area;
- directs pedestrian, building form, land use and heritage through a series of frameworks, to guide future development and activities;
- identifies key opportunity sites for development, that will contribute to the liveability and commercial function of the activity centre;
- identifies projects and works that support the implementation of a vision for the study area, whilst preserving valued characteristics of the area; and
- recommends guidelines and policies to ensure future development reflects, community aspirations, valued character whilst encouraging high quality design and development within the centre, to achieve stated visions.

Mordialloc has been identified as a highly valued and unique low rise centre. Main Street in particular is unique, comprising an extensive road reservation in which median palm planting dominate views along the street. Buildings along Main Street do not exceed the historic federation and Italianate two storey building heights, which range between 7.2 and 11.2 metres in height from the street level. The ‘Mordialloc: Pride of the Bay’ Structure Plan therefore seeks to retain the low rise form of development within Mordialloc, whilst encouraging responsive and sensitively designed infill development.

*Building Heights Main Street (East)*  
Historic double storey buildings along Main Street contain building heights up to approximately 11 metres, the same height as a contemporary 3 storey building.

Approximate building Heights based on calculated estimates. Photographs & lines not to scale.



## Defining the Structure Plan Area

The Pride of the Bay Study Area (refer Figure 1) is defined to the:-

- *west* by the foreshore area and the Mordialloc Pier;
- *south* by Parana Street / Nepean Highway / Station Street / Mordialloc College / Browns Reserve;
- *east and north* by Wells Road / J Grut Reserve / Governor Road (incorporates the residential areas immediately adjoining Governor Road to the north) / Park Street / Bear Street / Barkley Street / McDonald Street / Albert Street / Nepean Highway / High Street / Montgomery Street / Owen Street.

The Study Area, is based:-

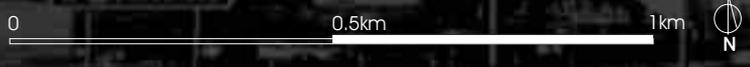
- on defining the activity centre and its core commercial functions;
- ensuring the inclusion of significant redevelopment sites within or adjacent to the centre;
- the inclusion of water based activities and industries proximate to the activity centre; and
- including land and environmental attributes which contribute to the unique character of the Mordialloc Activity Centre.



-  STUDY AREA
-  PARKLAND & OPEN SPACE
-  EXISTING PALM
-  BUILDING FOOTPRINT
-  RAILWAY LINE & RESERVATION



FIGURE 1  
**STUDY AREA**  
**MORDIALLOC STRUCTURE PLAN**

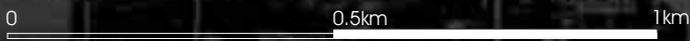


*Pride of the Bay*



FIGURE 2

**REGIONAL CONTEXT**  
**MORDIALLOC STRUCTURE PLAN**



*Pride of the Bay*



## Mordialloc Today

Mordialloc is a coastal icon for metropolitan Melbourne. It is a village surrounded by urban Melbourne, where generations have visited, lived, run businesses or played throughout the years.

The centre is located approximately 25 kilometres south-east of the Melbourne central activities district, on the central eastern edge of Port Phillip Bay. It is the first bay side activity centre on the Nepean Highway approach from Melbourne on Port Phillip Bay. The centre is proximate to Moorabbin Airport, a range of golf and recreation reserves, and industries that contribute to the wider metropolitan and national economies.



Located on the Frankston railway line, Mordialloc provides a point of interchange, with rail, bus and taxi services. Mordialloc forms the junction between the Nepean Highway and Beach Road, two significant arterials linking Mordialloc to the Melbourne central activities district. Increasingly, north-south bound traffic is being accommodated along the Boundary and Wells Road thoroughfares, east of the Study Area.



Mordialloc's location at the nexus of the railway line, the mouth of Mordialloc Creek and Port Phillip Bay, attributes much of the centre's historic development: heritage buildings are reminiscent of Mordialloc's historic growth as an early seaside fishing village, holiday destination and transport hub.

## Historic Growth & Development<sup>1</sup>

Mordialloc's diverse history is based upon incremental development of the centre and the wider district around 'exploration... squatting... the fishing village... [and] the market gardening'<sup>2</sup>.

Squatting commenced in Mordialloc during the 1840's, before which the area was inhabited by the Bunurong Aboriginal tribe. The presence of the Bunurong is acknowledged within the indigenous gardens. During the late 1800's the fishing village, that had developed was noted as a location for excursions and holidaying, renowned for the quality of the creek, the fish and evident fauna.

The railway connection to Caulfield during the 1880's transformed the rural settlement to a 'township', with Main Street comprising 'thirty-six shops along an unmade road' during this period. The centre's role as a summer time destination further intensified during the early 1900's. Indications of this time are contained within the number of architecturally significant structures within the study area including the former Pasadena Dance Hall, the pier, sea wall, band rotunda. Many of the public structures were raised by funds from the Mordialloc Carnival which took place where the Hazel Pierce Reserve is positioned today.

During the 1900's a number of the substantial two storey, masonry buildings were constructed, as were the landscape road medians. By the 1940's Main Street had become formal, more commonly characterised with tall brick buildings, and wide verandas to the street. However, it was the formal landscaping during this period that reinforced the north-south axis of Main Street. Elements within the public realm included paving, palm planting, the memorials and artillery pieces within medians established the current iconic appearance of the centre, and the two storey character that now forms the busy Main Street thoroughfare that we know today.



1930

Structures for funds within the foreshore reserve were frequently raised by the Mordialloc Fair. (Courtesy of the Mordialloc & District Historical Society)



1965

Pompei's Boat Launch (Courtesy of the Leader Collection)



1918

Main Street West, north of the Intersection of Beach Road. Double Storey, former hospital now site's Safeways.

(Courtesy of the Mordialloc & District Historical Society)

<sup>1</sup> Extracts from the 'City of Kingston Heritage Study: Place Identification Form - Main Street Mordialloc' (2001) Bryce Raworth Pty Ltd.

<sup>2</sup> McGuire, F (1985) 'Mordialloc: The Early Days' Argyle Press Pty Ltd, Chelsea.

## Physical Form

An appreciation of the urban design and physical form qualities of Mordialloc may be gained through an examination of existing urban design structure of the village, the visual experience of the Centre and its coastal context and the inherent sense of place of the primary Main Street streetscape.

### Urban Design Structure.

Mordialloc's urban design structure is most clearly defined by the strong linear quality of Main Street (Nepean Highway) between McDonald Street (north) and the Mordialloc Creek bridge (south), extending from a minor rise to the north down to the relative flat of the Creek bed and the intersection with the broad arc of Beach Road, which hugs the coastline to the north.

The linear structure of Mordialloc is reinforced by the relatively uniform single and double storey traditional street wall and the avenue of Canary Island Palms lining the generous 60m road reserve. The street wall is only broken to the east and west at intersections with perpendicular roadways, providing access to activities behind the strong retail skin. The nature of local topography, sloping down towards the Mordialloc Creek accentuates the low scale of the village, with fine grained forms sensitively stepped down the streetscape to the south.



Mordialloc Urban Design Structure

The form and structure of Mordialloc is also representative of a shift in the character and image of Nepean Highway as it passes from a contemporary highway format to the north, through the Main Street neck and beyond across the Mordialloc Creek as a confined four lane road. At Mordialloc, the Highway does not simply pass through the precinct (as experienced to the north or south). Together with the open service lanes and angled car parking, the Highway corridor characterises a uniquely open village format that is somewhat reminiscent of rural townships.

Mordialloc's core is confined by the limits of the railway line to the east, the Mordialloc Creek corridor to the south and the Bay to the west. Unlike other Activity Centres in Kingston and elsewhere, Mordialloc presents as a relatively contained setting, demanding sensitive infill consolidation along the Main Street corridor and opportunity for greater densities on key opportunity sites behind the Main Street spine and along the Railway line. There are significant attributes to the east of the Rail Line, including the Mordialloc Primary School, the Masonic Temple and related community services in association with a belt of redundant industrial land.

#### *Visual Framework.*

The visual experience of Mordialloc is not defined by feature buildings, landmarks or icons. Rather the sense of place in Mordialloc is communicated (to drivers, cyclists or pedestrians) as a relatively consistent collection of modest early 20<sup>th</sup> century buildings balanced with the strong Palm landscape (and associated clock tower and memorials). Within the visual threshold of the village, there are few individual buildings that stand out (with the exception of Safeway and other large format stores). It is the total streetscape form that is noticeable from many vantage points.

The visual setting in Mordialloc is also influenced by corridor views along Nepean Highway to the north and south. The important view to the south towards Mordialloc Creek from the Highway high ground (near the McDonald Street roundabout) reveals the open landscape of the Creek corridor and the bold Cypress landscape of the Attenborough Park beyond. The scale and format of buildings to the southern end of the streetscape do not presently interrupt this important aspect, which serves as a termination of the scenic experience of the Highway to the north.

Other view lines in Mordialloc are to the north along the Highway from south of Mordialloc Creek, which focus on the east side of Main Street. The broad arc in Beach Road (as it intersects with Nepean Highway) also presents exposed views to the stepped Main Street east elevation. Narrow views are available along east-west streets including Centreway, Owen Street, Bear and McDonald Street and open panoramic views to the Bay are available from Beach Road.



### *Traditional Streetscape Quality.*

The quantity, quality and distribution of traditional building forms in Main Street, Mordialloc is one of the precinct's primary attractions and one of the key factors contributing to the street's significance as a traditional village setting. Both sides of Main Street present examples of one and two storeys contributory forms with consistent glazed ground level frontages, cantilevered awnings and solid upper levels with regular fenestration and parapet form. The integrity of the streetscape and the extent of contributory buildings has been documented in the recent Heritage Study (Stage 2) and show a broad distribution of complementary streetscape buildings. Particular clusters of intact streetscape form occur in particular around Centreway and Mordialloc Station.

A series of large late 20<sup>th</sup> century forms on the western side of Main Street have in part eroded the integrity and flavour of Mordialloc village. The Safeway building in particular, which extends to a scale equivalent to three storeys in height, has had a significant negative impact on streetscape quality and image. While its scale is generally greater than that of the prevailing streetscape, the broad blank upper elevation demonstrates a conscious break from the traditional fine grained and articulate streetscape formats. This also occurs further to the north, strongly reinforcing the importance of sensitive interconnection and relationship with adjoining streetscape forms.

In recognising the valued traditional scale of Main Street and the sensitivity of the village fabric in general, opportunities exist for consolidated form in discreet locations beyond the Main Street spine. In particular this may be achievable, on excess Rail Land or on redundant industrial land, subject to visual and amenity impact.



## Functional Role & Context

Mordialloc is a designated 'Major' activity centre<sup>3</sup>, servicing a primary trade catchment of 12,580<sup>4</sup> people. Within a radius of 2 – 3 kilometres of the centre, it is estimated there are approximately 28,430 residents<sup>5</sup>.

Mordialloc is within the Kingston North Statistical Local Area (SLA), which accounts for approximately two-thirds of the total municipal population. Over the next 10 years this SLA is forecast to increase by approximately 8,570 residents<sup>6</sup>. Within the suburb of Mordialloc, the population is expected to reach approximately 7,300 people by 2011, a nett increase of around 1,700 persons from 2001.

Recent residential development within Mordialloc's wider catchment area recently constructed, or under construction includes:-

- Epsom Park : Approximately 420 lots
- Waterways : 700 lots
- Royal Palms : Approximately 350 lots

Although not all within the primary catchment area, it is anticipated these lots will result in a nett addition of approximately 3,200 residents living proximate to the Mordialloc activity centre.

### Retail

Issues affecting the catchment of the Mordialloc activity centre include:-

- competing activity centres within proximity of Mordialloc (particularly Southland, Frankston, Mentone and Aspendale Gardens);
- the bayside location of the centre draws an immediate boundary to the catchment area to the west; and
- constrained access (points of congestion and intersections) that limits the convenience of the core commercial areas, particularly to those arriving from the east.

The centre comprises a destination for weekly shopping and household needs, although access limitations are increasingly impacting on the ability of Mordialloc to perform this role. Subsequently disinvestment, into buildings and tenancies is not uncommon. Mordialloc's retail range includes a supermarket, discount retail store, and series of core retail activities, which comprise a total retail floor space<sup>7</sup> estimated at 17,300 m<sup>2</sup>.

#### **Explanatory Note**

##### Retail Access, Expenditure & Investment

*The convenience of access to a retail centre is a key determinant of escape expenditure (in the context of Mordialloc to proximate centres including Aspendale Gardens, Mentone, Southland and Frankston), and therefore total retail expenditure within the Mordialloc commercial centre.*

*As a general principle, centres which comprise higher levels of expenditure, generally contain higher levels of reinvestment into their building stock ( the quality of store front presentation, building facades, is continually improved or undertake frequent renovations).*

*Disinvestment (i.e. no or limited building improvements, ageing store fronts and displays) is characteristic of poorly performing retail centres, that comprise limited retail expenditure.*

<sup>3</sup> Melbourne 2030: Planning for Sustainable Growth'

<sup>4</sup> CDATE 2001, based on primary catchment area derived by SGS, 2000

<sup>5</sup> CDATE 2001.

<sup>6</sup> Based on population forecasts prepared by ID Consulting, 2004

<sup>7</sup> in the shopping centre (derived from SGS, 2000)

Service employment which includes retail, office, community services and other activities accounts for a significant share of the local economy. In addition to the provision of supporting commercial services including offices, agents, health care and associated services, it is estimated the centre employs in the order of 1,500 persons.

### *Visitor Retail & Service Demand*

Mordialloc has a developing role as both a land and water based visitor destination. Visitor industries are dependent on the presence and character of boating and related water based activities, that are within or adjacent to the creek and foreshore areas. The growth of these businesses within Mordialloc is evident through recent investments into tenancies and buildings, which cater to predominantly recreational dining within proximity of the foreshore and creek. It is however envisaged the future retail and service potentials of Mordialloc as a destination (particularly day and short stay boating and yachting related activities) has not yet been fully realised.

### *Industry*

Manufacturing makes up almost one third of businesses within Mordialloc<sup>8</sup> comprising a significant contribution to the local economy. Within a 2 kilometre radius of the commercial centre, Mordialloc comprises a consolidated node of industrial activity. Manufacturing and general industry nodes within immediate proximity of Mordialloc include:-

- the Boundary, Lower Dandenong and Hinkler Roads area north of Mordialloc;
- the area bound by Wells, Boundary and Governor Roads; and
- Governor Road, Lamana Street, Percy Street and Crown Avenue south of Mordialloc.

Many of these areas comprise small scale peripheral industry and general industrial services, dependent on exposure to primary arterial roads. They are generally provided with a low level of amenity, visual clutter and disinvestment, and are evident on the road network leading into the Mordialloc activity centre.

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<sup>8</sup> based on the Kingston Business Directory.

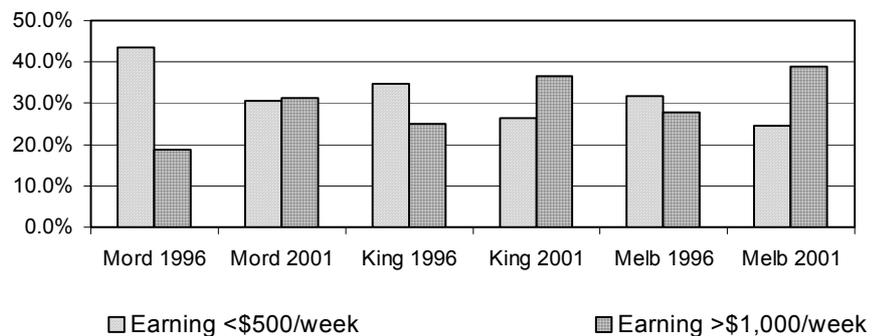
## Change & Growth

### Socio-Economic Change

Changes in the socio-economic and demographic composition of residents (1996 – 2001) within Mordialloc, reflect trends occurring in the wider metropolitan area, namely: an ageing population; smaller household size; increased higher density dwellings; lower home ownership rates; and growth in average income.

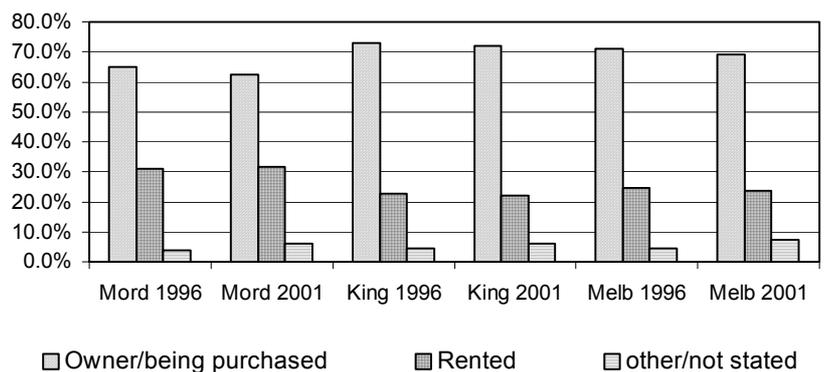
In particular, the suburb of Mordialloc :-

- comprises a relatively mature population, with 21.5% of person's aged 60+, higher than the Kingston and metropolitan respective averages of 19.3% and 15.9%;
- contains a lower proportion of residents aged less than 24 years (28.6%), which is proportionally 2 to 5 per cent lower, than that of Kingston and metropolitan Melbourne respectively;
- Mordialloc has experienced significant change in the population's income with household income of less than \$500/week dropping from 43.6% of the total population to 30.6% over the period 1996 – 2001; household income of greater than \$1,000/week increased from 18.7% to 31.2% over the same period. Changes of this extent were also experienced in Kingston and metropolitan Melbourne.



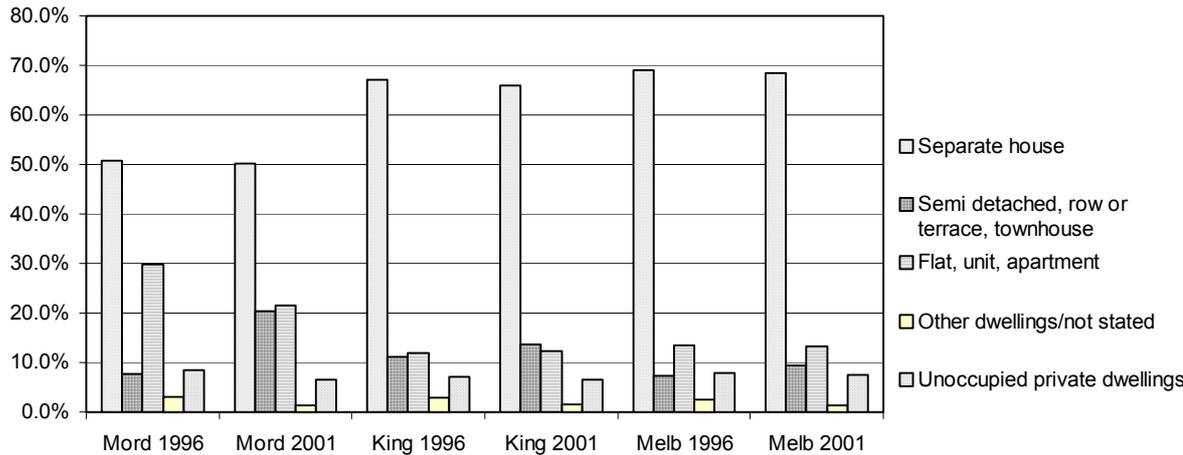
Graph 1 :  
Comparative Household Income - Change Over Time.  
Mordialloc, Kingston & Metropolitan Melbourne.

- has proportionally 7%-10% fewer dwellings owned or being purchased compared to Kingston and metropolitan Melbourne and Kingston averages. Thus, Mordialloc has a higher proportion of rental dwellings (approx 32%) compared to Kingston and metropolitan Melbourne.



Graph 2 :  
Tenure: Change Over Time.  
Mordialloc, Kingston & Metropolitan Melbourne.

- has a much lower household size 2.2pph (persons per household) then both Kingston 2.5pph and metropolitan Melbourne 2.7pph. This may be attributed to comparatively different dwelling types found in Mordialloc (compared to Kingston and metropolitan Melbourne), the most notable differences comprising the smaller quantity of separate houses.
- has a significantly higher proportion of semi-detached, terrace or townhouse, flats, units and apartments than Kingston and metropolitan Melbourne.



Graph 3 :  
Dwelling Type : Change Over Time.  
Mordialloc, Kingston &  
Metropolitan Melbourne.

## Contextual Summation

Mordialloc possesses a wide range of distinct social, physical and locational attributes, that form the basis of the vision and supporting projects to direct the future of Mordialloc. Attributes of the *Pride of the Bay* study area include:-

- **The Coast** – The study area is distinctively located on the waters edge of Port Phillip Bay and the Mordialloc Creek. It is one of only four locations within the wider metropolitan area, where natural a water course meets the eastern edge of Port Phillip Bay.
- **The Maritime Association** – The cultural associations, and positioning of the Mordialloc Village within a maritime and boating context.
- **Historic Development** – The visible historic development of the centre is provided through a series of remnant buildings, structures and landscape.
- **Community** – A community pride and spirit, that is notable within the region. It is a community proud of Mordialloc, which desires to protect and build on the intrinsic and unique qualities of the centre.
- **Retail Capacity** – Future retail needs, and the physical ability of the Centre to accommodate an additional 3,000m<sup>2</sup> retail floor space, will enable Mordialloc's retail and commercial operators to maintain and build on existing market share.
- **Environment** – The foreshore reserve and chain of parks along the Mordialloc Creek, comprising Attenborough Park, Peter Scullin Reserve, Hazel Pierce Reserve and the George Woods Reserve, provide an extensive open space network, within walking distance of the centre and surrounding residential areas.
- **Water Based Destination** – Mordialloc is a regional destination for water access, leisure, recreation and boating on Victoria's coastline. It is a centre which can build on existing attributes and provide facilities to enable the establishment of day and short stay yachting opportunities. Mordialloc can also further build on its existing ability to accommodate boating events.

- **Regional Infrastructure** – Mordialloc is provided with direct access to the Melbourne, surrounding principal activity centres (Frankston & Dandenong) and the Mornington Peninsula via principal arterial roads. Mordialloc's siting on the Frankston Railway line, further provides public transit access to both the Melbourne and Frankston.
- **Changing Demographic** – A maturing population with a higher than average household income provides opportunities for investments within retail and leisure expenditure within the centre. The continued provision of affordable and diverse residential accommodation to suit a range of lifecycle and lifestyle choices will remain important in responding to Mordialloc's changing demographic.

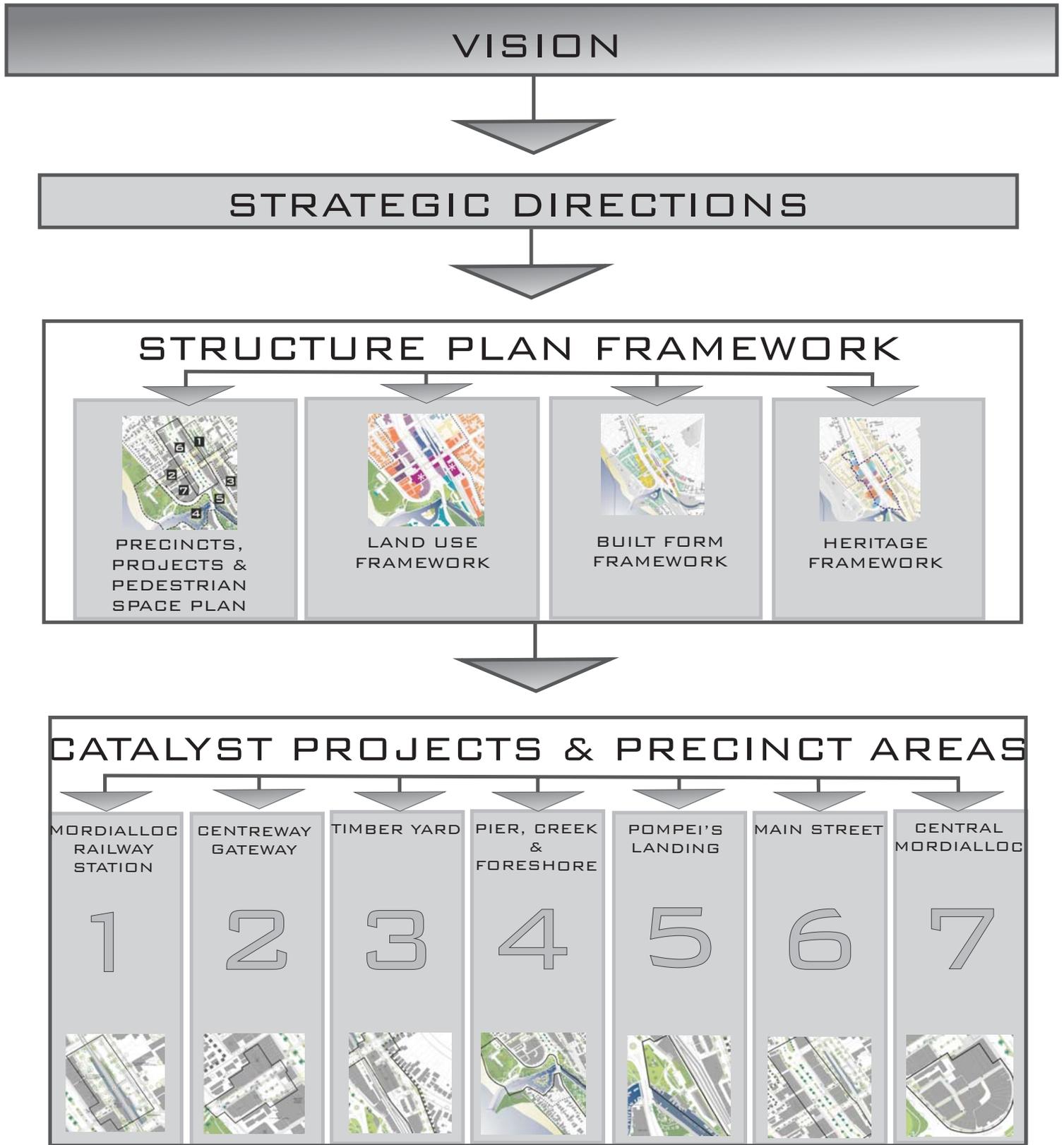
## 2.0 Pride of the Bay Structure Plan

The *Mordialloc: Pride of the Bay* Structure Plan provides a physical framework for short, medium and long term direction of Mordialloc, comprising five component elements:-

- Vision
- Strategic Principles
- Structure Plan Framework
  1. Precinct, Projects & Pedestrian Framework Plan
  2. Land Use Framework
  3. Built Form Framework
  4. Heritage Framework
- Catalyst Projects & Precincts
  1. Mordialloc Railway Station
  2. Centreway Gateway
  3. Timber Yard Redevelopment
  4. Pier, Creek & Foreshore
  5. Pompei's Landing
  6. Main Street Precinct
  7. Central Mordialloc
- Implementation

Figure 3 – Structure Plan Outline, diagrammatically represents the content and relationship of these five component elements.





**FIGURE 3  
STRUCTURE PLAN OUTLINE**

*Pride of the Bay*



## 3.0 Vision

### *'Mordialloc: Pride of the Bay'*<sup>9</sup>

The Vision adopted for this Structure Plan is that Mordialloc will be a place which is:-

- a unique coastal village that provides a vibrant focus for the Mordialloc community.
- A centre fostering unique heritage elements, including early buildings, memorials, boulevard landscaping and maritime attributes.
- A safe, pedestrian focused centre, providing ease of access for *all* users, at *all* stages of the life cycle.
- An environment which fosters the ongoing growth of a socially connected and cohesive community.
- A vibrant place for the Mordialloc community to shop, meet friends and go for a walk throughout all times of the day and night.
- A range of affordable, quality housing options, to accommodate a range of lifestyles and to enable ageing in place.
- A waterborne destination, that entices both land and water based visitors to enjoy the foreshore, the creek environs and island, whilst exploring Mordialloc's quality commercial attributes, dining within its unique heritage environs.
- The presence of boating and sustained reference to the existence of Mordialloc's iconic *'Pompei's'* boat building activities.
- An accessible centre by private vehicles, bikes, public transport and boat.
- A centre connected to the wider metropolitan area, readily accessible by an efficient public transport system.
- A colourful coastal village attracting creative, high quality development and urban design, reflective of heritage, environmental, and built form sensitivities.



<sup>9</sup> *'Mordialloc: Pride of the Bay'* was derived from interest group workshops conducted at the onset of the project. It plays on the distinctive visual images of the water (bay & creek), the place (Mordialloc) and the strong sense of community (pride).

Collectively the words provide a clear vision for Mordialloc.

It has since become the representative image for the Structure Plan. It has developed a level of acceptance within the broader community, forming the basis of trader marketing & business planning within Mordialloc.



- Mordialloc as a sustainable centre, applying best practice environmentally sustainable design principles to sensitively address interfaces with adjoining environmental attributes and environs.
- Accessible and extensive high quality open spaces.
- A sustainable commercial activity centre that addresses the resident population, whilst sensitively accommodating visitor needs.





## 4.0 Strategic Directions

The following Strategic Directions provide a foundation for the *Mordialloc: Pride of the Bay* Structure Plan.

### *Community Pride*

- To ensure the *Pride of the Bay* Study Area is reinforced as a central place for resident, working and visiting Mordialloc communities.
- Physical attributes of the centre will reflect the presence of current and future communities, within an identifiable and unique coastal community setting.
- To contribute to social cohesion and community connectedness by providing access to community meeting places.

### *Pedestrian Space*

- To ensure pedestrian safety and amenity are leading considerations of future works and development within Mordialloc.
- To facilitate access by a diversity of user and ability groups.
- To improve and enhance existing pedestrian routes and connectivity through the centre.
- To provide an attractive and direct pedestrian pathway connecting established residential areas, the centre and foreshore areas.
- To prioritise pedestrian movements and facilitate additional pedestrian crossings where opportune.
- To provide pathways where orientation is facilitated by visual cues drawing on views and vistas.
- To ensure informal pedestrian pathways within the study area are maintained.

### *Waters Edge*

- Maintain and augment destination orientated, recreational boating and maritime activities along the waters edge.
- To enable the future presence of historic boating, and maritime activities, including boat building, along the Mordialloc Creek.
- Aid remedial environmental works along the foreshore and creek environs to enable improvements to water quality, aquatic life and water flow.
- Ensure improvements to safety, surveillance and amenity.
- Establish benchmark tourism and recreation orientated commercial facilities.
- To enable linked visitor trips between the foreshore, creek area and the Mordialloc commercial area.

## Land Use

- Reinforce the Mordialloc commercial area as a vibrant, active and attractive 'Major' activity centre<sup>10</sup>, through the strengthening of special focus / mixed use precinct areas.
- To reinforce Centreway as a mixed use gateway to the Mordialloc Commercial Area.
- To maintain clearly defined edges to established residential areas, in which locations of increased housing diversity, accommodate affordable and diverse housing forms, reflective of Mordialloc's changing demographic, but which are designed to respond sensitively to the established characteristics of established residential areas.
- To accommodate growth in retail expenditure within the Mordialloc commercial area, where it can best facilitate sustainable retail expenditure throughout the centre.
- Enable expanding retail needs within proximity of retail catchment areas to the east of the centre.
- To promote residential and mixed use activity to enable living opportunities within the centre capitalising on the areas transport, open space and retail assets.
- To enable the provision of serviced and commercial accommodation within the study area to address the needs of visitors to proximate industrial areas, and to support the development of Mordialloc as a boating destination and centre for events.

## Built Form & Height

- Reinforce the traditional 2 storey scale of Mordialloc's Main Street spine as the primary village setting.
- Enable the activation of upper level Main Street buildings through discreet setback additions behind the primary parapet.
- Achieve scale transitions between established single storey stock and those areas where an increased building scale may occur.
- Strengthen the form, image and presence of the Beach Road terminal 'arc' at its meeting with the Nepean Highway, overlooking the Mordialloc Creek and coastline.
- Encourage sensitively designed consolidated development on redundant industrial land.
- Ensure Building heights, setbacks and massing that achieves solar access to principal pathways, public open spaces and forecourt areas.
- Encourage the highest standard of architectural design in the restoration and refurbishment of existing buildings and the development of new buildings, which are sensitive to the local place, and values of Mordialloc.

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<sup>10</sup> Activity Centre designation under the Victorian State Governments 'Melbourne 2030: Planning for Sustainable Growth'.

## *Access*

- To encourage walking as the most desirable option of travel towards and within Mordialloc.
- To aid the use of the Mordialloc Railway Station as a commuter point of access to the wider metropolitan transit system.
- To enable interchange between modes of transport including buses and the station area, commuter drop off points, park and ride, and bike locker facilities.
- To provide a rational approach to private vehicle and traffic access through, to and within Mordialloc.
- Consolidate and simplify vehicular points of access to off-street car parking facilities.

## *Heritage & Maritime Context*

- To protect early built fabric both within the public and private realm, including early buildings, memorials and landscaping reminiscent of the development of Mordialloc during the late 19<sup>th</sup> and early 20<sup>th</sup> Century.
- To protect places which reflect Mordialloc's historic maritime and recreational activities.
- Protection of sites of significance to Aboriginal communities.
- The provision of opportunity for the interpretation and understanding of significant aspects of Mordialloc's heritage.

## *Urban Design*

- Ensure that design is a central consideration during all stages of project development.
- To acknowledge and reflect the context and character of individual precinct areas.
- Both broad area and detailed design should seek to reinforce coastal village attributes, heritage features and unique landscape and environmental qualities within the study area.
- To enable the continued growth of Mordialloc in a contemporary urban form, that respects identifiable unique characteristics.
- To reinforce the form and land use function of identified precinct areas, that will strengthen the image of Mordialloc as a commercial and community hub.

## *Environmental Setting & Sustainable Design*

- To apply best practice environmentally sustainable design principles in the construction of new development, particularly ensuring proposed developments respond to local climatic conditions including wind mitigation.
- Provide opportunities through new development to foster alternate forms of transportation including walking and bicycle usage.
- Ensure that both public and private sector projects incorporate the key principles of Water Sensitive Urban Design.







## 5.0 Structure Plan Framework

The *Mordialloc: Pride of the Bay* Structure Plan Framework provides a preferred direction, for the physical structure and coordinated improvement of the Mordialloc Study Area. The structure plan is directed towards achieving the former mentioned vision and strategic directions. The Structure Plan Framework comprises of four component plans:-

- Precinct, Projects & Pedestrian Framework Plan;
- Land Use Framework;
- Built Form Framework; and
- Heritage Framework.

The above four element areas do not in isolation comprise an absolute means to achieve the stated Vision and Strategic Directions, but rather provide a broad framework. Council should direct place making programs and events, to ensure the achievement of directions relating to community pride and well being. Initiatives, including the establishment of the 'Mordialloc Market'<sup>11</sup>, and like events should be considered, particularly where there are demonstrated benefits to resident and commercial communities.

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<sup>11</sup> The establishment of the Mordialloc Market should consider: the regional need, and market position within the context of like bayside markets; the nomination of a preferred location within the centre where linked trips can provide a spread of economic benefits to local traders; and minimising traffic impacts.



# Precinct, Projects & Pedestrian Framework Plan

(Refer Figure 4 & 4a –Precincts, Projects & Pedestrian Framework Plans)

## Principal Pedestrian Linkage

The Structure Plan Framework is focused around a highly amenable Principal Pedestrian Linkage for all user groups and abilities. The Principal Pedestrian Link:-

- reinforces east-west pedestrian connection between the foreshore area, Main Street and established residential areas east of Albert Street;
- will link mutually supportive activity nodes throughout the Mordialloc commercial centre, and wider Study Area, in particular:-
  - facilitating pedestrian connectivity between the discontinuous commercial areas on Main Street east and Main Street west.
  - evening establishments and commercial entertainment premises (such as restaurants and bars located on Centroway or Main Street) with the foreshore promenade.
  - Mordialloc Railway Station with:-
    - established and new residential precincts;
    - commuter parking areas;
    - core commercial activities.
- will form a spine of activity and community focus.
- will provide stimulating, high quality pedestrian priority zones.
- provide convenient and safe points of access across existing barriers to pedestrian movement including the railway line, the highway and Beach Road.



The Principal Pedestrian Linkage will link mutually supportive activity nodes throughout Mordialloc.

## Pedestrian Space System

A secondary Pedestrian Space System supports pedestrian permeability through the wider study area for all user groups and abilities. The Pedestrian Space System will provide a series of safe and highly amenable, navigable routes. In addition to including existing pedestrian pathways, the Pedestrian Space System:-

- encourages the demarcation of pedestrian crossing points at intersections, or crossings;
- provides regular on-route rest points and seating;
- ensures access to the centre from surrounding residential environs;
- provides high quality access between the centre, residential areas and community facilities and areas of interest.
- provides an enhanced promenade experience, north-south along Main Street;
- improves access across Mordialloc Creek and considers an alternative option for future access to Mordialloc Island;
- extends pedestrian access along the northern and southern banks of the Mordialloc Creek;
- extends a landscaped Open Space Linkage between the Mordialloc Activity Centre, and the Doug Denyer Reserve.



The Pedestrian Space System will provide navigable routes through the wider study area, linking residential areas to areas of interest and community facilities.

Ancillary to the wider Pedestrian Space System, a Landscaped Pedestrian Linkage will improve pedestrian amenity between the Mordialloc commercial centre and designated open space areas west of the activity centre.

### *Gateway Urban Design Treatment*

The creation of memorable and distinct points of entry along main traffic routes at the edge of Mordialloc, will define the boundaries of Mordialloc. Although not located within the Pride of the Bay Study area, areas where distinct forms and formal intersections that would provide benefit to the purpose presence and designation within Mordialloc include:-

- the Nepean Highway, Railway overpass; and
- the intersection of Governor Road and Boundary Road.

These treatments may vary in forms from an artistic montage and up lighting to the Railway Bridge, a passage through banners, avenue planting, signage or the like.

### *Precinct & Catalyst Project Areas*

At the centre of the east-west Principal Pedestrian Linkage, the Mordialloc Railway Station catalyst project will reinforce the historic relationship between the Station and the Mordialloc Commercial Centre. The Railway Station project is the first of a series of projects and precincts which will branch east and west following the Principal Pedestrian Pathway.

In summary, the Structure Plan Framework comprises the following precincts and catalyst project areas:-

1. The Mordialloc Railway Station;
2. Centreway Gateway;
3. Timber Yard Redevelopment;
4. Pier, Creek & Foreshore;
5. Pompei's Landing;
6. Main Street Precinct; and
7. Central Mordialloc.

Collectively, the cluster of projects and precincts reinforce the east-west Principal Pedestrian Linkage and supporting Pedestrian Space System. They further address and facilitate the achievement of the Strategic Directions contained under Section 4.0 of this report.

A detailed description of catalyst projects and precinct areas are discussed in Section 6.0 of this Report.



- STUDY AREA
  - PARKLAND & OPEN SPACE
  - PEDESTRIAN SPACE SYSTEM
  - PRINCIPAL PEDESTRIAN LINKAGE
  - GATEWAY URBAN DESIGN TREATMENT
  - LANDSCAPED PEDESTRIAN LINK
  - EXISTING / NEW PALM
  - BUILDING FOOTPRINT
  - RAILWAY LINE & RESERVATION
- 1 MORDIALLOC RAILWAY STATION
  - 2 CENTREWAY GATEWAY
  - 3 TIMBER YARD REDEVELOPMENT
  - 4 PIER, CREEK & FORESHORE
  - 5 POMPEI'S LANDING
  - 6 MAIN STREET PRECINCT
  - 7 CENTRAL MORDIALLOC

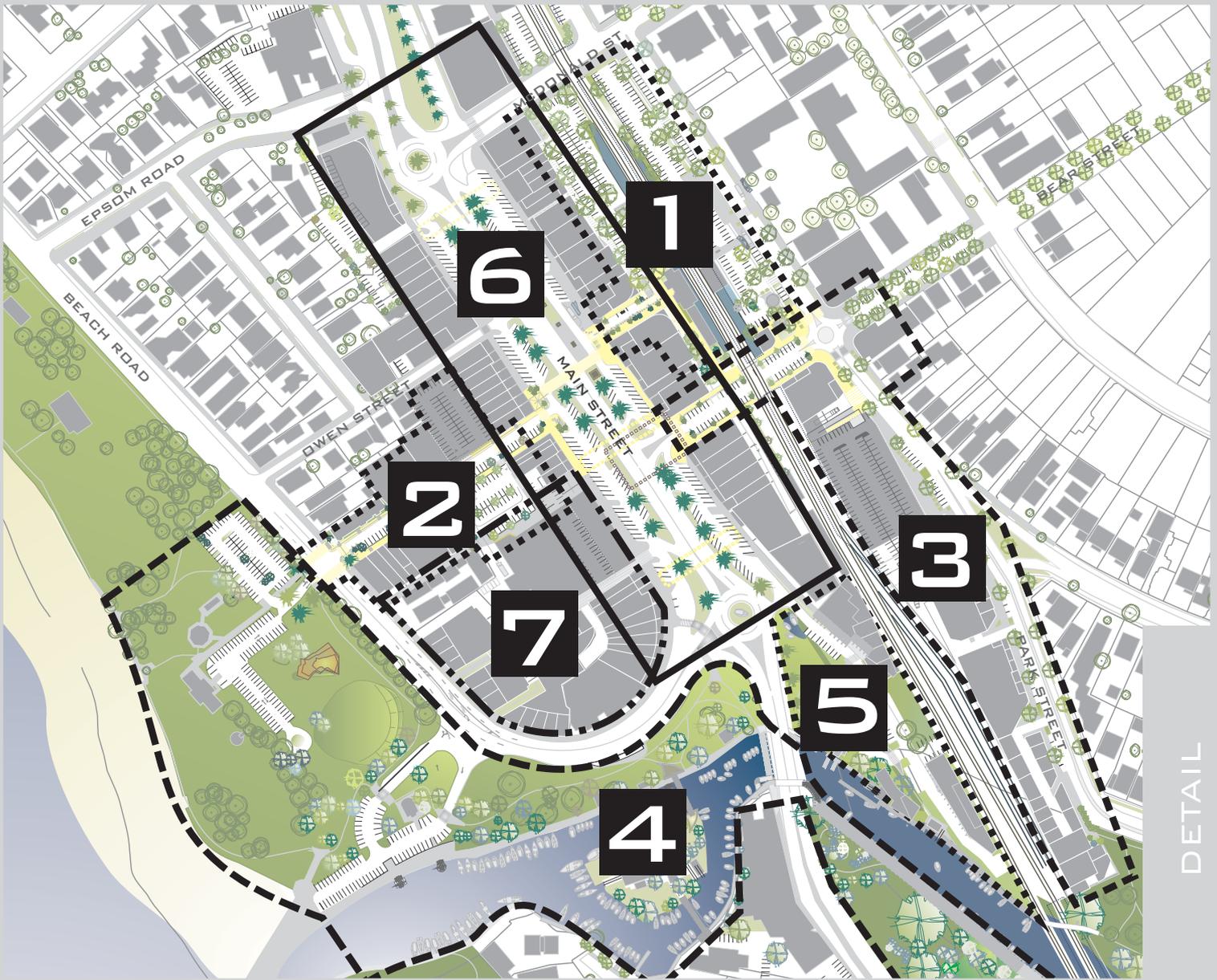
FIGURE 4

# MORDIALLOC STRUCTURE PLAN: PRECINCTS, PROJECTS & PEDESTRIAN FRAMEWORK PLAN

*Pride of the Bay*

0 0.5km 1km





DETAIL

- |                              |                                     |
|------------------------------|-------------------------------------|
| PARKLAND & OPEN SPACE        | <b>1</b> MORDIALLOC RAILWAY STATION |
| PEDESTRIAN SPACE SYSTEM      | <b>2</b> CENTREWAY GATEWAY          |
| PRINCIPAL PEDESTRIAN LINKAGE | <b>3</b> TIMBER YARD REDEVELOPMENT  |
| LANDSCAPED PEDESTRIAN LINK   | <b>4</b> PIER, CREEK & FORESHORE    |
| EXISTING / NEW PALM          | <b>5</b> POMPEI'S LANDING           |
| BUILDING FOOTPRINT           | <b>6</b> MAIN STREET PRECINCT       |
| RAILWAY LINE & RESERVATION   | <b>7</b> CENTRAL MORDIALLOC         |

FIGURE 4(A)

# MORDIALLOC STRUCTURE PLAN: PRECINCTS, PROJECTS & PEDESTRIAN FRAMEWORK PLAN DETAIL PLAN

*Pride of the Bay*



## Land Use Framework

(Refer Figure 5 & 5a –Land Use Framework Plans)

A series of structured land use precincts direct a range of diverse mixed use and specialised activity precincts within the Study Area.

### *Parklands & Open Space*

Enhanced access to Mordialloc's extensive open space network within vicinity of the study area. These reserves include:-

- the Mordialloc Foreshore;
- Attenborough Park;
- the Peter Scullin Reserve;
- the Hazel Pierce Reserve;
- the Ben Kavanagh Reserve;
- the Doug Deyner Reserve;
- the George Woods Reserve;
- the J.Grut Reserve;
- the Browns Reserve; and
- the Mordialloc Creek Reserve.

These reserves will continue to provide a range of passive and active recreational pursuits. They will continue to provide a buffer between the commercial, residential and industrial areas within Mordialloc and the environmental attributes of Port Phillip Bay and the Mordialloc Creek.

Significant improvements to safety, access and amenity are envisaged as part of a revitalised parkland setting within the Pompei's Landing precinct. Improvements to creekside open space in this area are central to improving wider open space linkages, and enhancing the landscaped connection between the Mordialloc Creek and Main Street Commercial area.

### *Mixed Use / Increased Residential Densities*

A series of sites within a 400m radius of the Mordialloc Railway Station have been identified for Mixed Use / Increased Density Residential Development.

These areas will comprise low rise residential development which may include small structured commercial premises (i.e. small scale retail, café, office, home office) which will contribute to the diversity, surveillance and levels of activity characteristic of an urban setting.

### *Increased Housing Diversity<sup>a</sup>*

Within these areas, encourage increased residential densities and a wider diversity of housing types and sizes. These are areas which are within convenient walking distance of public transport and the Mordialloc Activity Centre.

### *Incremental Housing Change<sup>a</sup>*

In suburban locations which are not within convenient walking distance of public transport or the Mordialloc activity centre, encourage a predominance of single dwellings and the equivalent of dual occupancy developments on average sized lots.

### *Mordialloc Commercial Core*

The Mordialloc Commercial Core area contains existing core retail activities, including the existing supermarket and series of specialty stores fronting Main Street. The redevelopment of the Timber Yard site to accommodate an additional retail attractor, coupled with the development of the east-west Principal Pedestrian Linkage (refer to *Precinct, Projects & Pedestrian Framework Plan*), will enable the further development of the retail area that spans Main Street.

It is envisaged, the new eastern retail attractor, will:-

- extend increased economic benefit to local traders on Main Street east; and
- be proximate to the residential catchment, east of the Railway Line and Main Street, thus more appropriately distributing traffic and congestion.

It is integral, smaller tenancies between the two anchor stores, provide active retail tenancies, that address the Principal Pedestrian Linkage. Inactive uses (offices, residential, services) should be contained on the upper floor levels.

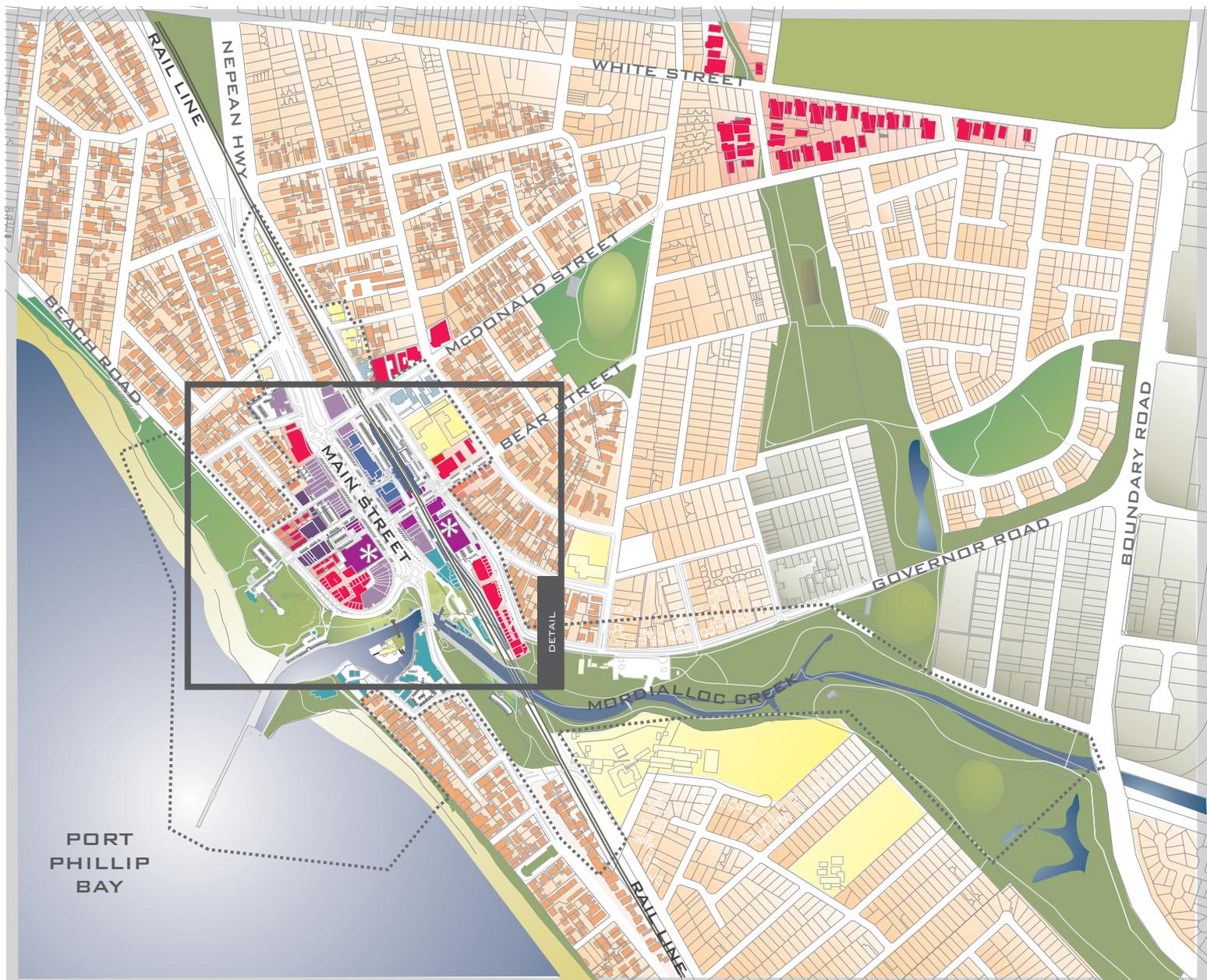
### *Main Street Retail Commercial Environs*

This precinct includes the two established retail strips adjacent to Nepean Highway and Main Street. It includes intact illustration of the historic development of Mordialloc as a coastal village centre.

The revival of this precinct should be focused around the two distinct commercial promenades along Main Street east and Main Street west. Specialised retail and commercial services will continue to address Main Street at the street level, contributing to an attractive and active commercial promenade.

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<sup>a</sup> Based on Council's Residential Development Policy (Amendment C8), formally adopted by the City of Kingston Council, 15 April, 2003.



- STUDY AREA
- PARKLAND & OPEN SPACE
- MIXED USE / HIGHER DENSITY RESIDENTIAL
- INCREASED HOUSING DIVERSITY
- INCREMENTAL HOUSING CHANGE
- MORDIALLOC COMMERCIAL CORE
- ✳ RETAIL ATTRACTOR
- CENTREWAY MIXED USE PRECINCT
- STATION FORECOURT
- FORESHORE & CREEK ENVIRONS
- COMMUNITY FACILITIES & INSTITUTIONAL USES
- MEDICAL & PROFESSIONAL SERVICES
- INDUSTRIAL
- MAIN STREET RETAIL & COMMERCIAL ENVIRONS

FIGURE 5

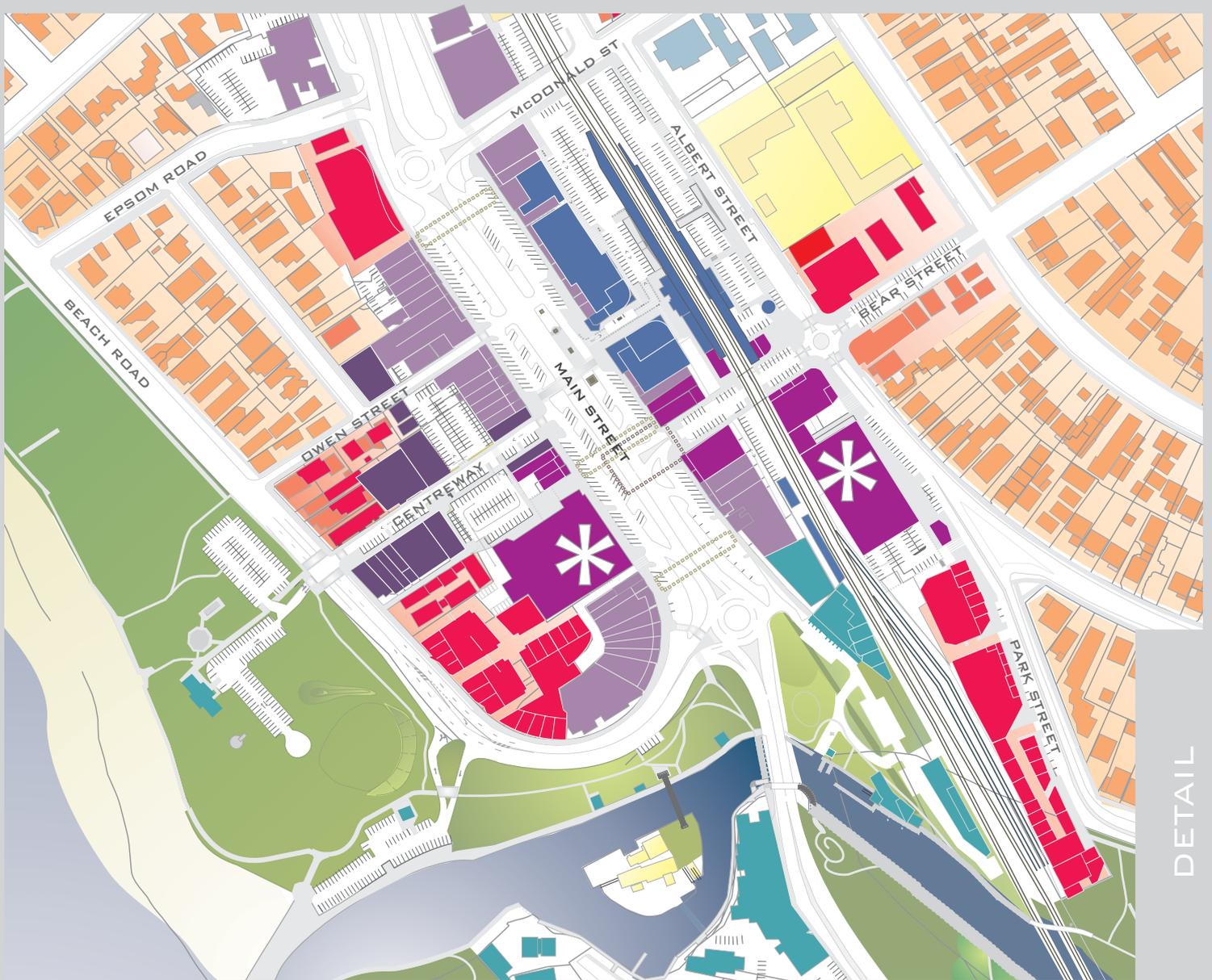
# MORDIALLOC STRUCTURE PLAN: LAND USE FRAMEWORK

0 0.5km 1km



*Pride of the Bay*





DETAIL

- PARKLAND & OPEN SPACE
- CENTREWAY MIXED USE PRECINCT
- MIXED USE / HIGHER DENSITY RESIDENTIAL
- INCREASED HOUSING DIVERSITY
- INCREMENTAL HOUSING CHANGE
- MORDIALLOC COMMERCIAL CORE
- RETAIL ATTRACTOR
- STATION FORECOURT
- FORESHORE & CREEK ENVIRONS
- COMMUNITY FACILITIES & INSTITUTIONAL USES
- MEDICAL & PROFESSIONAL SERVICES
- INDUSTRIAL
- MAIN STREET RETAIL & COMMERCIAL ENVIRONS

FIGURE 5(A)

# MORDIALLOC STRUCTURE PLAN: LAND USE FRAMEWORK DETAIL

*Pride of the Bay*



Mixed use residential and business functions will provide secondary activities at upper floor levels. This will enable a resident community presence within the Mordialloc commercial area throughout all times of the day and night.

### *Centreway Mixed Use Precinct*

A precinct fundamental to the success of the east-west Principal Pedestrian Linkage, Centreway will provide a stimulating street environ, linking the Mordialloc Foreshore to Main Street. Centreway will provide a gateway to the Mordialloc commercial area. It will contain a cluster of food premises, café's and small commercial premises which actively engage with the street.

Site redevelopment should facilitate upper floor residential activity.

### *Station Forecourt*

The redevelopment of the Mordialloc Railway Station Forecourt will reinforce the presence of the station within the commercial centre. The development of premises addressing the Mordialloc Station Forecourt, will enable ground floor tenancies to accommodate a range of retail and commercial services that address commuter needs.

It is envisaged premises currently facing Main Street east, will reorientate, providing dual store frontages onto a consolidated at grade car park. Existing retail and service orientated tenancies, will benefit from improved consumer / client access. Upper floor level activities will include residential and commercial functions.

### *Foreshore & Creek Environs*

The Foreshore and Creek Environs will comprise select destination orientated, recreational, boating and maritime activities that are dependent on access to the waters edge.

It is envisaged that recreational boating, and maritime activities, including boat building and repairing, will to continue to form a central function on the water's edge, particularly along Mordialloc Creek.

Tourism and recreation orientated commercial facilities (i.e. restaurants, kiosks, floating commercial structures) within the Foreshore & Creek Environs should reinforce both active and passive areas along the water edge, providing access to diverse user groups.

In select locations a mix of uses, including residential activity, secondary to the principal recreational, boating and maritime activities, may be accommodated where it can be demonstrated there are direct improvements to safety, surveillance and amenity.



Foremost, this is a location that will seek to build on its historic origin as a centre for festivals, tourism and maritime activities. This area should continue to accommodate existing events, including the 'Mordialloc Food and Wine Festival', but should also enable the growth of boating and sailing carnivals, and competitions.

### *Community Facilities & Institutional Uses*

Community Facilities & Institutional Uses will be retained on existing sites. These include a range of groups including churches, hall facilities, clubs and infrastructure users (Telstra, Ambulance) and although not limited to, include:-

- Bay Group Association for the Blind;
- Mordialloc Sea Scouts;
- Allan McLean Hall;
- Mordialloc Mentone Lions;
- Bayside Hostel;
- Mordialloc Neighbourhood House;
- Kingston Toy Library;
- Mordialloc Community Centre;
- Mordialloc Senior Citizens; and
- Oz Child Children Australia.

Additional facilities should seek to co-locate within proximity of existing institutional facilities around the intersection of Albert Street and Lewis Street.

### *Medical & Professional Services*

Encourage the refurbishment and/ or redevelopment of existing commercial premises within proximity of the McDonald Street, Albert Street intersection and enable the creation of additional medical and health related services and related functions.

### *Industrial*

The structure plan encourages the integrated redevelopment of fractionised general industrial services where these areas directly adjoin waterways, open space, railway and residential areas. A review of industrial needs, and the positioning of Mordialloc's Industrial areas should reaffirm future directions of areas within proximity to the study area.

Redevelopment of existing industrial site's should prioritise the achievement of significant remedial environmental improvements.



## Built Form Framework

(Refer Figure 6 – Built Form Framework)

The development scale framework sets out a vision in relation to the built form and urban design structure of Mordialloc. It supports the identified Strategic Directions and the specific Built Form and Height principles outlined earlier in this report. The proposed development scale framework has evolved around, and is sensitive to three key place values, including the inherent urban design structure of Mordialloc village, the visual experience of the Centre, including its coastal context and the inherent sense of place of the Main Street streetscape. It has also been closely interwoven with the recommendations of the Mordialloc Heritage review which has identified key buildings of note with the Main Street Heritage Precinct. The primary emphases of the development scale framework are outlined below.

### *Mordialloc Village Scale*

The development scale framework for Mordialloc reinforces the low profile of buildings within the village setting. The maximum height of development within Mordialloc should be no greater than 4 storeys or 14m, and will only occur on selectively identified opportunity sites. This will reiterate the sensitive coastal and creek side sense of place, and will allow an appropriate integration with the ostensibly low scaled Main Street spine and the residential periphery. Given the 'fixed' format of development to the Beach Road/ foreshore (within the foreshore DDO) at a maximum 2 storeys and the widely valued traditional 2 to 3 storey proportion of Main Street, only marginal increases in scale up to 4 storeys are envisaged (i.e. on identified opportunity sites including the Pompei's<sup>12</sup> and Timber Yard sites). This will allow the village to fit within its coastal and creek side landscape and avoid the prospect of an isolated 'spike' in height along the Bayside frontage. In this sense, it is the distinguishable landscape and topography of Mordialloc that marks it as a place along the Port Phillip Coastline.

### *Main Street Scale*

The scale of proposed development along Main Street, Mordialloc from McDonald Street to the junction with Nepean Highway should respond to the spacious dimension of the streetscape and the integrity of traditional building stock to either side of the spine, which comprise forms of both regional and local architectural significance. While there are also a number of ungraded or non contributory buildings along the corridor, the prevailing traditional 2 storey scale of the streetscape remains intact. There are opportunities for extension of marginally taller built elements behind the primary street parapet, which will better utilising scarce vacant land at the back of shops. The Main Street development form will consist of shop top forms within existing traditional

<sup>12</sup> Only marginal increases of height are provided under the Built Form Framework. As an example, the construction on the Pompei's site of a 3 storey to the street (11 metres) and recessed 4<sup>th</sup> storey development (14 metres), only marginally exceeds the estimated 10.5 metre building height of the existing shed structure and signage on this site.

building envelopes and new infill development that will match in with the primary horizontal alignments of adjoining buildings and the streetscape.

The development scale to the Main Street frontage should be 2 storeys or 7.5m, but may be increased to match in with the height of existing traditional buildings to either side, but should not exceed 2 storeys. A setback 3<sup>rd</sup> storey to a maximum height of 11m may be achieved to Main Street buildings behind the traditional parapet line. Along Main Street this setback should not be utilised as an outdoor deck or veranda. Structures, awnings, glazing or the like should not be located within the Main Street setback distance.

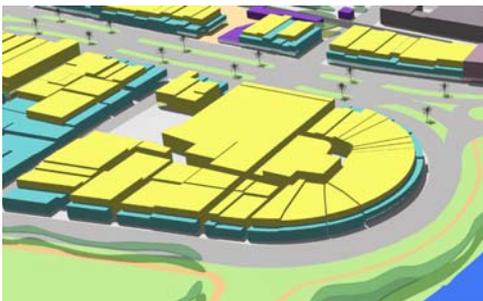
The setback distance behind the Main Street parapet should be determined with regard for the heritage significance of the existing building as follows:

- Buildings of Architectural Significance: 8m setback
- Buildings of Streetscape Value: 4m setback

No upper level additions are to be established on sites comprising buildings of National or State Significance, as occur to the north-eastern side of the streetscape.

The proposed setback conditions are not designed to make upper level development above 2 storeys invisible, rather recessive. The proposed setbacks of 4m and 8m respectively will allow for clear viewing of the total intact building elevation without the intrusion of any dominant upper level projection.

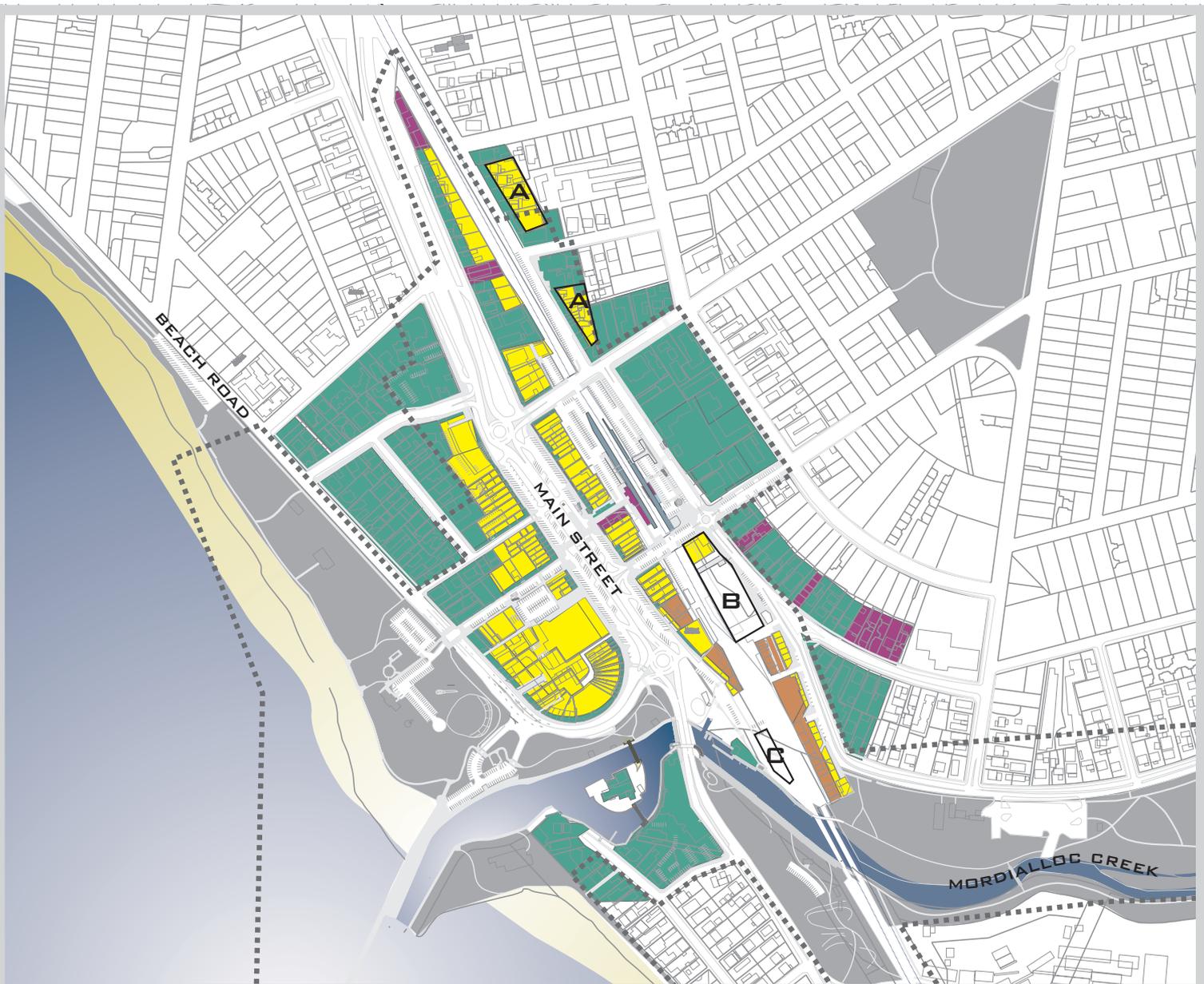
The incorporation of such upper level additions in an integrated manner provides an opportunity to regenerate the streetscape and accommodate for a mix of uses above the Main Street shopfront.



### *Beach Road Scale*

The scale of proposed development along Beach Road is anchored to a large degree by the existing 2 storey height control contained in the Design and Development Overlay (DDO) which applies to all residentially zoned land between the Beach Road and the rear of commercial properties up to Centre Way. The propensity for change along the Beach Road corridor is therefore limited to the relatively confined 'block' to the south of Centre Way which wraps in an arc to meet with Nepean Highway. This block presently houses some of the most substantial building forms in Mordialloc including the Safeway building and the south facing 3 storey glazed office building to the Beach Road arc.

The development scale to the Beach Road should be 2 storeys or 7.5m, but may be increased 3<sup>rd</sup> storey to a maximum height of 11m with an applied 4m setback, which may be utilised as an apartment terrace or balcony form. Development within this Precinct should be consolidated but accommodate for a suite of new east-west aligned lanes and pedestrian links which encourage permeability and connections between the beach and the Main Street spine. The scale of buildings abutting these lanes and links should be no greater than 3 storeys so as to encourage reasonable day/ sunlight penetration The



**STUDY AREA**

- 1 STOREY / 4.5M BUILDING HEIGHT
- 2 STOREY / 7.5M METRE BUILDING HEIGHT
- 3 STOREY / 11M METRE BUILDING HEIGHT
- 3 STOREY/ 4 STOREY SETBACK  
(1.1M BUILDING HEIGHT AT STREET FRONTAGE. RECESS 4TH STOREY 14M BUILDING HEIGHT)

**A** 3RD STOREY SUBJECT TO SITE CONSOLIDATION

**B** 11M BUILDING HEIGHT

**C** 10.5M BUILDING HEIGHT

**KEY URBAN FORM PRINCIPLES:**

- REINFORCE RECOGNISED SCALE OF MAIN STREET (TRADITIONAL 2 STOREYS)
- INCORPORATE SETBACK UPPER ADDITIONS (TO 3 STOREYS) ON MAIN STREET
- ACKNOWLEDGE FORESHORE DDO REQUIREMENT (2 STOREYS)
- ACHIEVE TRANSITION TO ADJOINING SINGLE STOREY STOCK
- ACCOMMODATE FOR SINGLE STOREY STEPPED SCALE TRANSITIONS
- ENCOURAGE 4 STOREY FORM ON FORESHORE ARC & RAIL EDGE

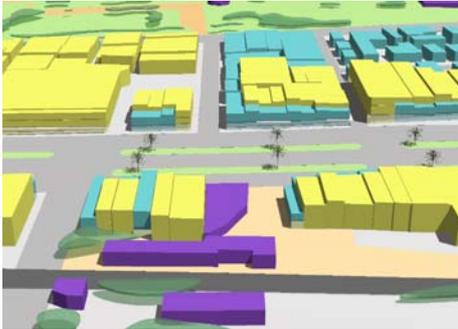


FIGURE 6

# MORDIALLOC STRUCTURE PLAN: BUILT FORM (BUILDING HEIGHT) PLAN

*Pride of the Bay*

proposed 3 storey scale of this urban block will effectively conceal the unsightly Safeway building and accommodate for contemporary projections of 2 storeys above narrow single storey shopfronts near the Beach Road and Nepean Highway junction. The proposed form will establish a proud 'urban' face to both the beach and creek and improve surveillance of these important public spaces. The gentle step in scale from a standard 2 storeys to the north, to a more consolidated 2 to 3 storeys to the south generates a fitting bookend at the terminal arc of Beach Road.



### *Station Precinct Scale*

The scale of proposed development within the Mordialloc Station Precinct, comprising the Station, Platforms and Water tower should strongly emphasise the historical importance and transit function of the Station, which is strategically positioned at the physical heart of Mordialloc Village. The Station buildings are important heritage elements and key local landmarks to Mordialloc residents and traders alike. It is important to protect (or enhance) view corridors from Main Street towards the Station buildings and to encourage a sense of spaciousness around the Station. An open threshold around these elements gives clarity to the heritage form and provides ample space for the movement and manoeuvrability of people.

Existing heritage buildings within the Station Precinct should be retained and restored in keeping with contemporary standards. There should be no significant alteration to these structures as they represent an important part of the evolution of the village. Existing retail forms to the west of the Station, which wrap around to face the Station forecourt from Main Street should not maintain a standard Main Street profile. Development on the southern edge of the Station forecourt should be constrained to 1 storey (as existing) and present active frontage to the north. Development to the north of the space should be 2 storeys to the frontage and may have a setback 3<sup>rd</sup> level, however it should not significantly overshadow the Station Forecourt.



### *Railway Abuttal Scale*

The proposed development of underutilised land along the Railway line presents one of the few opportunities to achieve a higher order of development without the direct streetscape, amenity or heritage impacts that constrain other parts of Mordialloc. The land to the south of Bear Street, between Albert Street and the Rail line provides the greatest (and most imminent) prospect for increased density and scale. On the Timber Yard site (which contains buildings of approximately 9 metres in height), and the Pompei's site to the western side of the Rail line fronting Mordialloc Creek (which currently contains a substantial shed structure of approximately 10.5 metres in height) more robust taller development may be suited. Its justification is grounded in the existing industrial and warehouse forms on the land which itself forms part of the local built form 'frame of reference'.

The development scale to the Railway Abuttal land to the south of Bear Street and east of the Rail line should be a maximum of 4 storeys and 14m, with a 3 storey frontage at 11m to Park Street. The development scale of Pompei's will be limited to 3 storey development to the site's street frontage, and a recessed

4<sup>th</sup> storey at the rear of the site. These forms may project a robust form that celebrates the 'working' flavour of the Creek corridor and the rail edge.



### *Eastern Fringe Scale*

The proposed development of key sites to the eastern side of Albert Street is also appropriate within the scope of the Mordialloc Structure Plan, given the areas proximity to the retail centre and the Mordialloc railway station as well as the relatively limited extent of residential interface. Key sites on the corners of Bear and Albert Streets, the areas around the Mordialloc Primary School and Masonic Hall and land further north of McDonald Street has the capacity to evolve from the existing single storey format to a 2 and in select locations a 2 storey recessed 3 storey scale. This reflects a subtle shift in building scale from the low scale hinterland to a more substantial core around Main Street and the Station, signalled by the existing taller structure of the Masonic Hall, Church buildings and the historic water tower.

The development scale to the Eastern Fringe should extend to a maximum height of 2 storeys and 7.5m, in particular to the south associated with the junction of Bear and Albert Street. Sites on the eastern side of Albert Street north may accommodate for 2 storey development to 7.5m.

### *Development Scale and Design Quality*

Within the framework of these development scale recommendations, the refurbishment of existing buildings and the development of new forms within Mordialloc should achieve the highest standard of architectural and design quality. While the recommendations of the structure plan identify limits with respect to development height and form, it is noted that these are 'maximum' and the greatest emphasis in development should be placed on the achievement of design quality, visual interest and development diversity.

Within the urban form recommendations provided above, the following architectural and design goals should be achieved. All new development should:

- Exhibit a high standard of architectural and urban design through the presentation of integrated building forms that have a sense of address clearly articulated façades and sensitively designed building silhouette.
- Introduce new building forms that are legible, attractive and representative of function.
- Apply design techniques, including façade variation, contrast, repetition, colour, texture and detail that will integrate a building with its natural landscaped surroundings and create an attractive and interesting form in the streetscape.
- Reflect sensitivity to the scale, design, character and configuration of the precinct through the proportional massing of building form and its parts, with appropriate built form.
- Highlight key corners, entries or landmarks through design projection, detailing and massing that presents to both street frontages and the site's surroundings.
- Seek to interpret in a contemporary manner prevailing local design

references, including relevant architectural features, forms, materials and finishes.

- Reflect the important patterns of local development through the careful definition of building form, alignment, levels, division and proportions of solid and transparent façade elements.
- Ensure that service infrastructure is appropriately sited into the design and obscured from the public domain.
- Exercise a high regard for the principles of environmentally sustainable development and design including solar orientation, use of storm water management systems and natural ventilation.



## Heritage Context & Framework

A series of heritage sites and precincts are located within the *Mordialloc: Pride of the Bay* study area. These sites and precincts present a dialogue of the historic growth and development of Mordialloc as a commercial area and residential neighbourhood. In addition to the Heritage Framework contained in the next section, the construction of new development within the study area should:-

- be sympathetic and designed to be compatible with the heritage areas context; and
- ensure new buildings preserve the important views and vistas enjoyed of the areas existing built form and natural heritage.

### *Main Street* <sup>13</sup>

Main Street Mordialloc is historically significant as it retains much of its original relationship to the Mordialloc Railway Station; the centre therefore remains as an unusually legible example of an early retail / transportation hub. It is architecturally significant for the quality of its early building stock, and the existence of memorials, the avenue layout and landscaping.

Of particular note, Main Street comprises varied shops dating from c.1890 to the late 20<sup>th</sup> century. The earliest buildings comprise late Victorian and Edwardian shops and other commercial premises. Extant Federation and Italianate buildings comprise two storeys and parapets, providing *historic* building heights which range between 7.2 and 11.2 m above the street level. In some cases these buildings provide an equivalent height of a contemporary three storey building. Facades have generally been modified at ground floor level but remain unaltered above.

Interwar structures contribute to the valued character of the precinct through their massing, scale and form. Shops from this period are typically two stories, some featuring decorative brickwork and parapets, or strong geometrical detailing. The war memorials were also constructed during this period.

Whilst much of the area contains interwar character, many buildings are recent replacements of early building stock.

### *Mordialloc Creek and Foreshore* <sup>14</sup>

The Mordialloc Creek and Foreshore Precinct provides a reasonably intact example of a late nineteenth, early twentieth century seaside resort. The area contains a number of unusual structures which illustrate the early development of place. These include the pier, sea wall, band rotunda and toilet block. Since earliest days of white settlement, the creek has been popular with visitors and locals for fishing and boating pursuits, while the beach was popular for bathing.



Mordialloc Railway Station  
Main Street retains much of its original relationship to the Mordialloc Railway Station.



Main Street East  
Intact examples of the historic development of Main Street Mordialloc, provide a reference for future development forms and height



Creek & Foreshore  
The band rotunda is one of many early late nineteenth, early twentieth century seaside resort structures.

<sup>13</sup> Extracts from the 'City of Kingston Heritage Study: Place Identification Form - Main Street Mordialloc' (2001) Bryce Raworth Pty Ltd.

<sup>14</sup> Extracts from the 'City of Kingston Heritage Study: Place Identification Form - Mordialloc Creek and Foreshore' (2001) Bryce Raworth Pty Ltd.

## Heritage Framework

(Refer Figure 7 – Heritage Framework)

The Heritage Framework areas comprise the following designations:-

- National or State Significance
- Regional Significance
- Architectural & Historic Contribution
- Intact Building of Streetscape Value

### *National or State Significance*

Buildings of national or state significance are generally contained on the Victorian Heritage Register. These are buildings which may be considered irreplaceable parts of Australia's built form heritage, and are significant as rare or distinct in comparison to other buildings of its type, use, era, style or state.

These buildings should not be demolished modified or added upon.

### *Regional Significance*

These contain sites of local significance at a municipal or regional level, which stand as important milestones in the architectural development of the city. These buildings do not demonstrate importance in a manner or extent which is rare or distinctive (as above), in comparison to other buildings of their type, use, era, style on a state-wide basis, although they may be considered rare or distinctive within the context of the City of Kingston.

Demolitions of these buildings are not generally supported, and where additions are desired, these should be modest and sympathetically sited.

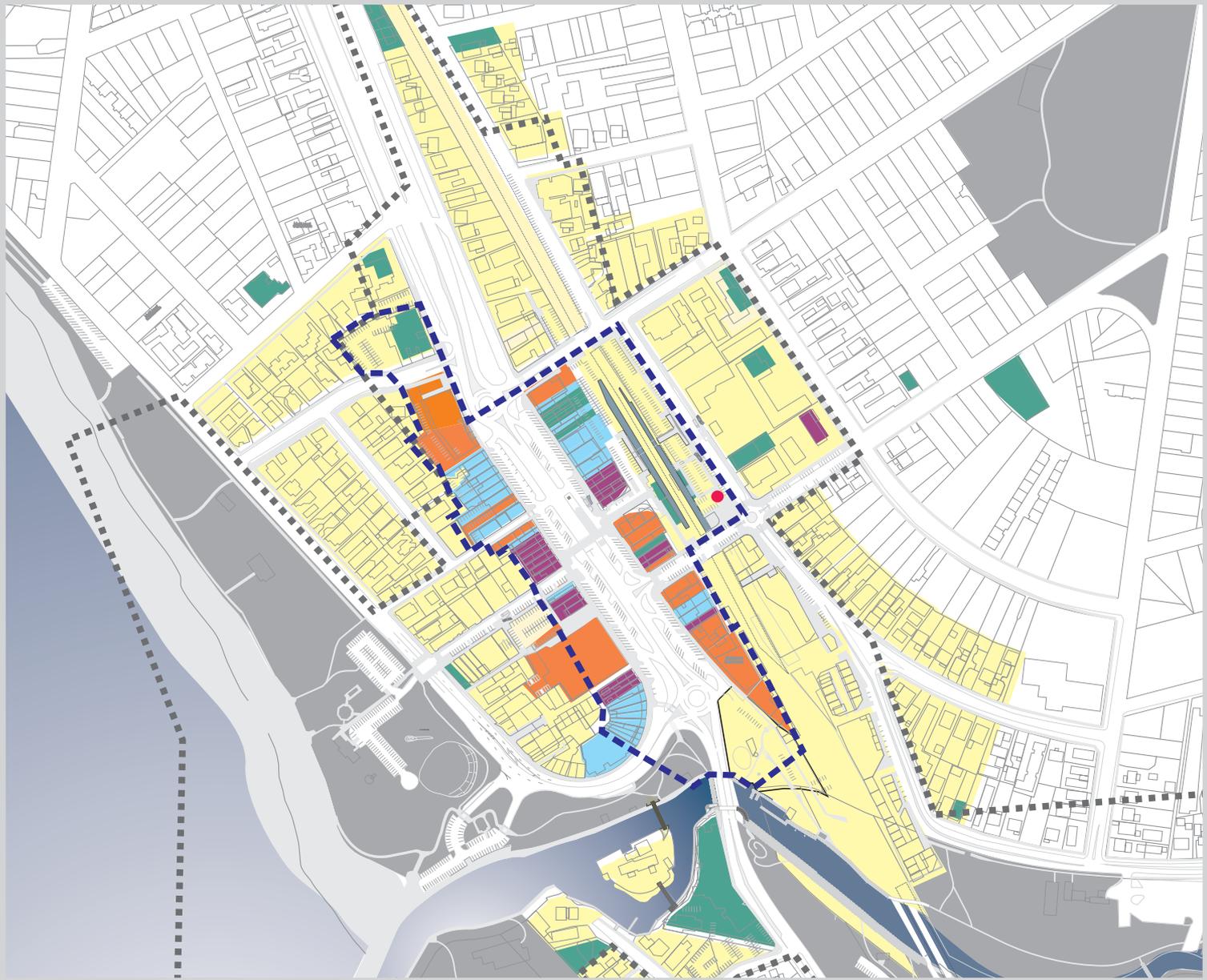
### *Architectural & Historic Contribution*

Buildings of architectural and historic contribution, contribute to the character of a group of buildings and the wider streetscape. Buildings in this category will usually be fine examples of their type, era or style. They generally contain a substantial amount of their original material and appearance.

Many of these buildings gain part of their significance from their location within the architecturally and historically rich context. These buildings retain sufficient architectural character to make a useable and irreplaceable overall streetscape.

Future development of such buildings should retain the character of the cluster of buildings, and should enable sympathetic additions.





-  STUDY AREA
-  PROPOSED MAIN STREET HERITAGE OVERLAY
-  NATIONAL / STATE SIGNIFICANCE (NOT TO BE DEMOLISHED, MODIFIED OR ADDED UPON)
-  REGIONAL SIGNIFICANCE (DEMOLITION UNSUPPORTED. MODEST SYMPATHETICALLY SITED ADDITIONS)
-  ARCHITECTURAL & HISTORIC CONTRIBUTION (RETAIN CHARACTER OF THE GROUP OF BUILDINGS OR BUILDING. SYMPATHETIC ADDITIONS TO CONCEALED LOCATIONS)
-  INTACT BUILDING OF STREETScape VALUE (DEMOLITION OF BUILDINGS OF POOR CONDITION)
-  UNGRADED BUILDING (NON-CONTRIBUTORY)
-  OUTSIDE HERITAGE PRECINCT STUDY



FIGURE 7

# MORDIALLOC STRUCTURE PLAN: HERITAGE FRAMEWORK PLAN

INFORMATION PROVIDED BY  
BRYCE RAWORTH PTY LTD 2004

*Pride of the Bay*

### *Intact Building of Streetscape Value*

These sites are generally reasonably intact representatives of particular periods or styles, or they have been substantially altered, but comprise a row or street, which retains much of its original character.

The demolition of buildings in poor condition may be allowed. Additions and works should be sympathetic and within concealed locations.

### *Ungraded Buildings*

These are buildings which makes no important contribution to our understanding of the importance of unique aspects of the historical and architectural development of the area.



## 6.0 Catalyst Projects & Precinct Areas

The Mordialloc activity centre provides a focus for both the local Mordialloc community and for visitors alike. The centre not only comprises of a commercial district, but also benefits from its unique setting on Port Phillip Bay and the Mordialloc Creek.

The centre is faced with many unique challenges and opportunities, in striving to benefit from growing retail and service activity, and wider economic growth. There are also many challenges in addressing Mordialloc's role as a designated Major Activity Centre under Melbourne 2030.

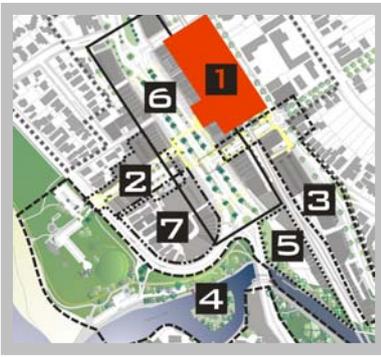
Within this context the *Mordialloc: Pride of the Bay* Structure Plan has identified seven catalyst project or precinct areas which collectively are aimed at achieving the stated *vision* and improved future of Mordialloc. These include:-

1. The Mordialloc Railway Station (catalyst project);
2. Centreway Gateway (catalyst project);
3. Timber Yard Redevelopment (catalyst project);
4. Pier, Creek & Foreshore (catalyst project);
5. Pompei's Landing (catalyst project);
6. Main Street Precinct (precinct area); and
7. Central Mordialloc Precinct (precinct area).

Catalyst project areas are more likely to require a range of intervention and project facilitation, and will include both public and private sector development and investment in select areas. On the whole, precinct areas principally comprise policy and guidelines to facilitate desired objectives within selected areas. Notwithstanding this, each identified area comprises a range of implementation measures to achieve desired conditions.

Propositions for each of the areas are described below.





# 1

## Mordialloc Railway Station

### Context

The Mordialloc Railway Station provides a highly unamenable and unsafe interface with the surrounding areas including:-

- Main Street via a poorly surfaced asphalt car park;
- the rear of premises fronting Main Street;
- fragmented publicly accessible commuter and private car parking areas accessed from McDonald street and Main Street; and
- the Albert Street commuter parking area.

From a historic perspective, the Mordialloc Railway Station is representative of a significant period of historic growth and development of Mordialloc. The Mordialloc Railway Station project area comprises a series of valued and unique characteristics, in particular:-

- heritage buildings and elements including:-
  - The garden cottage style historic Mordialloc Railway Station Buildings (1882 & 1887); and
  - The last remaining example of similarly designed Edwardian Railway Water Tower(1910).
- Proximity to and direct sightlines of the Main Street Commercial Area and Centreway.
- Direct lines of sight to identifiable monuments.

The site is challenged by limited directional clarity for both vehicular and pedestrian access, into and through this local area.



Existing Railway Forecourt  
 Expansive and underutilised car parks,  
 vacant tenancies and inactive building  
 edges contribute to a low level of perceived  
 safety and amenity.

## Project Description

(Refer Figure 8(a) – Mordialloc Railway Station Catalyst Project Option A & Figure 8(b) – Mordialloc Railway Station Catalyst Project Option B)

Given the site's central location, Mordialloc Railway Station provides a foundational project to facilitate the development of the east - west pedestrian linkage from the public transit hub, to the centre and proximate foreshore and residential areas.

At a more detailed level, the Mordialloc Railway Station catalyst project seeks to address issues of safety and amenity, increased public transport patronage and access within the project area. This will be achieved through the provision of a pedestrian focused Station Forecourt, flanked by peripheral activity.

Project elements are designed to ensure a functional and accessible transit station, and an active and amenable pedestrian node.

## Project Elements

Key project elements include:-

- the reformation of the existing Y structured car park area as a new Railway Station Forecourt;
- a consolidated car park between the railway line and the Main Street shops; and
- the provision of increased on-street commuter parking.

### A New Railway Station Forecourt

Two options have been prepared for the Railway Station Forecourt. The determination of a preferred option must consider the needs of commuters, stakeholder requirements, and the contribution of the works to wider economic and urban improvements.

Both options include the following project elements.

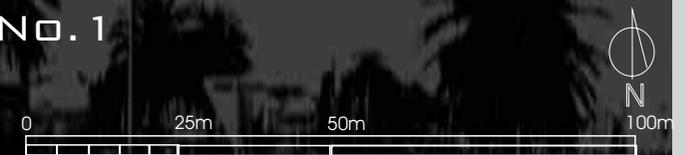
- The reformation of the current Y structured building alignment to a rectangular form. This will enable the forecourt to contain a direct, contiguous building edge between Main Street and the Station. The restructure of the forecourt will facilitate the commercial viability of premises fronting the station, and therefore the levels of activity within the forecourt. A curved edge to retail premises at the street level may provide reference to the former alignment of the court, while extending pedestrian lines of sight at the corner of pedestrian pathways.
- Development along the northern edge of the Railway Station Forecourt should provide a maximum 2 Storey (7.5m) building height to the site's



- 1 LIGHTING AND SIGNAGE TO CAR PARK & STATION AT THE McDONALD STREET ENTRY
- 2 RAILWAY STATION INTERFACE: PROVIDE NEW FENCE AND PLANTING
- 3 INVESTIGATE WITH TRADERS & PROPERTY OWNERS POTENTIAL TO CONSOLIDATE REAR SERVICE AREAS/ FRAGMENTED CAR PARKS, AS A CONSOLIDATED, HIGHLY LANDSCAPED CAR PARK & PEDESTRIAN PATHWAY AT THE REAR OF EXISTING SITES. CARPARK TO INCORPORATE WATER SENSITIVE URBAN DESIGN. PROVIDE SHORT TERM PARKING RESTRICTION FOR SHOPPER ACCESS TO MAIN STREET SHOPS ADJACENT TO NEW REAR SHOPFRONTS. ALL DAY COMMUTER PARKING TO COMPRISE THE REMAINDER OF THE CAR PARK.
- 4 INCLUDE OPEN PEDESTRIAN CONNECTION BETWEEN STATION CAR PARK AREA & MAIN STREET COMMERCIAL STRIP AS PART OF REDEVELOPMENT
- 5 DUAL STORE FRONTAGE: REORIENTATION OF SHOPS TOWARDS REAR CAR PARK
- 6 LIGHTING & AMENITY IMPROVEMENTS TO STATION UNDERPASS
- 7 UP GRADE EXISTING HERITAGE RAILWAY STATION BUILDINGS. INCLUDE ARCHITECTURAL UPLIGHTING OF BUILDINGS
- 8 MORDIALLOC RAILWAY STATION FORECOURT: EXTEND PAVING AREA, & SHARED PEDESTRIAN / VEHICLE ZONE. USE OF BOLLARDS TO DELINEATE PEDESTRIAN SPACES FROM SHARED VEHICLE / PEDESTRIAN AREAS. INCORPORATE PLANTING AND STREET FURNITURE ENCOURAGE ON-STREET ACTIVITY
- 9 BUS STOP (POTENTIAL BUS PULL IN)
- 10 ARTISTIC DIRECTIONAL SIGNAGE AT POINTS OF ARRIVAL (BUS STOPS AND DROP OFF ZONES) & KEY INTERSECTIONS
- 11 PEDESTRIAN PATHWAY TO BEAR STREET. UPGRADE PAVING, PROVIDING LIGHTING AND SIGNAGE
- 12 109 OFF STREET CAR PARKING SPACES (NETT LOSS OF 17 SPACES TO ACCOMMODATE WATER TOWER COURT)
- 13 RETAIN EXISTING HERITAGE BUILDING NORTH OF FORECOURT

- |                                       |                                  |
|---------------------------------------|----------------------------------|
| RAILWAY STATION CATALYST PROJECT AREA | VEHICLE BARRIER / BOLLARD        |
| LANDSCAPE STRIP                       | SHARED PEDESTRIAN / VEHICLE AREA |
| PEDESTRIAN SPACE SYSTEM               | PEDESTRIAN SPACE SYSTEM          |
| BUILDING FOOTPRINT                    | EXISTING BUILDING OUTLINE        |
| RAILWAY LINE & RESERVATION            | PROPERTY BOUNDARY                |
|                                       | DEMOLISH EXISTING BUILDING       |

FIGURE 8(A)  
MORDIALLOC RAILWAY STATION  
OPTION A  
AREA No. 1



*Pride of the Bay*



- 1 LIGHTING AND SIGNAGE TO CAR PARK & STATION AT THE McDONALD STREET ENTRY
- 2 RAILWAY STATION INTERFACE: PROVIDE NEW FENCE AND PLANTING
- 3 INVESTIGATE WITH TRADERS & PROPERTY OWNERS THE POTENTIAL TO CONSOLIDATE REAR SERVICE AREAS & FRAGMENTED CAR PARKS FOR A CONSOLIDATED HIGHLY LANDSCAPED CAR PARK & PEDESTRIAN PATHWAY AT THE REAR OF EXISTING SITES. CARPARK TO INCORPORATE WATER SENSITIVE URBAN DESIGN. PROVIDE SHORT TERM PARKING RESTRICTION FOR SHOPPER ACCESS TO MAIN ROAD SHOPS ADJACENT TO REAR SHOPFRONTS. ALL DAY COMMUTER PARKING TO COMPRISE THE REMAINDER OF THE CAR PARK.
- 4 INCLUDE OPEN PEDESTRIAN CONNECTION BETWEEN STATION CAR PARK AREA & MAIN STREET COMMERCIAL STRIP AS PART OF REDEVELOPMENT
- 5 DUAL STORE FRONTAGE: REORIENTATION OF SHOPS TOWARDS REAR CAR PARK
- 6 LIGHTING & AMENITY IMPROVEMENTS TO STATION UNDERPASS
- 7 UP GRADE EXISTING HERITAGE RAILWAY STATION BUILDINGS. INCLUDE ARCHITECTURAL UPLIGHTING OF BUILDINGS
- 8 MORDIALLOC RAILWAY STATION FORECOURT: EXTEND PAVING AREA, & SHARED PEDESTRIAN / VEHICLE ZONE. USE OF BOLLARDS TO DELINEATE PEDESTRIAN SPACES FROM SHARED VEHICLE / PEDESTRIAN AREAS. INCORPORATE PLANTING AND STREET FURNITURE ENCOURAGE ON-STREET ACTIVITY
- 9 BUS STOP (POTENTIAL BUS PULL IN)
- 10 ARTISTIC DIRECTIONAL SIGNAGE AT POINTS OF ARRIVAL (BUS STOPS AND DROP OFF ZONES) & KEY INTERSECTIONS
- 11 PEDESTRIAN PATHWAY TO BEAR STREET. UPGRADE PAVING, PROVIDING LIGHTING AND SIGNAGE
- 12 109 OFF STREET CAR PARKING SPACES (NETT LOSS OF 17 SPACES TO ACCOMMODATE WATER TOWER COURT)
- 13 DEMOLISH EXISTING BUILDING. ENABLE SIGHTLINES BETWEEN STATION, THE CLOCK TOWER & CENTREWAY. BALANCE OF COMMERCIAL AREA TO BE RECONFIGURED NORTH OF THE STATION SQUARE
- 14 RETAIN EXISTING HERITAGE BUILDING NORTH OF FORECOURT

- |   |  |
|---|--|
| <ul style="list-style-type: none"> <li>■ RAILWAY STATION CATALYST PROJECT AREA</li> <li>■ LANDSCAPE STRIP</li> <li>■ PEDESTRIAN SPACE SYSTEM</li> <li>■ BUILDING FOOTPRINT</li> <li>■ RAILWAY LINE &amp; RESERVATION</li> </ul> | <ul style="list-style-type: none"> <li>■ VEHICLE BARRIER / BOLLARD</li> <li>■ SHARED PEDESTRIAN / VEHICLE AREA</li> <li>■ PEDESTRIAN SPACE SYSTEM</li> <li>■ EXISTING BUILDING OUTLINE</li> <li>■ PROPERTY BOUNDARY</li> <li>■ DEMOLISH EXISTING BUILDING</li> </ul> |
|---|--|

FIGURE 8(B)  
**MORDIALLOC RAILWAY STATION  
 OPTION B  
 AREA NO. 1**



*Pride of the Bay*

street frontage. A recessed 3<sup>rd</sup> storey (11m) maximum height may be accommodated where it is demonstrated sunlight access is provided to the whole of the pedestrian pathway at 2pm at the Winter Solstice.

- The forecourt will require extended paving areas comprising shared pedestrian and vehicular zones.
- Bollards or the like may be used to delineate pedestrian spaces from shared vehicle / pedestrian areas.
- Incorporate planting, street furniture, street vendors and kiosks to encourage on street activity founded on commuter needs.
- The forecourt will rationalise vehicular movements to oneway access to the station and through to a consolidated car park. This will not only improve function, but will increase areas available for pedestrian space and landscaping.
- Taxi Parking Area.
- Bus drop off area (potential bus stop) to be provided close to the intersection of Main Street and the Station Forecourt.
- The provision of directional signage at points of arrival (bus stops & drop off zones) and at the intersection of the new station court and Main Street.
- Improve the interface of the railway station to adjoining areas. Provide new fencing and planting.
- Upgrade the historic Mordialloc Railway buildings and provide architectural up lighting of the building's facades.

*Option A*

- In the reformation of the current Y structured building alignment to a rectangular form, Option A seeks to retain the existing alignment of buildings<sup>15</sup> at the intersection of Main Street and the existing Station Forecourt.
- The building south of the Station Forecourt shall overtime have its existing above veranda / building signage removed, to enable direct lines of sight from the station entrance, towards the clock tower memorial. Additions to this building may accommodate a modest increase in building height, where it can be demonstrated:-
  - existing views of the memorial clock tower from the station are maintained;
  - the existing direct line of site from Centreway to the roof line and gable of the Former Masonic Lodge on Albert Street are maintained.



*View Lines  
Ensure existing views of the former  
Masonic Lodge from Centreway, are  
retained in the redevelopment of site's  
south of the Station Forecourt*

<sup>15</sup> No.503 & No.507 Main Street, Mordialloc

### Option B

- In the reformation of the current Y structured building alignment to a rectangular form, Option B shifts the alignment and entrance to the Station Forecourt to enable:-
  - an unobstructed line of sight between the memorial clock tower, within the Main Street median, and the historic Station building;
  - sightlines between Centreway and the Station building.
- Demolish and redevelop the existing building south of the Station Forecourt. The new building sited further south of the Station Forecourt should not exceed 2 storeys and should ensure existing direct line of site from Centreway to the roof line and gable of the Former Masonic Lodge on Albert Street is maintained.
- Reconfigure an amounting commercial area north of the Station Forecourt. The development north of the forecourt should:-
  - be designed in a contemporary fashion, that relates to the existing heritage building, with reference to its composition, architectural rhythm, proportions, fenestration, building form, materials, texture and colour;
  - retain and reference the original veranda of the existing heritage place.
- Provide a small landscaped seating area south of the existing heritage building at the intersection of Main Street and the Station Forecourt. A new palm will assist to facilitate a focus for this area.



#### Station Forecourt

*The reformation of the Station Forecourt, should extend pedestrian pathways, pick up and drop off points, whilst enabling vehicular movement through the precinct.*

### Consolidated Retail and Commuter Parking Area

- Improved function and accessibility of parking west, of the railway line, increasing the car park capacity from 72 to approximately 94 car parking spaces.
- Seek cooperation with traders and property owners, to consolidate the rear service areas and fragmented car parks (west of the railway line) to accommodate a consolidated, highly amenable and safe car park area.
- Accommodate a balance of both long and short term parking. Provide short term parking restrictions for shopper access to Main Street shops adjacent to new shop fronts. All day commuter parking to comprise the remainder of the car park. The location of commuter parking within direct walking proximity of retail premises will foster economic activity within the Mordialloc commercial area, by facilitating linked trips and increased pedestrian activity.
- Construct a paved pedestrian pathway along rear of shops. Reorientate shops and commercial premises to enable dual store frontages to Main Street (with access to upper floor residential / office use) and new



#### Consolidated Retail & Commuter Parking

*Improve the usability & function of existing fragmented car parking areas between the railway line and rear of shops fronting Main Street.*

consolidated rear car park. Enable limited service vehicle access to shops along paved pedestrian pathway.

- Incorporate canopy tree planting and water sensitive urban design within car parking area.

### Other

- Within the existing off-street commuter car park east of the railway line, reconfigure the car park layout to enable improved pedestrian linkage to Albert Street from the Station building, and to enable the construction of a landscaped feature court to mark the presence of the regionally significant Railway Water Tower(1910). This will result in an approximate nett loss of 18 spaces (Currently 127 spaces, proposed 109 spaces).
- Works within Albert Street to provide 32 angle car parking spaces (increase from current provision of 10 spaces) to accommodate additional commuter parking. Works should further reinforce the central pedestrian crossing point and bus pull in shelters.
- Reinforce the landscape qualities of Albert Street, between Bear and McDonald Street through canopy tree planting.
- Enhance and conduct improvements to the stations pedestrian underpass. This may include feature lighting to architectural features of the underpass.
- Pending redevelopment opportunities, investigate the provision of an open pedestrian connection between the station car park area and the Main Street commercial area, midpoint between McDonald Street and the Station Forecourt. Ensure opportunities for passive surveillance are provided.
- Provide a bus pull in area or potential bus stop. This may in the short term provide a drop off / pick up function, but may also form a bus stop in the longer term. Place the bus stop and waiting areas in a central location close to shops and the intersection where activities can be observed.
- Amenity improvements to the pedestrian path to Bear Street include:-
  - prune vegetation to improve lines of sight from Bear Street to the Station Forecourt;
  - repave;
  - provide feature up lighting of established trees, west of the pathway; and
  - the provision of sheltered commuter bike rack and locker facilities.

- Provide sheltered cycle racks and locker facilities close to station entrances. Design-in facilities for bike parking in undertaking streetscape and landscape works within the precinct.





# 2 Centreway Gateway

## Context

At present the vehicular thoroughfare between the the foreshore and the Centre's commercial core along the Centreway, presents an incoherent and poorly maintained streetscape.



Centreway  
Poor pavement quality, an extensive road space, overhead wires and extensive car parks, detract from view lines from Main Street, towards the foreshore and Bay.

Within the public realm damaged footpaths, frequent and disused driveways, poor landscape quality, an unattractive bus shelter and dominance of overhead wires, contribute to a cluttered and unmemorable street. Centreway is further fragmented by a discontinuous built form edge to the street: extensive open car parks, inconsistent building setbacks, and extensive areas of inactive building edges. This results in Centreway, not only being an unattractive pedestrian space, but a perceptually unsafe environment.

Centreway provides a relatively wide, and observably underutilised two way street, accessed from Beach Road and Main Street. From Centreway vehicles access a seperated car park area to the north (at the rear of properties fronting Main Street), and to the south (Safeway's car park). Observably, both off street car park areas comprise relatively high utilisation rates, during retail peak, with a low level of utilisation outside of these hours. The limited availble parking along Centreway, contains low utilisation rates. Service vehicles access the Safeways loading area through the safeways car park via Centreway from both Main Street and Beach Road.

Centreway comprises of a mix of uses, including car parking, light industry and residences. More recently a number of restaurant and café's have located within the street, and development approvals have been issued for a number of new residential developments, which will benefit from proximity to both the commercial centre and the foreshore area.

Centreway encompasses a series of valued characteristics, in particular:-

- proximity to core retail activities, the foreshore and Mordialloc Railway Station;
- the street's alignment provides opportunity to create a sheltered pedestrian pathway from the winter's south and south-westerly winds;
- walking distance to the railway station and bus stops;
- providing a notable entry to Centreway (and the Mordialloc commercial area) from Beach Road, the historic former Pasadena Café & Dance Hall (1930), now containing a restaurant and cafe;



Centreway  
Centreway is provided with visual focal points to the east comprising the Masonic Hall and the Clock Tower, framed by the Main Street Palms.

- recent developments including:-
  - a residential development at No.211 Beach Road & No.7 Centreway presently under construction; and
  - the refurbishment and fitout of the Former Pasadena Café as a restaurant and bar, and No.9 Centreway as a gallery café.
- directional clarity for pedestrians is provided through direct sightlines between the Main Street commercial area and foreshore, in particular:-
  - the historic Clock Tower Memorial (Main Street Median), the roofline to the former Masonic Hall (Albert Street) framed by the canary island palm trees provide an outstanding focal point for pedestrians walking east along Centreway,
  - the topographical fall towards the foreshore enables clear views towards Port Phillip Bay, Windows by the bay and the octagonal public shelter within the foreshore reserve.

## *Project Description*

(Refer Figure 9 – Centreway Catalyst Project)

An identifiable gateway to Mordialloc from Beach Road, is envisaged at the Centreway, Beach Road intersection.

The Centreway catalyst project is integral in aiding the development of the east-west pedestrian linkage. It will facilitate a link between the Main Street commercial core and the foreshore area. New works within Centreway will aid the formation of a pedestrian priority zone: defined pedestrian crossing points, detailed works and landscaping will provide an amenable and stimulating space.

Centreway will contain a highly active mixed use street environment, building on the recently established tenancies, including the family orientated gallery and café premises. Centreway will accommodate a range of active uses, which ‘spill out’ onto the street.

Traffic and parking elements are designed to increase the overall number of vehicle parking spaces for the precinct, while facilitating vehicular access to core retail activities within the centre.

## *Project Elements*

The Centreway catalyst project comprises of the following key project elements:-

- Centreway streetscape works;
- the parking street and off-street parking areas;
- a common frontage & active street edge.



- 1** MIXED USE RESIDENTIAL DEVELOPMENT
  - 2** MAINTAIN RESIDENTIAL EDGE
  - 3** VEHICULAR ACCESS TO UPPER LEVEL CAR PARK STRUCTURE FROM OWEN STREET
  - 4** VEHICULAR ACCESS TO GRADE LEVEL CAR PARKING STRUCTURE FROM CENTREWAY
  - 5** CAR PARKING DECK  
GROUND LEVEL (ACCESS FROM CENTREWAY @ GRADE): 58 CAR PARKING SPACES  
1ST LEVEL(RAMP UP ACCESS FROM OWEN STREET): 60 CAR PARKING SPACES (APPROX.)  
POTENTIAL 2ND LEVEL (ACCESSED FROM OWEN STREET ENTRY ONLY) : 52 CAR PARKING SPACES (APPROX.)  
TOTAL : 171 APPROX.(CURRENTLY PROVIDES 42 CAR PARKING SPACES)
  - 6** PEDESTRIAN ACCESS TO CAR PARK STRUCTURE
  - 7** CONSOLIDATE ENTRY & EXIT POINT TO SAFEWAYS CAR PARK
  - 8** ENCOURAGE MIXED USE COMMERCIAL / RESIDENTIAL DEVELOPMENT WITHIN CENTREWAY & PROXIMATE BEACH ROAD PROPERTIES
  - 9** BUS STOP (TRIAL AS STOP, TO DISCOURAGE DRIVER BREAKS IN CENTREWAY. MAY ACCOMMODATE POTENTIAL BUS PULL IN).  
INCORPORATE STREET FURNITURE, AT PEDESTRIAN CROSSING POINTS AND BUS STOPS
  - 10** RECONFIGURE SAFEWAYS CAR PARK SPACES ( PROPOSED 50 CAR PARKING SPACES. CURRENTLY PROVIDES 84 CAR PARKING SPACES). INCORPORATE SUBSTANTIAL LANDSCAPE IMPROVEMENT, INCLUDING USE OF CANOPY TREES & WATER SENSITIVE URBAN DESIGN
  - 11** REDEVELOP CAR PARK AREA WEST OF SAFEWAYS FOR COMMERCIAL DEVELOPMENT  
INCORPORATING GROUND FLOOR SPECIALTY RETAIL. ENCOURAGE UPPER FLOOR COMMERCIAL ACTIVITY.
  - 12** ENCOURAGE IMPROVEMENTS TO SAFEWAYS FACADE & PEDESTRIAN CONNECTIONS TO MAIN STREET & NEW COMMERCIAL DEVELOPMENT AT WESTERN EDGE OF CAR PARK.
  - 13** URBAN DESIGN TREATMENT OF CENTREWAY GATEWAY. IMPROVE PEDESTRIAN LINK BETWEEN THE FORESHORE MORDIALLOC COMMERCIAL CENTRE.  
REALIGNMENT OF PEDESTRIAN CROSSING & TACTILE SURFACE TO FACILITATE PEDESTRIAN CROSSING POINT AT BEACH ROAD / CENTREWAY INTERSECTION. ON STREET DINNING & ACTIVE SHOP FRONTS TO FACE CENTREWAY AT GROUND LEVEL.
- CENTREWAY URBAN DESIGN IMPROVEMENTS  
 UNDERGROUND POWERLINES  
 REPLACE STREET LIGHTING THAT REINFORCES THE PEDESTRIAN SCALE AND SENSE OF ENCLOSURE WITHIN CENTREWAY.  
 PAVING UPGRADE TO INCLUDING BANDING (ART INSETS) 15 - 12M APART  
 CENTREWAY ON STREET CAR PARKING SPACES : 33 (CURRENTLY 19) INCORPORATE SUBSTANTIAL LANDSCAPE IMPROVEMENT, INCLUDING USE OF CANOPY TREES & WATER SENSITIVE URBAN DESIGN
- CENTREWAY CATALYST PROJECT AREA
  - LANDSCAPE STRIP
  - PEDESTRIAN SPACE SYSTEM
  - BUILDING FOOTPRINT
  - PERMEABLE ART FEATURE TO CAR PARK FACADE
  - VEHICLE BARRIER / BOLLARD
  - CAR PARK (UPPER OR LOWER LEVEL)
  - EXISTING BUILDING OUTLINE

FIGURE 9  
**CENTREWAY  
 AREA No.2**

*Pride of the Bay*

## Centreway Streetscape Works

A central project element in facilitating the east-west pedestrian connection, the southern boundary of Centreway will accommodate an extended pedestrian pathway. The principal pathway on the southern side of Centreway will:-

- comprise a pathway where a contiguous building edge provides shelter from prevailing winter winds;
- will attain solar access to the pathway during the winter solstice;
- remove existing nature strips, and will be replaced with vegetated swales treating stormwater run off, and recurrent landscape bump-outs to enable the planting of canopy trees;
- be reinforced, where the pathway interfaces the open Safeways car park area. Landscape detail adjacent to the path may include vegetated swales and the continuation of canopy tree planting, and will reinforce the sense of movement and east – west transition.

A secondary pathway will be provided along the northern edge of the Street.

Additional elements forming part of the Centreway Streetscape works include:-

- construct urban design treatments at the gateway (intersection of Centreway and Beach Road) to improve the perceptions of a commercial presence for passing traffic, including:-
  - reinforcing the pedestrian point of entry to Mordialloc through the realignment of the pedestrian crossing and provision of tactile surfaces to facilitate pedestrian access at this intersection;
  - encouraging an active street frontage to address Centreway at street level at this intersection. The extension of the pavement at this intersection may accommodate landscape and public art, bike racks and furniture and should be directed towards enabling on-street dining and commercial activity, directly visible to passing traffic along Beach Road.
- Repave pedestrian pathways and reconstruct kerbs, landscape strips and street tree planting.
- Reinforce a sense of movement and transition along Centreway through the provision of banding or pavement insets (12 – 15 metre spacing) on the floorscape. Points of arrival and departure from the street, or pedestrian crossings, should be further reinforced through banding or pavement insets.

The distinction and identity of Centreway could be further strengthened through the design of the insets or banding that provide a historical or coastal association, or that treat the floorspace, banding or insets as public art.



### Centreway Gateway

Provision of on street activities (dining and displays), public furniture and bike racks visible from Beach Road traffic, will enable the Centreway Beach Road intersection to form an important point of entry to the commercial area.



### Pavement Detail

Pavement insets within the Queen Street Mall, Brisbane, facilitate a sense of transition, and provide visual interest within the streetscape.

- Replace street lighting to reinforce the scale of development within Centreway. Ensure street lighting is designed for pedestrian scale, with particular care given to illuminate pedestrian crossings and intersections.
- Undergrounding the powerlines.



Centreway : Parking :  
The provision of 90° angle parking along the street will spread car park access to commercial premises along the length of Centreway

### The Parking Street and Off-Street Parking Areas

Central to creating a stimulating street environment, convenient and accessible car parks are essential to ensure the precinct is well utilised and commercially viable. Centreway provides a challenge to incorporate parking within the precinct, whilst ensuring it ceases to dominate the street. Project elements relating to parking within Centreway include:-

- the reconstruction of Centreway to provide 90° angle parking along the street (increases the number of spaces available on Centreway from 19 to approximately 30). This will spread car park access to commercial premises along the street;
- require new developments south of Centreway to provide off-street parking areas accessed from the laneway at the rear of these premises. Crossovers to access these sites from Centreway should be discouraged.
- Reconfigure the Safeways car park (proposed approximately 50 spaces, currently 84 car parking spaces provided), including the following elements:-
  - redevelop the car park area west of Safeways, as a commercial development enclosing the reconfigured car park area;
  - consolidate entry and exit points to the Safeways car park to Centreway;
  - encourage minimal conflict between service vehicle loading and general car park circulation & ensure sufficient vehicle turning movements for semi-trailer access to the Safeways loading area; and
  - incorporate substantial improvements to pedestrian amenity, delineate pathways and incorporate landscape improvements including shade canopy trees and water sensitive urban design.
- Construct a car park deck north of Centreway. This will require the acquisition of properties adjoining the existing parking area to the south-west, and incorporates the laneway, and a partial component of properties facing Main Street . The low level car parking deck will comprise :-
  - an at grade ground level car park area, provided with vehicular access from Centreway only. This will provide approximately 58 car parking spaces;

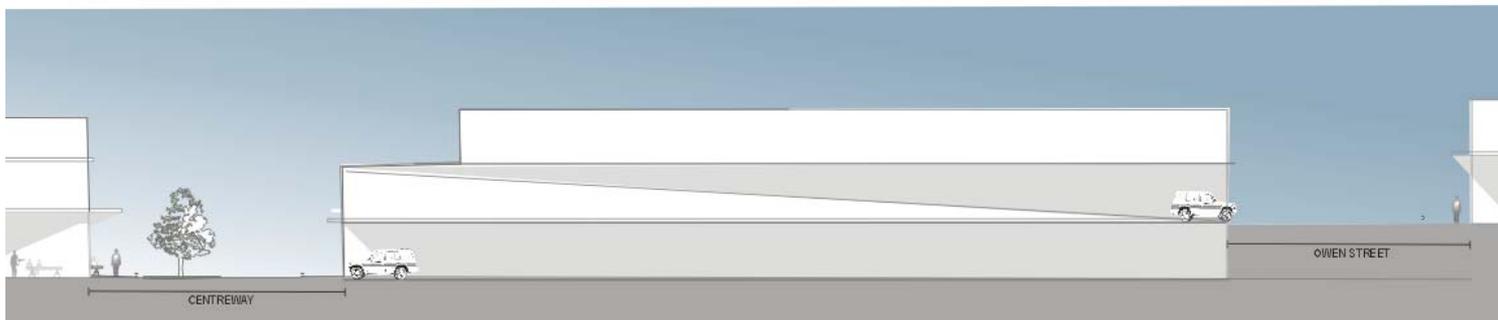
- a first level deck which will present as an at grade car park to Owen Street. The first level will be provided with vehicular access from Owen Street only. Approximately 60 car parking spaces may be accommodated at the first level;
- The potential for a second level (access from Owen Street only, ramp to upper level) must acknowledge the need to set back a third level to the Centreway streetscape (to enable solar access to the street). A potential 2<sup>nd</sup> level may accommodate approximately 52 car parking spaces.

The car park area north of Centreway currently provides 42 car parking spaces. The construction of a two level deck will accommodate approximately of 119 car parking spaces (approximately 77 nett additional spaces); the option for a third level will accommodate approximately 171 car parking spaces (approximately 129 nett additional car parking spaces).

Direct pedestrian access will be provided to the deck structure from Safeways, via a designated pedestrian pathway and crossing point.

It is integral the deck façade to adjoining streets (Centreway and Owen Street) does not present a black wall, or open deck, but rather forms a stimulating building within the context of the street. As an active retail frontage to the street (as a component of the deck) is not achievable given the limited site area:-

- incorporate the car park structure into the street through the use of permeable art work, textured walling or grill artistic motifs or the like;
- ensure the structure provides chance opportunities for passive observation to ensure user safety, particularly at night; and
- incorporate lighting into the design by up lighting building features and ensure adequate levels of light around car park areas.



Indicative Elevation:  
Centreway, Owen Street  
Car Park Deck.

In summary, collectively the above *Parking Street and Off-Street Parking Area* project elements will result in approximately the following number of publicly accessible off street car parking spaces:-

- Option for a 2 level deck  
Total car parking spaces within Centreway project area : 202 approx.  
Nett new car parking spaces within project area : 57 approx.

- Option for a 3 level deck  
Total car parking spaces within Centreway project area : 254 approx.  
Nett new car parking spaces within project area : 109 approx.

### A Common Building Frontage & Active Street Edge

Building heights and form should ensure an enclosed street environment that creates continuity and provides a sense of enclosure, whilst enabling sunlight access to the southern side of the street throughout the year. To aid the creation of an enclosed, but active Centreway, projects include:-

- the redevelopment of the car park area west of Safeways, as a commercial development incorporating ground floor specialty retail (ancillary to Safeways core retail function) and upper level commercial or residential use. Ensure ground floor uses provide expansive fenestrated shop fronts; the encouragement of a café / deli spilling activity onto Centreway should be facilitated.
- Encourage sidewalls of the buildings at the intersection of Centreway and Main Street to open up onto Centreway. This may allow the incorporation of additional tenancies facing Centreway, or could improve commercial viability of these premises by increasing sightlines to available stock within the store. It is essential these alterations sensitively address any identifiable heritage elements of these structures.

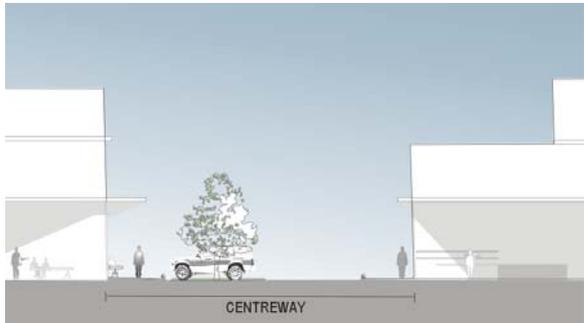
### Other

- The removal of the existing bus shelter and bus parking area to improve sightlines for vehicles exiting the Safeways parking area. Reinststate a permeable bus shelter adjacent to an active store frontage. Provide sufficient area within the design to enable a bus pull-in, however trial the provision of a bus-stop only to discourage driver breaks, (and therefore extended periods where there is a loss of sightlines for commercial premises);
- Architectural improvements to the Safeways Façade; and
- North of Safeways, improve pedestrian connections to Main Street and the new commercial development at the western edge of the Safeways Car Park.

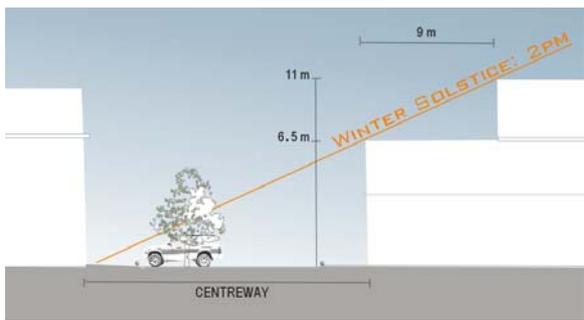
## *Design Guidelines*

Substantial opportunities are provided for infill development along Centreway. Guidelines for future development to ensure the integrity of the street as a principal pedestrian pathway include:-

- Provide a zero lot alignment to the street frontage, and encourage buildings to physically address the street through entrances and fenestration.
- Rezone properties within Centreway and proximate Beach Road properties to a Mixed Use zone. New development should seek to accommodate active commercial premises at the ground floor level. Inactive (such as residential and office) uses should only be contained within upper floor levels of development.
- Provide pedestrian protection from the elements through the provision of contiguous awnings, verandas and canopies.
- Ensure a stimulating street environment by providing an active street environment to Centreway. Encourage commercial and retail activities on Centreway including on-street dining or the display of goods or services.

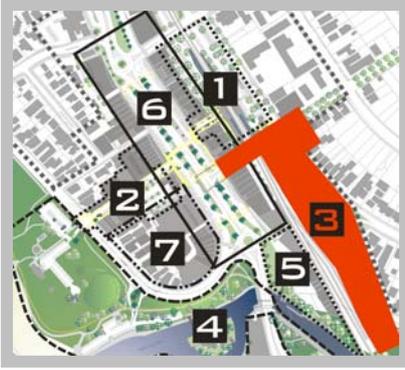


*Centreway Section:  
The provision of an active street frontages to contribute to pedestrian engagement at the street level, and improve perceptions of safety within the street.*



*Centreway Section:  
Ensure sunlight access to the southern side of Centreway.*

- Ensure sunlight access to the Principal Pedestrian Pathway at 2pm at the Winter Solstice. Buildings on the northern side of Centreway should:-
  - provide a maximum building height of 6.5 metres to the street;
  - a third storey providing a maximum building height of 11 metres may be accommodated where it is recessed 9 metres from the street frontage. This setback may be reduced where the building height is decreased, and where it can achieve sunlight access to the street as described above;
  - setbacks may be used to accommodate balconies and open air decks;
  - Balustrades and structures enabling sunlight permeability may encroach within the above building envelope. Setbacks may be used to accommodate balconies and open air decks.



# 3

## Timber Yard Redevelopment

### Context

The Mordialloc Activity Centre provides a major commercial focus for residents of Mordialloc and surrounding areas with a retail catchment comprising approximately 28,400 residents<sup>16</sup>. The centre is a weekly shopping destination for many households, but is constrained by difficult access to the established core retail activities, and limited variety of retail tenancies. Although select reinvestments have occurred within restaurant and café tenancies, the centre contains large areas where disinvestment into the building stock is evident, particularly along Main Street east.

The Timber Yard Redevelopment project comprises a lengthy site on the east side of the railway line, extending from Bear Street through to the George Woods Reserve. Comprising a limited number of ownerships, the site currently contains a series of peripheral commercial premises, a timber yard, commuter parking, rail shunting and service yards.

The site is located along Albert Street and Park Street which forms one of the primary vehicular entries to Mordialloc. The site is provided with opportune access to an expanding retail catchment with a growth in residential development to the east of Mordialloc.

Given the proximity of the northern edge of the site to the Main Street commercial area, the Timber Yard site provides opportunity to accommodate a substantial mixed use development. Notwithstanding this, the Timber Yard site comprises a series of unique opportunities and constraints for future development, in particular:-

- the ongoing provision of commuter parking and rail facilities in this location is integral in ensuring the patronage and maintenance of the railway;
- the proximity of the rail line, and the need to accommodate a reservation for a future third railway line;
- the opportunity to gain distant views of Port Phillip Bay, along the southern edge of the site;

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<sup>16</sup> GHD reference to primary trade catchment, based on SGS 2000 data. Includes 2001 ABS update.

- intersections along Park and Albert Streets, and the Park Street arc, contain relatively high traffic accidents;
- proximity to and views of the open space network adjoining the Mordialloc Creek.

## *Project Description*

(Refer Figure 10 – Timber Yard Redevelopment Catalyst Project)

The Timber Yard site provides opportunity to draw the commercial core east across Main Street and the railway line, through the provision of a new retail attractor. The redevelopment will form the eastern commercial node and gateway to the Mordialloc commercial centre. The site's location benefits from negating the need for users to cross the railway line and Main Street, thereby avoiding additional congestion at these existing traffic pressure points.

Accompanying a new retail development, streetscape improvements including the provision of on-street parking along Bear Street will ensure the new retail development provides a physical link to the established retail tenancies west of the railway line. This will enable the spread of economic benefit through the centre.

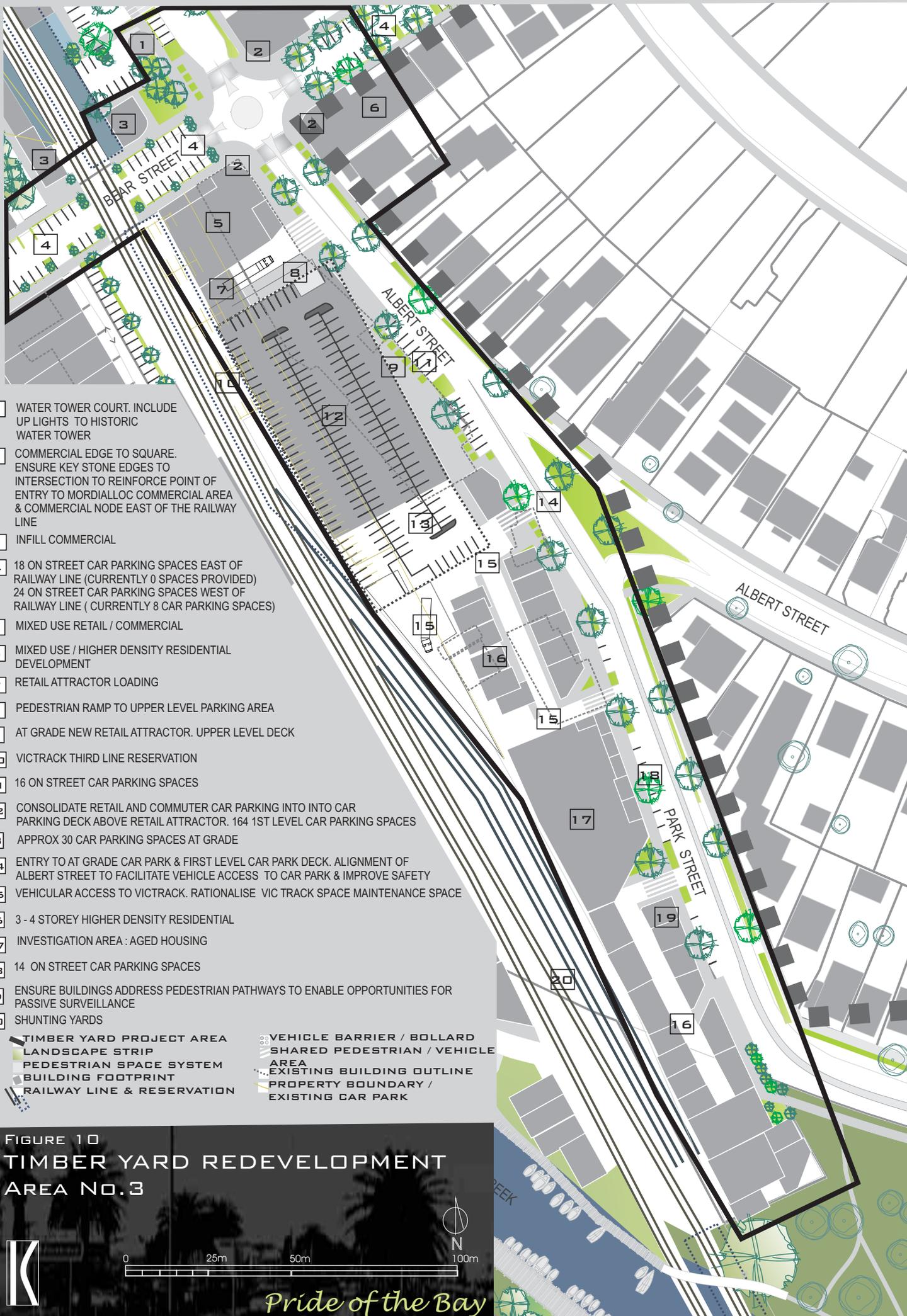
Linked trips (retail and commuter trips) will be facilitated through the co-location of commuter and retail parking within an upper level deck, above the new attractor.

Levels of amenity and safety for established residential properties east of Park Street will significantly improve by the accommodation of residential and potentially aged housing on the former Timber Yard site. The site's walking distance to the railway station, commercial centre and adjoining open space network, provides an ideal location for an increased resident population to contribute to the vitality of Mordialloc.

## *Project Elements*

The Timber Yard catalyst project comprises of the following key project elements:-

- New Retail Attractor
- Bear & Albert Street Gateway
- Mixed Use Higher Density Residential



- 1 WATER TOWER COURT. INCLUDE UP LIGHTS TO HISTORIC WATER TOWER
- 2 COMMERCIAL EDGE TO SQUARE. ENSURE KEY STONE EDGES TO INTERSECTION TO REINFORCE POINT OF ENTRY TO MORDIALLOC COMMERCIAL AREA & COMMERCIAL NODE EAST OF THE RAILWAY LINE
- 3 INFILL COMMERCIAL
- 4 18 ON STREET CAR PARKING SPACES EAST OF RAILWAY LINE (CURRENTLY 0 SPACES PROVIDED) 24 ON STREET CAR PARKING SPACES WEST OF RAILWAY LINE ( CURRENTLY 8 CAR PARKING SPACES)
- 5 MIXED USE RETAIL / COMMERCIAL
- 6 MIXED USE / HIGHER DENSITY RESIDENTIAL DEVELOPMENT
- 7 RETAIL ATTRACTOR LOADING
- 8 PEDESTRIAN RAMP TO UPPER LEVEL PARKING AREA
- 9 AT GRADE NEW RETAIL ATTRACTOR. UPPER LEVEL DECK
- 10 VICTRACK THIRD LINE RESERVATION
- 11 16 ON STREET CAR PARKING SPACES
- 12 CONSOLIDATE RETAIL AND COMMUTER CAR PARKING INTO INTO CAR PARKING DECK ABOVE RETAIL ATTRACTOR. 164 1ST LEVEL CAR PARKING SPACES
- 13 APPROX 30 CAR PARKING SPACES AT GRADE
- 14 ENTRY TO AT GRADE CAR PARK & FIRST LEVEL CAR PARK DECK. ALIGNMENT OF ALBERT STREET TO FACILITATE VEHICLE ACCESS TO CAR PARK & IMPROVE SAFETY
- 15 VEHICULAR ACCESS TO VICTRACK. RATIONALISE VIC TRACK SPACE MAINTENANCE SPACE
- 16 3 - 4 STOREY HIGHER DENSITY RESIDENTIAL
- 17 INVESTIGATION AREA : AGED HOUSING
- 18 14 ON STREET CAR PARKING SPACES
- 19 ENSURE BUILDINGS ADDRESS PEDESTRIAN PATHWAYS TO ENABLE OPPORTUNITIES FOR PASSIVE SURVEILLANCE
- 20 SHUNTING YARDS

- TIMBER YARD PROJECT AREA
- LANDSCAPE STRIP
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- RAILWAY LINE & RESERVATION
- VEHICLE BARRIER / BOLLARD
- SHARED PEDESTRIAN / VEHICLE AREA
- EXISTING BUILDING OUTLINE
- PROPERTY BOUNDARY / EXISTING CAR PARK

**FIGURE 10**  
**TIMBER YARD REDEVELOPMENT**  
**AREA NO.3**

0 25m 50m 100m

N

Pride of the Bay



Timber Yard Redevelopment

The provision of a street level new retail attractor and upper level car park deck accommodating commuter and retail parking.

New Retail Attractor

Projected retail growth in expenditure<sup>17</sup> suggests Mordialloc’s ability to accommodate a nett additional floorspace area of around 3,000m<sup>2</sup> by 2011. The accommodation of a new retail attractor and series of speciality stores will enable Mordialloc to consolidate and strengthen the centre’s convenience and weekly shopping role. Further the site’s proximity to the established commercial functions along Main Street provide opportunity to distribute shared economic benefit to existing tenancies, by facilitating linked commercial trips to the commercial core, west across Main Street and the railway line towards the existing Safeway’s site. It is intended the new retail attractor will further support, rather than contend with established retail functions within the centre. It is therefore integral pedestrian links between the new facility and established retail areas be provided.

Elements of the new retail attractor redevelopment comprise:-

- seeking a confirmed agreement between the owners of the sites required for consolidation, to accommodate the redevelopment. Site’s including the former Timebery site, and the VicTrack site (partial);
- the accommodation of a street level new retail attractor, accommodating approximately and additional floor space area of 2000-3000m<sup>2</sup>;
- the provision of a consolidated car park accommodating demands generated by the new retail attractor and the existing commuter spaces currently provided on the VicTrack site. New car park areas will comprise:-
  - a car park located above the new retail attractor, provided with ramp access from Albert Street. The 1<sup>st</sup> level car park area will accommodate approximately 164 car parking spaces. It is envisaged this area will contain the existing commuter parking (currently approximately 32 car parking spaces are provided by VicTrack west of the Timber Yard site), which must be incorporated within any development of the VicTrack site. Pedestrian access to the Supermarket and to Albert Street (and station area) is via a pedestrian ramp north of the new retail attractor.
  - An undercover car park at grade accommodating approximately 30 car parking spaces, south of the new retail attractor. Vehicular access to this car park is from Albert Street (shared with access to the ramp).
  - Provision of approximately 16 on-street car parking spaces along Albert Street, to enable high turn over convenience shopping.

<sup>17</sup> Based on, ‘Retail Floor Space Potential 2011’ contained within the Draft *Pride of the Bay*, GHD, March 2002. A retail needs analysis should form part of future development applications on this site.

On the whole the redevelopment may accommodate approximately 210 car parking spaces. In accounting for the car parking demand generated by a new 2000-3000m<sup>2</sup> retail facility (approximately 120 spaces), the development will provide an additional 58 (approximate) commuter parking spaces.

- access to the 1<sup>st</sup> level car park area will require the partial realignment of Albert Street / Park Street to increase sightlines from the vehicular point of entry / exit. Not only will the realignment of the street improve vehicular safety and access, but it will also open up vistas north along Albert and Park Streets towards the former Masonic Hall;
- the possible closure of Albert Street, east of Park Street would simplify this intersection and minimise opportunities for through traffic within local residential streets;
- at the south-west corner of the intersection of Bear Street and Albert Street, provide a mixed use commercial development. Tenancies to the street level may accommodate specialty stores ancillary to the new retail attractor. Upper floor levels may accommodate office or residential development. Shared vehicular access with the loading area will enable on-site undercover parking at the rear of these premises;
- access to a loading facility from Albert Street, north of the new attractor;
- provision of a reservation for the future accommodation of a 3<sup>rd</sup> railway line. The proposed design is limited east of the existing railway line by an 8 - 12 metre wide reservation;
- rationalisation of the VicTrack maintenance space is required to enable the construction of a ramp to access the carpark deck. It is proposed to provide heavy vehicle entry to the shunting yards and workspace (area extended to the east) from Albert Street (shared with vehicular access to ramp). An additional exit may be provided through the residential development to the south;
- it is proposed the height of the new retail attractor and car park deck will not exceed an 11 metres building height which is not dissimilar to the current heights contained on this site.



Timber Yard Redevelopment  
New development should not overly exceed existing building heights (11 metres) on the Timber Yard site.

### Bear Street & the Albert Street Gateway

The opportunity to distribute shared economic benefit of a new retail attractor through linked commercial trips, must be aided by direct physical pedestrian links between the new development and the existing commercial core. The Bear and Albert Street Gateway, and associated streetscape works within Bear Street will create a physical link to achieve this objective. Project elements include:-

- the creation of an identifiable node at the eastern point of entry into Mordialloc. Buildings at the intersection of Bear and Albert Streets should provide a commercial edge to the square and where possible

accommodate on street activity. With the exception of the Water Tower Court (north-west of the intersection) redevelopment adjacent to the intersection must comprise a contiguous two (7.5m maximum building height) storey keystone edge to the intersection;

- consideration of the provision of 90° angle parking along Bear Street. This would comprise:-
  - approximately 18 on-street parking spaces east of the railway line, west of Albert Street (currently no spaces are provided); and
  - approximately 24 on-street parking spaces west of the railway line, east of Main Street (currently 8 spaces are provided).
- reconstruct pedestrian pathways including the railway crossing, provide water sensitive urban design and street tree planting to improve pedestrian amenity and streetscape appearance along Bear Street;
- *Railway Water Tower Court*: north-west of the intersection of Albert and Bear Street construct a landscape court to mark the presence of the remaining, Edwardian Railway Water Tower (1910). This will require the reconfiguration of the southern end of the railway station parking area. This area may in the future include a kiosk addressing both the court and Bear Street providing an interface and level of activity within the court.



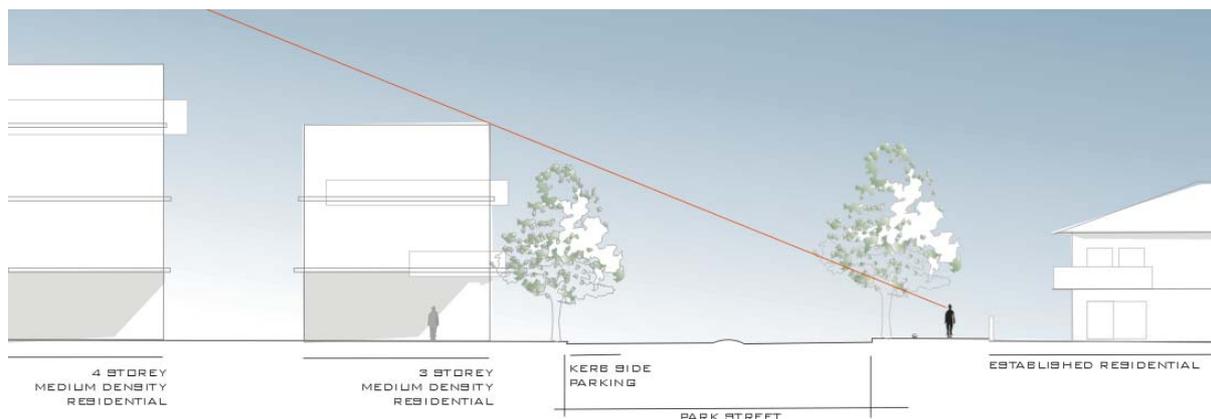
*Park Street*  
The redevelopment of the former Timber Yard site, as residential, will significantly improve the presentation and amenity from the residential edge east of Park Street.

### Mixed Use Higher Density Residential

Key elements of the Mixed Use Higher Density Residential development will comprise:-

- the inclusion of small structured, commercial premises (i.e. small scale retail, café, office, home office at the street level) may contribute to the diversity and levels of activity characteristic of an urban setting;
- a feasibility to determine the need for aged housing to accommodate Mordialloc's generally higher levels of aged persons, compared to MSD and municipal averages;
- new residential development may comprise 3 storeys (11 metres maximum) to the site's street frontage, and may incorporate a recessed 4<sup>th</sup> storey (maximum 14 metres) where it can be demonstrated the fourth storey will not be visible from Park Street;

Indicative Building Heights & Sightline  
New residential development will provide an improved interface between the railway line and established residential areas west of the subject site. 4<sup>th</sup> storey development will not be visible from Park Street.



- a limited number of vehicular access points to Park Street. Access to off street car parking areas may be provided via common access ways within the site;
- the extension of an on street pedestrian pathway on the west side of Park Street to provide access between the new development and adjoining open space areas to the south;
- new residences that address the street through the provision of individual entrance areas to the pathway. Ensure development enables surveillance on the street, incorporating fenestration;
- design features that enable surveillance whilst maintaining privacy, such as a slight level change between the ground floor of the residence and pavement level. Enable projections from the buildings including balconies, bays, porches or colonnades to facilitate the extension of activity and surveillance onto Park Street.





# 4

## Pier, Creek & Foreshore

### Context

The Pier, Mordialloc Creek and the foreshore provide a fundamental point of difference for Mordialloc. The Pier and mouth of the Mordialloc Creek is not only locally identifiable, but is a key landscape feature of the wider metropolitan edge of Port Phillip Bay. The mouth of the Mordialloc Creek is one of only four locations within the wider metropolitan area, where a natural watercourse meets the eastern edge of Port Phillip Bay.

There are only two pedestrian points of access between the Mordialloc commercial area and the foreshore. The lights at the intersection of Centreway and Beach Road, and the lights / crossing at the intersection of Beach Road and Main Street. The pedestrian crossing at the lights to Centreway do not align with the pedestrian pathways, and poor quality treatments (paving, fencing) create a low level of amenity.

The wider foreshore and creek open space network provides a series of pedestrian and cycle pathways. There are a number of interruptions within both networks along the southern edge of the creek and through the Pompei's landing.

Current works within the Peter Scullin Reserve, are directed towards revising car parking, facilitating pedestrian access and improving the usable recreational area within the reserve.

Attenborough Park, provides a substantial underutilised open space resource. The site is currently restricted by barriers to pedestrian movement limiting access to the site (the creek, highway and railway line). Notwithstanding this, the Park contains a notable reference to aboriginal heritage within the indigenous gardens in the north-western reaches of the Park.

Mordialloc Island provides a unique feature centrally located within the project area. The Island is within close proximity of the established Bridge Hotel (Doyle's Restaurant) and provide an important feature of the Mordialloc Creek & foreshore environs. The island contains the Mordialloc Motor Yacht Club and the Mordialloc Sea Scouts. Access to the island is currently limited to members of the above groups: the approach is via a gated step bridge from the Doyles car park, which restricts disabled and public access to the island. The awareness and history of the Island is little known to many visitors to the area.

The Creek is heavily used for boating and mooring facilities. Ceremonial launches of the vessels constructed within the Pompei's Boat Building Yard, and boating meets, provide occasions for communal celebration and interest in the



1923  
Creek & Mordialloc Island  
Courtesy of the Leader Collection



Island Access  
Current access to the Mordialloc Island is via a gated bridge to members of the Sea Scouts and Mordialloc Yacht Club.

maritime activities supported by the creek and bay. The water quality within Mordialloc Creek is challenged by urban and industrial activities within the creeks catchment area. Recent improvements including revegetation, stabilisation of the bed and banks and works to create upstream wetlands have resulted in improvements to the health of the creek. Management and monitoring of the Mordialloc Creek in terms of water quality, hydrology, physical forms (water body, bank and bed condition), saltation, weed invasion, erosion of the creek bank, quantity and quality of waterside vegetation and biodiversity of aquatic life have been identified as key areas for improvement.

## *Project Description*

(Refer Figure 11 – Pier, Creek & Foreshore Catalyst Project)



*Pier Road  
Pier Road adjoining Peter Scullin Reserve  
provides a foreshore visitor destination .*

The improvement of the foreshore, pier and creek environs will include an upgraded and contiguous pathway system, that will provide access through the extensive open space network, to the commercial area. The improvement of these later linkages will reinforce commercial benefits derived by linked pedestrian trips between the central commercial area and the commercial features (including boat hire, restaurants, café's and kiosks) within and adjoining the pier, creek and foreshore.

Improved access & facilities will also enable increased utilisation of the enhanced parkland setting south of Mordialloc Creek within Attenborough Park. A revised carparking arrangements, reduced congestion and improved vehicular access to the beach through the conversion of the tennis club site will enable better access to Peter Scullin Reserve and the adjoining foreshore.

Ongoing monitoring and remedial works to improve water quality will steadily improve the environmental and recreational attributes of Mordialloc Creek.

Mooring, commercial and community recreational boating will continue to be provided within the creek and on Mordialloc Island.

## *Project Elements*

The Pier, Mordialloc Creek and the Foreshore are significant features. Key components of this project area include:-

- Foreshore & Creek Side North;
- Creek Side South; and
- Mordialloc Island Access.



- 1 NEW PATH LINKAGE: FORESHORE TO BEACH ROAD
- 2 CAR PARK EXTENSION: EXISTING TENNIS COURT TO BE REMOVED WITH PROPOSED NEW CAR PARK & GRASSED OPEN SPACE / PICNIC AREA.
- 3 URBAN DESIGN TREATMENT TO CENTREWAY GATEWAY: IMPROVE PEDESTRIAN LINK BETWEEN THE FORESHORE & THE MORDIALLOC COMMERCIAL CENTRE. REALIGN & MARCATE PEDESTRIAN CROSSING TO FACILITATE MOVEMENT AT BEACH ROAD / CENTREWAY INTERSECTION. ON STREET DINNING & ACTIVE SHOP FRONTS TO FACE CENTREWAY AT STREET LEVEL.
- 4 PUBLIC AMENITIES & GAZEBO & PICNIC STRUCTURE. UPGRADE PEDESTRIAN PATHWAY
- 5 WINDOWS BY THE BAY RESTAURANT
- 6 BAND ROTUNDA
- 7 EXISTING KIOSK
- 8 RECONFIGURE PIER ROAD & FORESHORE PARKING
- 9 FLOATING KIOSK STRUCTURES
- 10 MORDIALLOC SAILING CLUB: UPGRADE ACCESS TO SAILING CLUB BUILDING, INCLUDING SIGNAGE & POTENTIAL EXPANSION (NO NETT INCREASE OF BUILDING FOOTPRINT WITHIN THE FORESHORE RESERVE) OF EXISTING CLUBHOUSE FACILITIES.

- 11 EXTEND PEDESTRIAN ACCESS ALONG SOUTHERN BANK OF MORDIALLOC CREEK
- 12 LOADING & RESTRICTED PARKING (SEA SCOUTS & MORDIALLOC MOTOR YATCH CLUB.)
- 13 INVESTIGATE PROVISION OF LOW LEVEL ARCH PEDESTRIAN BRIDGE. CLEARANCE 2M FROM HIGH WATERMARK. ENABLE DISABLED ACCESS ISLAND. INCLUDE ARCHITECTURAL UPLIGHTING.
- 14 PUBLIC OPEN SPACE INCORPORATING PICNIC FACILITIES. INCLUDE HISTORIC INTERPRETIVE FEATURE DETAILING THE CONSTRUCTION & USE OF THE MORDIALLOC ISLAND, & INDIVIDUALS ASSOCIATED WITH THE SITE.
- 15 CLUB ACCESS ONLY
- 16 EXISTING SERVICE CONNECTIONS RETAINED VIA EXISTING BRIDGE. NO PEDESTRIAN ACCESS
- 17 MAINTAIN PUBLIC PEDESTRIAN ACCESS ALONG CREEK SIDE PROMENADE
- 18 DOYLE'S DECK
- 19 BOAT HIRE KIOSK
- 20 PEDESTRIAN BRIDGE: CANTILEVER STRUCTURE FROM HIGHWAY BRIDGE. REINFORCE GATEWAY ENTRY TO MORDIALLOC. ENSURE VIEW LINES TO CREEK IS MAXIMISED. ARCHITECTURAL DETAIL TO RESPOND TO CREEK SETTING & BOATING ACTIVITY
- 21 STEPS TO RIVER LEVEL DECK & PEDESTRIAN UNDERPASS
- 22 INVESTIGATE DESIGN OPTIONS FOR A PEDESTRIAN UNDERPASS BETWEEN ATTENBOROUGH PARK & DOYLES. ENSURE STRUCTURE ENABLES PEDESTRIAN INTERACTION/ EXPERIENCE WITH THE MORDIALLOC CREEK
- 23 ARCHITECTURAL LIGHTING INSTILLATION OF BRIDGE STRUCTURES
- 24 IMPROVE ACCESS TO & PRESENCE OF INDIGENOUS GARDEN
- 25 PEDESTRIAN PROMENADE & BOAT MOORING
- 26 PASSIVE OPEN SPACE / PICNIC FACILITIES UPGRADE
- 27 RATIONALISE VEHICLE ACCESS & CAR PARKING
- 28 JUNIOR SKATE FACILITY

- PIER CREEK & FORESHORE PROJECT AREA
- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- RAILWAY LINE & RESERVATION
- PEDESTRIAN BRIDGE / DECK
- FLOATING KIOSK
- BOAT MOORING

FIGURE 11  
**PIER, CREEK & FORESHORE**  
**AREA No.4**



*Pride of the Bay*

## Foreshore & Creek Side North

- Landscape & amenity improvements are under construction within the Peter Scullin Reserve. Components of staged improvements include:-
  - upgrade pathways linking key facilities along the foreshore;
  - relocation of parking areas over existing tennis courts;
  - improved vehicular access; and
  - the creation of an open space link between the existing playground facilities and the foreshore by the removal and reconfiguration of existing car parking spaces including Pier Road.
- It is envisaged select commercial activities (boat hire, kiosks) remain within existing premises. The northern bank of the creek provides an appropriate location for the accommodation of small scale commercial premises that may locate activity within boats or impermanent floating kiosk structures at the waters edge. Key locations for such premises include at the foot of the proposed pedestrian bridge to Mordialloc Island, or adjacent to the Pier Road car park.
- Urban design treatment to the Centreway gateway to facilitate pedestrian access between the foreshore and the Mordialloc Commercial Centre. Realign and marcate pedestrian crossings to facilitate movement at the Beach Road, Centreway intersection. On-street dining and active store fronts at the western edge of Centreway will assist pedestrian transition into the commercial core.
- Urban design treatment to facilitate pedestrian access between Main Street, the foreshore area, and Bridge Hotel. Marcate pedestrian crossings to facilitate movement, upgrade pedestrian barriers and paving.

## Creek Side South

Increased utilisation of Attenborough Park will be facilitated through increased linkages south of the Creek and enhancements to the parkland setting. Further improving the pedestrian connections proximate to Nepean Highway will facilitate linked commercial trips between the Bridge Hotel and Main Street Commercial activities, and enable improved pedestrian connectivity towards proximate residential areas to the south. Project areas include:-

- construction of cantilever style pedestrian bridge on the east and west sides of the Nepean Highway Bridge. This will comprise a component of works conducted to increase the number of lanes (from two to four) on the existing Nepean Highway Bridge.

The Bridges should maintain the existing clearance of the Nepean Highway Bridge, above the high water mark (2 metres). It's physical form will provide a landmark point of entry, and memorable pedestrian

experience upon entering into Mordialloc. Balustrades, railing and detail should maximise permeable materials, enabling view lines to the creek. The detailed design should reflect environmental and the cultural maritime attributes of the site. Incorporate architectural lighting to guide pedestrian movement and feature architectural elements of the new and existing bridge structures.

- From the Nepean Highway pedestrian bridge, enable access to the creekside promenade and pedestrian underpass on the southern edge of the creek (steps, subject to access review). Significant changes in grade between the highway level path and the creek side promenade present challenges to any design: although restrained, possible options for disabled access should be sought through detailed design.
- Enable a contiguous pedestrian promenade linking Attenborough Park to the mouth of the Mordialloc Creek. Varying in scale, the pathway should be maintained proximate to the southern edge of the Mordialloc Creek.
- The pedestrian underpass, between the Doyles deck and northerly promenade within Attenborough Park is a central component of the pedestrian link on the southern edge of the Mordialloc Creek. It is integral the development of detailed designs concepts, consider as a primary consideration, the quality of the pedestrian experience, in particular, ensuring pedestrian interaction and exposure to the creek. This may be achieved by the construction of an underpass which is partially sunk, but which provides a perceived floating or like experience.
- Within Attenborough Park works include:-
  - a new dual pedestrian, bicycle boardwalk along the southern edge of the Mordialloc Creek.
  - Upgrade boat mooring facilities;
  - the provision of passive open space areas and upgraded picnic facilities;
  - rationalised vehicular access and car parking;
  - construction of junior skate facility;
  - improve the presence and access to the indigenous gardens at the northern edge of Attenborough Park. Provide a new garden area with emphasis on local indigenous coastal planting;
  - Investigate a pedestrian level crossing, linking beach side residential areas with Attenborough Park;
  - construct new public amenities proximate to new picnic and visitor facilities.
- Promote increased community use of the foreshore area around the sailing club. Improve pedestrian and vehicular access to the foreshore and sailing club users.
- The Doyle's car park forms the southerly commercial point of entry into the Mordialloc commercial area. Subject to a detailed urban design review, it is envisaged this site may accommodate a redevelopment comprising serviced accommodation.

- As a long term exploratory project undertake an economic, social and environmental review of the potential to contain the Nepean Highway within an underpass below the creek to create a mast boat destination within the Pompei's landing and Attenborough Park area. Examine potentials for staged works, the wider economic benefits of accommodating a short / long term yachting destination within proximity of the centre, potential environmental issues, and pedestrian access across the creek under this scenario.

### Mordialloc Island

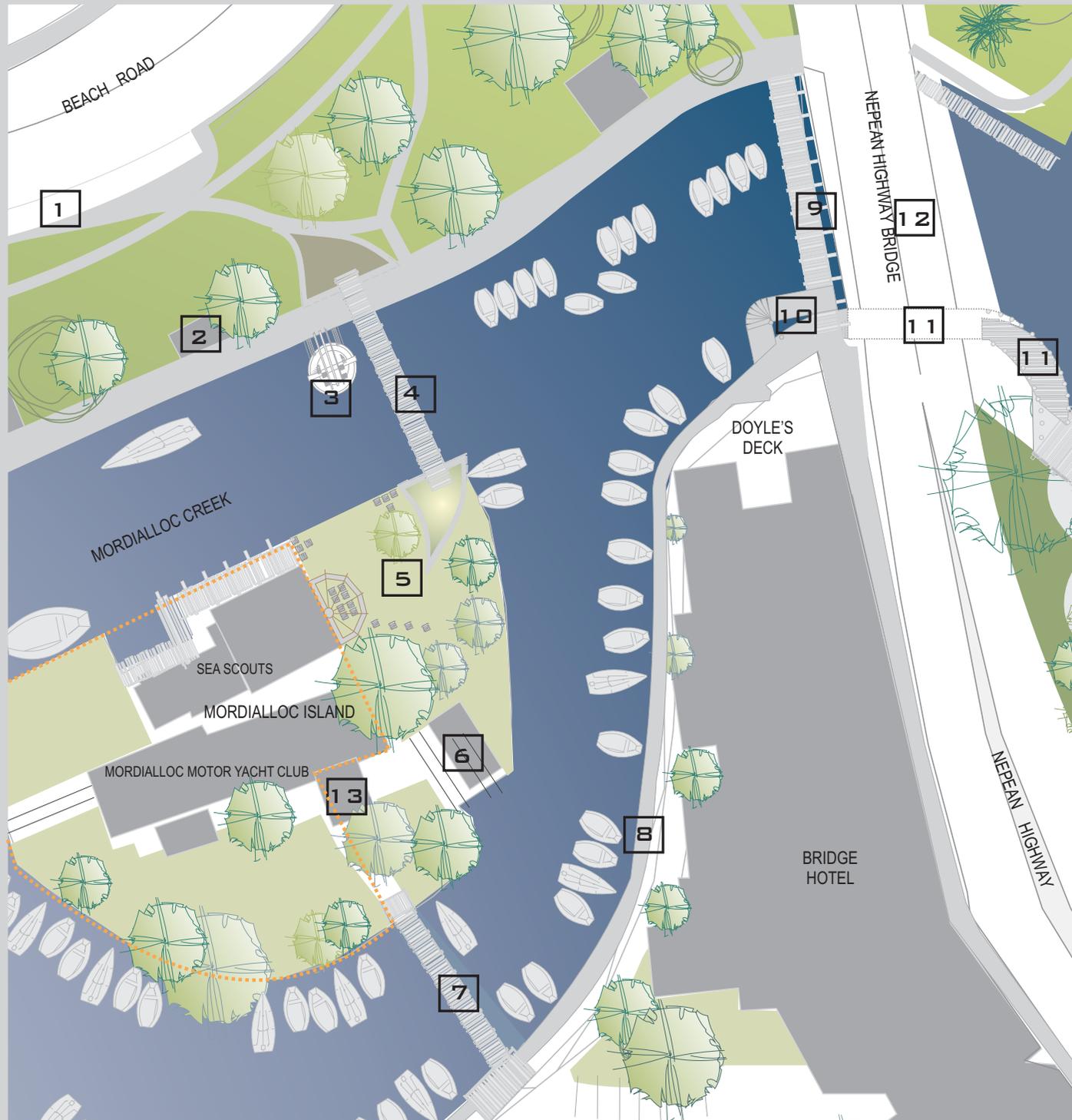
(Refer Figure 11 (a) – Detail Island Access)

The vision for Mordialloc Island is rediscovering the community '*jewel*' in the heart of the Creek. Access to the island is restricted to members of the Mordialloc Motor Yacht Club (MMYC) and the Sea Scouts, via a gated bridge structure from the Doyle's car park. If it is imminent, future access to the island is restricted (as access is currently provided via private property) or if the community indicates a desire to more readily access the island, public access from the northern bank of the Mordialloc Creek (Hazel Pierce Reserve) may be considered. The works may consider the provision of a designated area for public access, and maintaining restricted access to select areas for the MMYC and Sea Scouts. Project elements relating to island access may include:-

- The construction of a low level (to enable disabled access) arch pedestrian bridge linking the northern bank of the Mordialloc Creek to the eastern edge of the Island. The bridge should maintain the existing clearance of the Nepean Highway Bridge, above the high water mark (2 metres).

Balustrades, railing and detail should maximise permeable materials, enabling view lines along the creek. The detailed design should reflect environmental and the boating attributes of the site, and may incorporate public art and interpretive or educational references. The structure should incorporate architectural lighting to guide pedestrian movement and feature architectural elements of the new structure.

- Provide a designated loading area and drop-off pick up zone for the use of the Sea Scouts and the Mordialloc Motor Yacht Club on Beach Road proximate to the new bridge access.
- Explore architectural detailing to north and east building facades of the buildings on the island.
- Construct a designated public island park in the north east corner of the island. The park will form a destination for pedestrians along the northern bank of the river, and may include:-
  - picnic facilities;



- 1** LOADING & RESTRICTED PARKING (SEA SCOUTS & MORDIALLOC MOTOR YACHT CLUB.)
- 2** BOAT HIRE KIOSK
- 3** FLOATING KIOSK STRUCTURES
- 4** LOW LEVEL ARCH PEDESTRIAN BRIDGE. CLEARANCE 2M FROM HIGH WATERMARK. ENABLE DISABLED ACCESS TO MORDIALLOC ISLAND. INCLUDE UPLIGHTING OF STRUCTURE
- 5** POTENTIAL PUBLIC OPEN SPACE INCORPORATING PICNIC FACILITIES. INCLUDE HISTORIC INTERPRETIVE FEATURE DETAILING THE CONSTRUCTION & USE OF THE MORDIALLOC ISLAND, & INDIVIDUALS ASSOCIATED WITH THE SITE.
- 6** POTENTIAL RELOCATION OF SLIPWAY / BOAT HIRE INTO MMYC FACILITY, TO ENABLE CREEK WIDENING
- 7** EXISTING SERVICE CONNECTIONS RETAINED VIA EXISTING BRIDGE, NO PEDESTRIAN ACCESS (SHORT TERM). INVESTIGATE OPPORTUNITIES TO SUBMERGE SERVICES TO THE ISLAND (LONG TERM, TO ENABLE MAST BOAT ACCESS EAST OF MORDIALLOC ISLAND.
- 8** MAINTAIN PUBLIC PEDESTRIAN ACCESS ALONG CREEK SIDE PROMENADE
- 9** PEDESTRIAN BRIDGE: CANTILEVER STRUCTURE FROM HIGHWAY BRIDGE. REINFORCE GATEWAY ENTRY TO MORDIALLOC. ENSURE VIEW LINES TO CREEK IS MAXIMISED. ARCHITECTURAL DETAIL TO RESPOND TO CREEK SETTING & BOATING ACTIVITY
- 10** STEPS TO RIVER LEVEL DECK & PEDESTRIAN UNDERPASS
- 11** PEDESTRIAN UNDERPASS: ATTENBOROUGH PARK / DOYLES LINKAGE (REFER FIGURE 11)
- 12** ARCHITECTURAL LIGHTING INSTALLATION OF BRIDGE STRUCTURES
- 13** INVESTIGATE POTENTIAL FOR SHARED CLUB / PUBLIC AMENITIES

- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- PEDESTRIAN BRIDGE / DECK
- FLOATING KIOSK
- BOAT MOORING
- SLIPWAY
- CLUB ACCESS ONLY

FIGURE 11 (A)

## PIER, CREEK & FORESHORE: DETAIL MORDIALLOC ISLAND ACCESS

Pride of the Bay

- interpretive features detailing the history, range of uses and users of the island;
- signage and pathways to improve linkages to the new park; and
- places for rest and repose.

It is integral the privacy and safety of the Mordialloc Motor Yacht Club and the Sea Scouts premises are ensured.





# 5

## Pompei's Landing

### Context

Key assets within this precinct include the renowned Pompei's Boat Building Yard and associated Seechange Boatworks, administered by the Bayside Skills Training Incorporated. The boat building industry formed a trade within Mordialloc during late last century, however it is the establishment of the Pompei's boat building works in the 1930's, that settled a name and industry that has become synonymous with Mordialloc. The Pompei's Boat Building Yard is one of the only remaining wooden boat building business in Australia, given declining markets for wooden boats.

The precinct is provided with ready access to the Mordialloc Creek and foreshore open space network. Established operations along the creek include boat hire, kiosks and the Mordialloc Boating and Angling Club.

A number of large trees contribute to the enclosed character of the site, although they are upon their cessation. Much of the precinct has been constructed by the Pompei family, including a number of outbuildings, boardwalks and moorings (although in need of replacement) and a boat ramp (Pompei's Landing) contribute to the locally focused, maritime character of the precinct. The presence of boats on the road verge further provide a lasting impression of boat building activities on this site.



*Mordialloc Creek North*

*A low level of surveillance and amenity, and a discontiguous pathway adjoining the creek, facilitates the perception of an isolated creek side environment.*

The site is however challenged by difficult vehicular access into and from the site, low levels of perceived safety (particularly the pedestrian railway underpass), and vandalism. Excessive signage (billboards and business identification) and visual clutter present further challenges to the presentation of the site. A road reservation to accommodate the expansion of the Nepean Highway Bridge comprises the western edge of the site adjacent to Nepean Highway.

The site's significance is further reinforced as it is located at the gateway to Main Street from the south along Nepean Highway, and the north from Beach Road.

### Project Description

(Refer Figure 12 – Pompei's Landing Catalyst Project)

It is envisaged Pompei's Landing will comprise a maritime village fostering the boating history of Mordialloc. It will provide a destination forming part of both the Mordialloc Main Street commercial area and the Mordialloc Creek environs. Facilities may include a restaurant, an educational and interpretive museum, a

new boat building shed, and mixed use residential development on the existing boat building yard. Pedestrian and bike pathways and open space linkages through the site are to be maintained and upgraded. Artistic features, interpretive facilities and design will emphasise the importance of the precinct, providing linkage to previous uses and users of the site and their contribution to Mordialloc.

Pending the long term vision of masted boat access to Pompei's Landing, this site may one day become a short and medium term yachting destination.

### *Project Elements*

A partnership approach is required to achieve general area improvements including the renewal of public open space areas, the northern bank of the Mordialloc Creek, and the provision of a interpretive boat building museum. These investments and facilities are in part dependent on the redevelopment of the boat building yards.

Key project elements within the Pompei's Landing project include:-

- review interest in the redevelopment of Pompei's Landing, and establish public / private partnership based on confirmed heads of agreement between key stakeholders.
- the roundabout at the intersection of Beach Road, Main Street and Nepean Highway should incorporate an up-lit nautical gateway sculpture, reminiscent of the proximate Pompei's boat building activities. The provision of additional sculptural / or real boat storage, within the open space area adjacent to the roundabout, will further reinforce the boat building activities on this site;
- conduct a traffic review of vehicular ingress and egress to the precinct;
- rationalise parking within the precinct;
- review established vegetation. The review should acknowledge the contribution established vegetation makes to character of the precinct, and make recommendations for the retention, removal, ongoing management and replacement of species within the precinct including revegetation of the creek bank;
- the provision of an open space linkage adjacent to the eastern and southern boundaries of the precinct. Improving access to (and perceptions of access to) an open space link along the northern edge of the Creek, which will enable significant improvements to pedestrian amenity and safety. It will further provide a contiguous pedestrian connection between Hazel Pierce Reserve to the west, and the George Woods Reserve to the east.

The extension of the open space adjacent to the Nepean Highway, (within the west segment of the precinct) will provide a strong landscape link from the Creek north towards Main Street. The extension of palm

- 1 ADDITIONAL ON-STREET PARKING TO ACCOMPANY REDEVELOPMENT ALONG MAIN STREET SOUTH
- 2 NAUTICAL GATEWAY SCULPTURE
- 3 RELOCATE BOAT BUILDING WORKS & REDEVELOP TO INCORPORATE ACTIVE COMMERCIAL OPERATION TO STREET LEVEL & UPPER FLOOR RESIDENCES.
- 4 INTERACTIVE NAUTICAL SCULPTURAL ELEMENT REPRESENTATIVE OF THE STORAGE OF BOATS ON THE ROAD VERGE
- 5 EXTEND LINE OF MAIN STREET PLANTING
- 6 ENABLE PEDESTRIAN ACCESS TO MAIN STREET EAST
- 7 REFURBISH & REUSE EXISTING STRUCTURES ADJACENT TO THE CREEK INCLUDING MORDIALLOC BOATING & ANGLING CLUBS
- 8 REVIEW VEHICULAR INGRESS & EGRESS TO THE PRECINCT
- 9 BOAT STORAGE
- 10 CAFE & BOAT BUILDING MUSEUM
- 11 RELOCATE EXISTING BOAT BUILDING WORKS INTO A NEW BOAT BUILDING FACILITY & MARITIME WORKSHOP: INCREASE OPPORTUNITIES FOR PASSIVE SURVEILLANCE & SAFETY ADJACENT TO THE RAILWAY LINE BY ENSURING DEVELOPMENT ADDRESS THE CREEK, OPEN SPACE AND PEDESTRIAN PATHWAYS THROUGH TRANSPARENT BUILDING FORM TO ENABLE VISUAL PERMEABILITY.
- 12 PUBLIC AMENITIES
- 13 BOAT HIRE & SLIPWAY
- 14 FACILITATE PEDESTRIAN ACCESS ALONG NORTHERN BANK OF THE MORDIALLOC CREEK
- 15 PARKING
- 16 IMPROVE PEDESTRIAN SAFETY & ACCESS ACROSS THE RAILWAY LINE. REMOVE THE EXISTING UNDERPASS AND PROVIDE A FLOATING PEDESTRIAN UNDERPASS ADJACENT TO THE MORDIALLOC CREEK. INCORPORATE LIGHTING & ENSURE CLEAR LINES OF SIGHT ACROSS THE STRUCTURE.

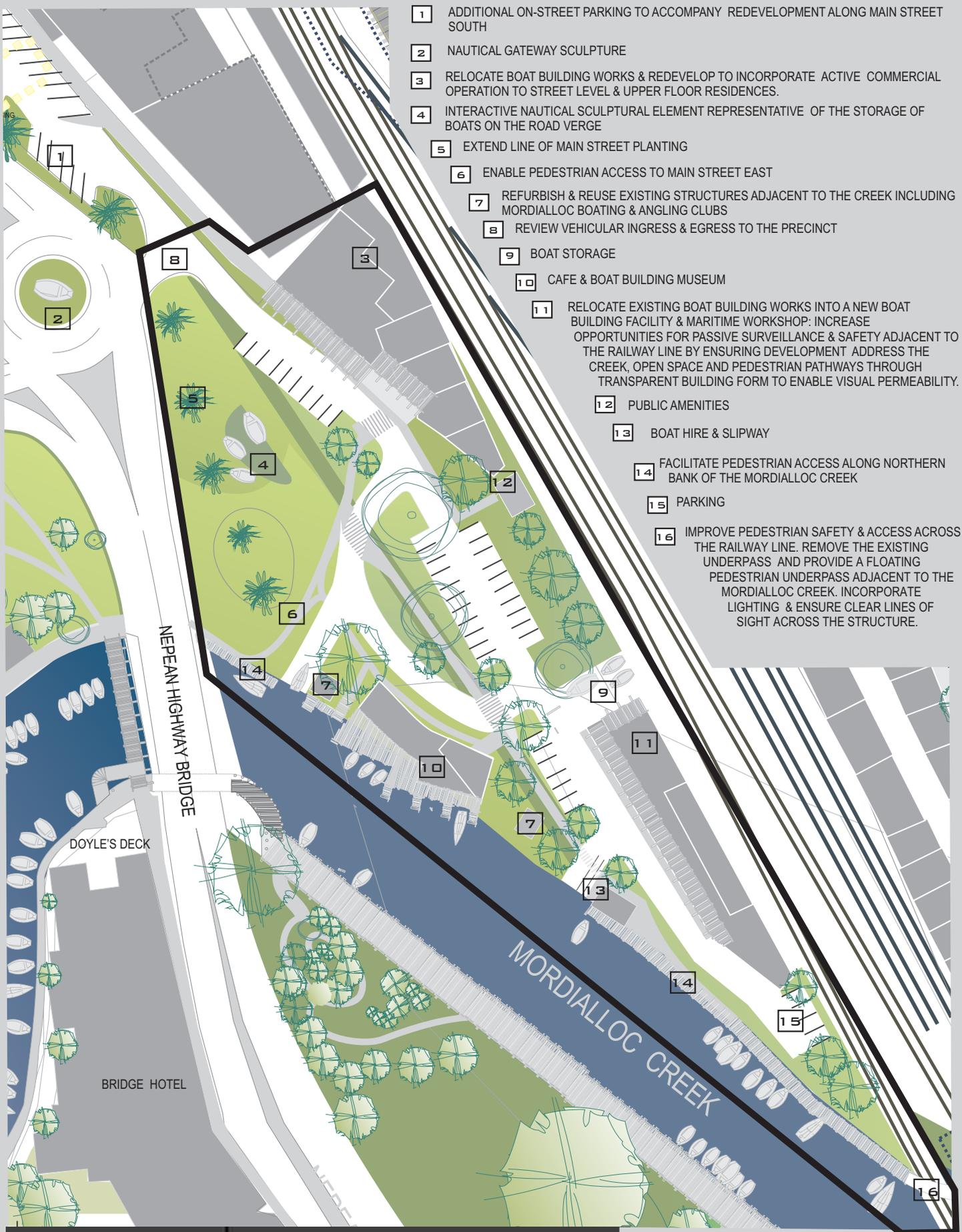
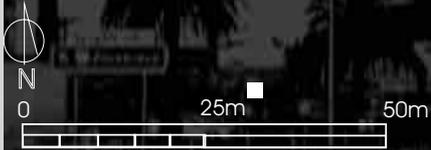


FIGURE 12  
**POMPEI'S LANDING  
 AREA NO.5**



*Pride of the Bay*



- POMPEI'S LANDING PROJECT AREA
- OPEN SPACE
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- RAILWAY LINE

planting into the precinct will further reinstate the transition between Main Street and the creek, extending land use and cultural themes;

- the relocation of the existing Pompei's Boat Building and Seechange Boatworks yards (refer below) and their replacement with a mixed use, residential development. The development should comprise active frontage to the street level, while residential or other activities locate at upper levels, taking advantage of views to the Bay along Mordialloc Creek;
- the relocation of the existing boat building activities, into a new complex, west of the railway line, adjoining the Mordialloc Creek. The new facility will enable the continued existence of a moderately scaled Pompei's operation, within an improved facility.

Ancillary to the boat building complex, provide a boat hire facility (including slipway) adjacent to the Creek. Enable appropriate signage and presence to ensure the viability of this operation;

- provide a destination on the waterfront: a restaurant or café on the water's edge will provide an anchor within the precinct. It is envisaged the new facility will draw on the environmental and the local historic characteristics of the site.

A central component of the waterfront destination will include a museum or exhibit ancillary to the restaurant / cafe facility. This will provide a purpose built display, showcasing the development of the boat building works within the precinct, and the current day workmanship within the Pompei's Boat Building Works;

- review demand to accommodate office and administrative functions of waterbased clubs and organisations, which will reinforce boating and maritime activities within this precinct;
- the southbound extension of the Main Street east pedestrian pathway through the precinct to meet the creek;
- disable pedestrian access to the railway underpass in its current location. Route the pedestrian pathway to the creek, with access to the George Woods Reserve via a floating pedestrian underpass beneath the railway. Architectural lighting of the railway bridge and new floating underpass, and remedial works to the northern bank of the Mordialloc Creek will also form component elements;
- Reconstruct the pedestrian pathway, jetties and decks adjacent to the northern banks of the Mordialloc Creek.
- Refurbish, renew and improve existing sheds, slipways and boat ramps, used by community fishing and boating groups including the Mordialloc Boating and Angling Club. Restrict access to facilities, and encourage wider public access to the boat ramp and facilities west of the rail line.



Pedestrian Access

*Pedestrian access through the Pompei's site is characterised as providing poor surveillance and a low level of amenity. Future pedestrian links to the George Woods Reserve, should build on the presence and amenity of the creek.*

## Design Guidelines

- The redevelopment of the Pompei's boat building yard, will provide a scale of development consistent with the established 10 metre<sup>18</sup> (approximate calculation) high workshop on this site. It is proposed the development will comprise a maximum building height of 3 storeys (11 metre building height) to the street frontage and a recessed 4<sup>th</sup> storey (14 metre building height) to the rear of the site. Underground car parking may be accommodated within the development.

*Mordialloc Boat Works Redevelopment  
Building heights*



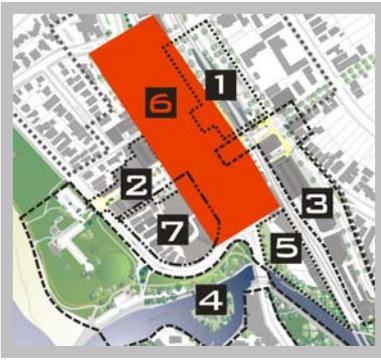
- The new boat building facility will comprise a maximum building height of 10.5 metres, similar to the height of the existing boat building work shed.
- New buildings on the creek should comprise a maximum building height of 2 storeys (7.5 metre building height).
- Retain and reuse existing creek side structures where appropriate, particularly structures representative of the early historic development of the precinct.
- In select locations a mix of uses, including a minor residential component, secondary to the principal recreational, boating and maritime activities, may be accommodated where it can be demonstrated there are direct improvements to safety, surveillance and amenity.
- Refer to the '*Kingston Foreshore and Waterways Landscape Guidelines*' for the Management of the landscape adjacent to the Mordialloc Creek.
- The form and materials of new development and works within the public realm should provide reference to the boat building and maritime activities within the site.
- Where the railway line is not adjoined by buildings, the landscape design should seek to minimise its visual presence without compromising safety and surveillance.



*The form and materials of new development & works should provide a reference to boat building activities within the site.*

<sup>18</sup> The existing boat building shed comprises an approximate building height of 10 – 10.5 metres.





# 6

## Main Street Precinct

### Context

The Main Street precinct includes the two established retail strips adjacent to Nepean Highway and Main Street. It includes intact illustration of the historic development of Mordialloc, through a series of historic buildings and memorials. The exceptional Canary Island Palm boulevard planting dominates impressions of Main Street.

Within the retail environment it is noted:-

- large areas of disinvestment and poor tenancy presentation, diminish perceptions of the street;
- poor quality paving, public furniture and landscape boxes within the public realm encroach on pedestrian movement;
- disabled access is challenged by significant topographical grade changes. It is further impeded by kerb and channel grade changes at intersections and pedestrian crossing points; and
- premises facing Main Street are provided with direct vehicular access, with Main Street accommodating extensive angle parking on the street and within centre medians.



*Trading Environs*  
 Poor placement of public furniture and landscape features restrict pedestrian movement along Main Street.

Perceptions of streetscape and built form quality along Main Street are further challenged by:-

- the dominance of highway scale lighting along the length of the street;
- the extrusion of telecommunication facilities above building facades; and the
- prevalent overhead wires.

Main Street principally comprises retail activity. As this precinct only contains a few select tenancies that provide a focus for night time activities, Main Street comprises a low level of on street activity after dark. The precinct is under increased pressure to accommodate increased building heights. New forms of development should respond to identified characteristic elements that comprise Mordialloc, and building heights as directed under the Built Form Framework of this strategy.

The physical separation of Main Street east and Main Street west is further impeded by difficult pedestrian access across the street. Projects will seek to review pedestrian accessibility and permeability through this precinct.

## Project Description

(Refer Figure 13 – Main Street Precinct)

Projects within the Main Street precinct will improve pedestrian amenity and the retail environment. Collectively, between identified projects and a series of design guidelines to facilitate appropriate new forms of development within the private realm, Main Street will continue to evolve as a highly amenable mixed use retail environment, identified through prevalent historic elements.

## Project Elements

- Review the pedestrian, and commercial benefits to be derived by facilitating pedestrian access east-west across Main Street. An integrated urban design and traffic study should review current and projected pedestrian flows within the centre, and propose detailed options to accommodate additional crossing points. Options for review should include coordinated central, north and south pedestrian lights, namely:-
  - northern pedestrian lights (between 554 Main Street west and 475 Main Street east) setback approximately 23 metres from the intersection;
  - the provision of central pedestrian lights coordinated with a signalised intersection to Bear Street, and the existing pedestrian crossing; and
  - southern pedestrian lights between the former Police Station site, Main Street east and No.626 Main Street west.

The installation of a variable speed sign along Main Street to reduce vehicular speeds during critical retail period (Saturday Mornings) and outside of traffic peaks should also form part of any review.

- Take advantage of redevelopment on hill crests, and encourage the relocation of existing telecommunication facilities within an integrated building design. Discourage telecommunication facilities that extrude above building facades and parapets, where visible from Main Street, the Creek and Foreshore and the Principal Pedestrian Pathway. Facilities may be enabled where integrated through screening into less profiled building facades.
- Replace highway scale lights within the centre medians that excessively exceed the established palm and building heights. Replace with signature low energy lights, that do not exceed the height of Main Street's signature tree.



*Main Street Telecommunications*  
Avoid excessive visual clutter within the streetscape, and encourage telecommunications facilities to co-locate within new development, within an integrated building design



- MAIN STREET PRECINCT AREA
- LANDSCAPE STRIP
- PEDESTRIAN SPACE SYSTEM
- BUILDING FOOTPRINT
- SHARED PEDESTRIAN / VEHICLE AREA
- EXISTING PALM TREE / NEW PALM TREE
- EXISTING BUILDING OUTLINE
- PROPERTY BOUNDARY / EXISTING CAR PARK

**MAIN STREET DIRECTIONS**

WITHIN THE RETAIL CORE PROVIDE ACTIVE AND PERMEABLE STORE FRONTS & ON STREET DINNING.

ENCOURAGE AMENABLE STOREFRONT PRESENTATION & ON STREET DISPLAYS.

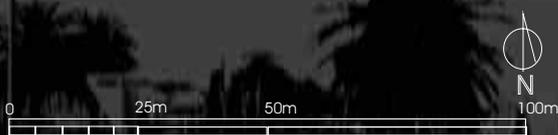
LIGHTING OF STOREFRONT DISPLAYS OUTSIDE OF NORMAL TRADING HOURS.

NEW STREETScape FURNITURE, PLANTER BOXES & DIRECTIONAL SIGNAGE.

EXISTING CANARY ISLAND PALMS TO BE RETAINED & TIDIED

- 1 REMOVE STREET LIGHT. UPLIGHT FEATURE TREE / SCULPTURAL ELEMENT AT NORTHERN ENTRY TO THE MORDIALLOC COMMERCIAL CENTRE
- 2 GATEWAY BUILDING MIXED USE RETAIL / RESIDENTIAL PROVIDE CONSOLIDATED ENTRY TO REAR CAR PARK FROM MAIN ST & EPSOM ROAD
- 3 STUDY TO DETERMINE SUITABLE LOCATIONS FOR ADDITIONAL PEDESTRIAN CROSSING POINTS TO FACILITATE PEDESTRIAN PERMEABILITY ACROSS NEPEAN HIGHWAY.
- 4 INTEGRATION OF TELECOMMUNICATIONS FACILITIES TO WITHIN NEW DEVELOPMENT. FACILITIES SHOULD NOT BE GENERALLY VISIBLE FROM MAIN STREET.
- 5 SIGNATURE LIGHTING TO MAIN STREET. REDUCE HEIGHT OF HIGHWAY LIGHTS. CONTAIN WITHIN PALM TREE HEIGHT.
- 6 MAIN STREET PAVING UPGRADE. INCLUDE BANDING (FACILITATE PEDESTRIAN SENSE OF MOVEMENT) WITHIN THE RETAIL ENVIRONMENT
- 7 UPLIGHT HISTORIC CLOCK TOWER
- 8 SIGN ACCESS TO CAR PARKING DECK FROM MAIN STREET. FACILITATE PEDESTRIAN LINKAGES TO DESIGNATED OFF STREET CAR PARKING AREAS
- 9 SAFEWAYS FACADE TREATMENT. REINFORCE VERTICAL ELEMENTS THAT REFLECT THE RHYTHM & SPACING OF HISTORIC DEVELOPMENT ALONG MAIN STREET.
- 10 INVESTIGATE PEDESTRIAN CROSSING & LIGHTS AT BEAR STREET INTERSECTION
- 11 SETBACK DEVELOPMENT TO ENABLE TWO WAY VEHICULAR ACCESS TO REAR OF PREMISES.
- 12 16 OFF STREET CAR PARKING SPACES.
- 13 INCREASE PRESENCE OF HERITAGE FACADES ALONG MAIN STREET. UPLIGHT HERITAGE FACADES. INCREASED SETBACK OF THIRD STOREY ADDITIONS TO HERITAGE BUILDINGS
- 14 ADDITIONAL ON-STREET PARKING
- 15 NAUTICAL GATEWAY SCULPTURE
- 16 BUS STOP

FIGURE 13  
**MAIN STREET PRECINCT  
 AREA NO.6**



*Pride of the Bay*

- The development of an urban design framework to direct works within the public realm. It is anticipated works will comprise streetscape improvements ie pavement. Ensure the incorporation of banding, insets or artistic detailing to facilitate a sense of transition and movement within the retail environment. Ensure new furniture, street trees and planter boxes, facilitate pedestrian movement and provide timely locations for rest, repose and gathering.
- Conduct programs and provide incentives for trader improvements to store front presentation and fit out, external building improvements and signage. Ensure the on-street display of goods and services contribute to the quality and attraction of the street, and do not present excessive clutter.
- Undertake a lighting strategy. Component elements may include the up lighting of heritage and noteworthy building facades, the palms and memorials within the street. The lighting strategy should seek to facilitate evening pedestrian movements between activity nodes and areas of interest.
- Facilitate access to off street car parks and deck structures through directional signage from Main Street.
- Undertake architectural treatment to the Safeway's Façade. The façade should reinforce the historic rhythm of commercial street frontages within Main Street through the incorporation of vertical elements, materials and details that reflect these elements. The incorporation of additional vertical fenestration may reinforce Main Street Characteristics.
- Consider the continuation of median and the kerb angle parking along Main Street east, south of Bear Street.

Component elements of the Main Street project area include reinforcing the northern and southern point of entry into the centre, by:-

- The extension of the palm boulevard north and south of the centre.
- The roundabout at the intersection of McDonald Street, Main Street and the Nepean Highway is an opportune location to incorporate a feature element marking the entry to the Mordialloc commercial area. The removal of the existing highway light, and replacement with an up-lit feature tree or sculptural element and sign to reinforce the northern entry point.
- The roundabout at the intersection of Beach Road, Main Street and Nepean Highway should incorporate an up-lit nautical gateway sculpture, reminiscent of the proximate Pompeii's boat building activities.

- Reinforce the northern gateway through the redevelopment of the site west of the junction of McDonald Street and Nepean Highway, south of Epsom Road. The gateway building may accommodate a mixed use development, comprising retail and commercial street level activities, and upper level residences or serviced accommodation. The development should locate vehicular parking at the rear of the site, comprising consolidated points of entry from Main Street, and exit onto Epsom Road.

The redevelopment should reinforce a contiguous building alignment to the streetscape and maintain vertical elements that reflect the historic rhythm of commercial street frontages within Main Street. The development should further consolidate and extend the pedestrian pathways at the site's street frontage.

## *Design Guidelines*

Recognisable infill and redevelopment opportunities exist along Main Street. Objectives for future development are centred around ensuring the integrity of historic buildings, and the dominance of the Palms. New development should therefore:-

- Not overwhelm identified heritage buildings of regional, architectural & historic contribution or streetscape value (refer to Figure 7 – Heritage Framework). New development should reinforce the perception of a traditional two storey building height along Main Street.
- Setback third storey additions may be incorporated where it is ensured new buildings (particularly those adjacent to Main Street heritage buildings) are sympathetic to the significance and character of these heritage places.
- Provide pedestrian protection from the elements through the provision of contiguous awnings, verandas and canopies.
- The form of new development should reflect the rhythm of store fronts facing Main Street. Ensure the 7 – 9 metre spacing of store frontages to the street, is reinforced through the provision of similar width store frontages, and the incorporation of building elements that reinforce this pattern of development at upper floor levels. Tenancies requiring large store footprints, should reinforce the 7 – 9 metre spacing, through the use of columns or breaks in fenestration.

### Built Form

Building heights should refer to the Built Form Framework contained within Section 5.0 of this report.

- provide a zero lot alignment to the street frontage, and encourage buildings to physically address the street through permeable fenestration and points of entry into the building.

### *Street Frontage : Building Height*

- Directly adjoining the street frontage of the site, the maximum height of buildings must not exceed two storeys (7.5 metre building height).
- The maximum height of the building on the street frontage may be increased where the height of heritage buildings in immediate context of the site exceed 7.5 metres, but must not exceed a two storey building height.
- New development should align with the prevailing horizontal awning and parapet level of the existing streetscape and seek to be viewed as an integrated infill form.
- The height of the new development adjacent to the street frontage must not exceed the maximum height of established heritage buildings.
- Where the development is between two heritage buildings, the maximum height of new development at the street frontage should not exceed the lesser building height.

### *Recessed Third Storey : Building Height*

- A maximum building height of three Storeys (11 metres) may be accommodated where development is recessed from the street. The setback distance of the upper most third storey should correspond to the heritage significance of the site, and should be as follows:
  - Buildings of Architectural Significance: 8m setback
  - Buildings of Streetscape Value: 4m setback
- The proposed setback conditions are not designed to make upper level development above two storeys invisible, rather recessive. The proposed setbacks of 4m and 8m respectively should allow for clear viewing of the total intact building elevation without the intrusion of any dominant upper level projection.

### Principal Pedestrian Path : Bear Street & Centreway

- Ensure sunlight access to the Principal Pedestrian Pathway at 2pm at the Winter Solstice. Buildings on the northern side of Bear Street and Centreway:-
  - should provide a maximum building height of 6.5 metres to the street;
  - may accommodate a third storey providing a maximum building height of 11 metres may be accommodated where it is recessed 9 metres from the street frontage. This setback may be reduced where the building height is decreased, and where it can by illustrated sunlight access to the street as described above.
  - Balustrades and structures enabling sunlight permeability may encroach within the above building envelope. Setbacks may be used to accommodate balconies and open air decks.



## Central Mordialloc

### Context

Within the Central Mordialloc Precinct, the Beach Road foreshore arc, comprises a significant composition of urban form, defining the commercial and residential edges of the centre from the foreshore and creek open space areas. Central Mordialloc comprises:-

- a significant change in grade between the Main Street commercial area and Beach Road;
- an isolated and disjoint residential area, proximate to and with views of the foreshore and creek. Residential buildings comprise a mix of detached and mutli-dwelling development contained within ageing building stock. Many of the site's are provided with difficult vehicular access to Beach Road, particularly where premises are located on the Beach Road arc;
- a laneway that provides access to the rear of properties facing Beach Road. It further provides access to a garage structure for units fronting Beach Road;
- poor levels of amenity (surface, width, landscaping) along the Beach Road pedestrian path;
- a dominant building comprising the rear of the Safeway's building, which is visible from the foreshore and Port Phillip Bay.



### Description

(Refer Figure 14 – Central Mordialloc Precinct)

Central Mordialloc will comprise a substantially improved mixed use residential environment, providing a transition between the core commercial activities along Main Street, and the foreshore environment. The strategy seeks to provide a sensitive building interface to the foreshore and adjoining open space resources.

The consolidated redevelopment of existing residential premises will enable a mixed use residential environment, where residents will dwell proximate to the finest aspects of Mordialloc's offer. Consolidated vehicular access will substantially improve safety along the Beach Road arc.

## *Project Elements*

- Incorporate a pedestrian (shared pedestrian vehicle zone) connection between Beach Road and Main Street. The laneway should incorporate textured road surfaces to reduce vehicle speeds. Development adjoining the laneway should physically address the lane, through direct points of access to dwellings at ground levels and fenestration. Alternatively, create visual interest through public art, landscaping or vertical planting to building facades.
- Landscape and amenity improvements to Beach Road, adjacent to the pedestrian pathway. Include hardy coastal planting, and extend landscape strips over disused driveways. Incorporate coastal planting within new development and facilitate the establishment of coastal landscaping between street frontages and established units.

### Medium Density Mixed Use / Residential Environment

It is envisaged, site consolidation will enable the integrated redevelopment of residential sites adjacent to Beach Road. The development should:-

- restrict vehicular access to the new development via a single entry / exit point to Beach Road and an additional vehicular access point via the existing laneway to Main Street;
- provide undercroft parking;
- should disallow direct lines of sight to the rear of the Safeway's building from Beach Road, the foreshore & Port Phillip Bay;
- improve pedestrian perception of safety and engagement on the street, by enabling direct points of access to individual dwellings from Beach Road and the laneway. Particularly along Beach Road, encourage a lively mixed use streetscape comprising residential, shopfronts and café's;
- enable flexible ground floor use (residential, small scale commercial or retail operators, home office or commercial services).

-  CENTRAL MORDIALLOC PRECINCT
-  LANDSCAPE STRIP
-  PEDESTRIAN SPACE SYSTEM
-  BUILDING FOOTPRINT
-  EXISTING BUILDING OUTLINE
-  SHARED PEDESTRIAN / VEHICLE AREA
-  AREA FENESTRATION



- 1** LOADING
- 2** UNDERCROFT PARKING : 43 (APPROX)
- 3** SAFEWAYS STAFF PARKING : 11 (APPROX). ENABLE PEDESTRIAN LINK TO CENTREWAY / SAFEWAYS FROM NEW RESIDENTIAL DEVELOPMENT. NO VEHICLE ACCESS
- 4** ENCOURAGE COASTAL LANDSCAPING BETWEEN STREET FRONTAGE & ESTABLISHED UNITS
- 5** CONSOLIDATE VEHICLE ENTRY / EXIT TO BEACH ROAD.
- 6** UNDER CROFT PARKING TO NEW RESIDENTIAL / MIXED USE DEVELOPMENT
- 7** PEDESTRIAN CONNECTION : BEACH ROAD - MAIN STREET (SHARED VEHICLE / PEDESTRIAN ZONE. TEXTURED ROAD SUR FACES TO REDUCE VEHICLE SPEEDS.
- 8** PEDESTRIAN ACCESS ONLY
- 9** STREET FURNITURE
- 10** HIGHER DENSITY MIXED USE / RESIDENTIAL DEVELOPMENT.

CONSOLIDATION OF SITES ADJACENT TO BEACH ROAD. RESTRICT VEHICULAR ACCESS TO NEW DEVELOPMENT TO A SINGLE ENTRY TO BEACH ROAD & CURRENT POINT OF ACCESS FROM LANEWAY TO MAIN STREET.

NEW DEVELOPMENT SHOULD DISALLOW DIRECT LINES OF SIGHT TO THE REAR OF THE SAFEWAYS BUILDING FROM BEACH ROAD, THE FORESHORE & PORT PHILLIP BAY.

FRONTAGE TO BEACH ROAD & PEDESTRIAN CONNECTION SHOULD PHYSICALLY ADDRESS THE STREET THROUGH, DIRECT PEDESTRIAN POINTS OF ACCESS. GROUND FLOOR DEVELOPMENT FORMS SHOULD ENABLE FLEXIBLE USE AS A RESIDENTIAL AND SMALL SCALE COMMERCIAL PREMISES (HOME OFFICE, SPECIALTY RETAIL, SERVICE BASE).

CREATE VISUAL INTEREST THROUGH LANDSCAPING & VERTICAL PLANTING TO BUILDING FACADES

- 11** IMPROVE LANDSCAPE TREATMENT OF BEACH ROAD ADJACENT TO THE PEDESTRIAN PATHWAY. INCLUDE HARDY COASTAL PLANTING EXTEND LANDSCAPE STRIPS OVER DISUSED DRIVEWAYS
- 12** LOADING & RESTRICTED PARKING (SEA SCOUTS & MMYC)
- 13** EXTENSION OVER CAR PARK OF COURT TO MAIN STREET. FACILITATE VIEWS THROUGH TO THE DANDENONGS.

FIGURE 14  
CENTRAL MORDIALLOC  
PRECINCT AREA NO. 7



*Pride of the Bay*

## *Design Guidelines*

### Built Form

Building heights should refer to the Built Form Framework contained within Section 5.0 of this report.

- The detailed design of new development should establish a rhythm of consolidated and attached built forms along Beach Road with discreet building breaks that emphasise intersecting openings and laneway.
- Buildings should be built hard to the Beach Road frontage with clear and visible ground level activity including fenestration and building entry off the street frontage
- Vehicle access to basement or undercroft car parking should not be achieved directly off Beach Road and should only occur via Centreway.
- Roof form and building silhouette should be noticeable at the junction of Beach Road and the Highway, and should draw upon the sensitive arc of Beach Road.

### *Building Height*

- Directly adjoining the street frontage of the site, the maximum height of buildings must not exceed 2 storeys (7.5 metre building height).
- A maximum building height of 3 Storeys (11 metres) may be accommodated where development is setback 4m from the street frontage to ensure that the building reads as a 2 storey form from the abutting pedestrian footpath. Sensitively designed apartment terraces and balconies may be located within the setback.
- The building height may be increased to screen the Safeway's Façade from the foreshore and Beach Road.

## 7.0 Implementation

The 'Mordialloc: Pride of the Bay Structure Plan' will be implemented through the development of controls incorporated into the Kingston Planning Scheme, and a range of public and private sector projects. Although not the sole stakeholder, it is envisaged the City of Kingston will establish:-

- processes and policies which direct intervention (i.e. boundary realignments, site consolidation);
- coordinated partnerships and incentive based approaches to achieve physical change; and
- a policy and regulatory framework, to ensure future development opportunities are not compromised by site specific development.

### Kingston Planning Scheme

A planning scheme amendment will be prepared to incorporate recommendations within the *Mordialloc: Pride of the Bay Structure Plan* into the Kingston Planning Scheme. Although intended as a guide, it is envisaged the amendment will comprise:-

- an overriding strategic vision and series of precinct specific performance based guidelines and objectives within a *local policy* under the *Local Planning Policy Framework*.
- a series of building height controls (number of storeys and building heights) and qualitative guidelines (i.e. sunlight and shade access, fenestration, detailing, sightlines) to direct built form to be contained within a *Design and Development Overlay*.
- selected precincts and sites will be placed in the Schedule to the Heritage Overlay. This work will comprise the recommendations of Council's Heritage Study, currently being implemented through the exhibition of Amendment C46.

The determination of the most appropriate planning controls to integrate the *Mordialloc: Pride of the Bay Structure Plan* into the Kingston Planning Scheme, must be determined upon consultation of the Department of Sustainability and Environment to best ensure Ministerial Approval.

## Project Implementation

It is desirable that the Structure Plan addresses matters raised by the Mordialloc community and key stakeholders. The implementation framework therefore seeks to:-

- address remedial issues and conduct higher profile improvements;
- undertake projects that establish a framework for wider catalyst projects;
- facilitate catalyst projects through a partnership approach to works;
- review and seek opportunities to achieve the longer term directives and projects.

### *Implementation Framework*

(Refer Figure 15 – Implementation Plan)

Considering the project elements ability to implement the strategic directions under section 4 of this report, project elements have been prioritised into four categories, namely: -

1

→ Priority 1 – Project Under Development  
(*Estimated Completion 2004 – 2009*)

These comprise projects that are currently under development, or which are contained within Council's current works program.

2

→ Priority 2 - Foundational  
(*Estimated Completion 2005 – 2015*)

These are highest priority projects which are envisaged to significantly improve the function and amenity of Mordialloc. These projects are integral in providing a framework for the attraction of directed investment within the centre.

3

→ Priority 3 – Medium Term  
(Estimated Completion 2005 – 2020)

High Priority projects which will achieve the ongoing improvement and development of the centre.

4

→ Priority 4 – Longer Term  
(Estimated Completion 2005 – 2025)

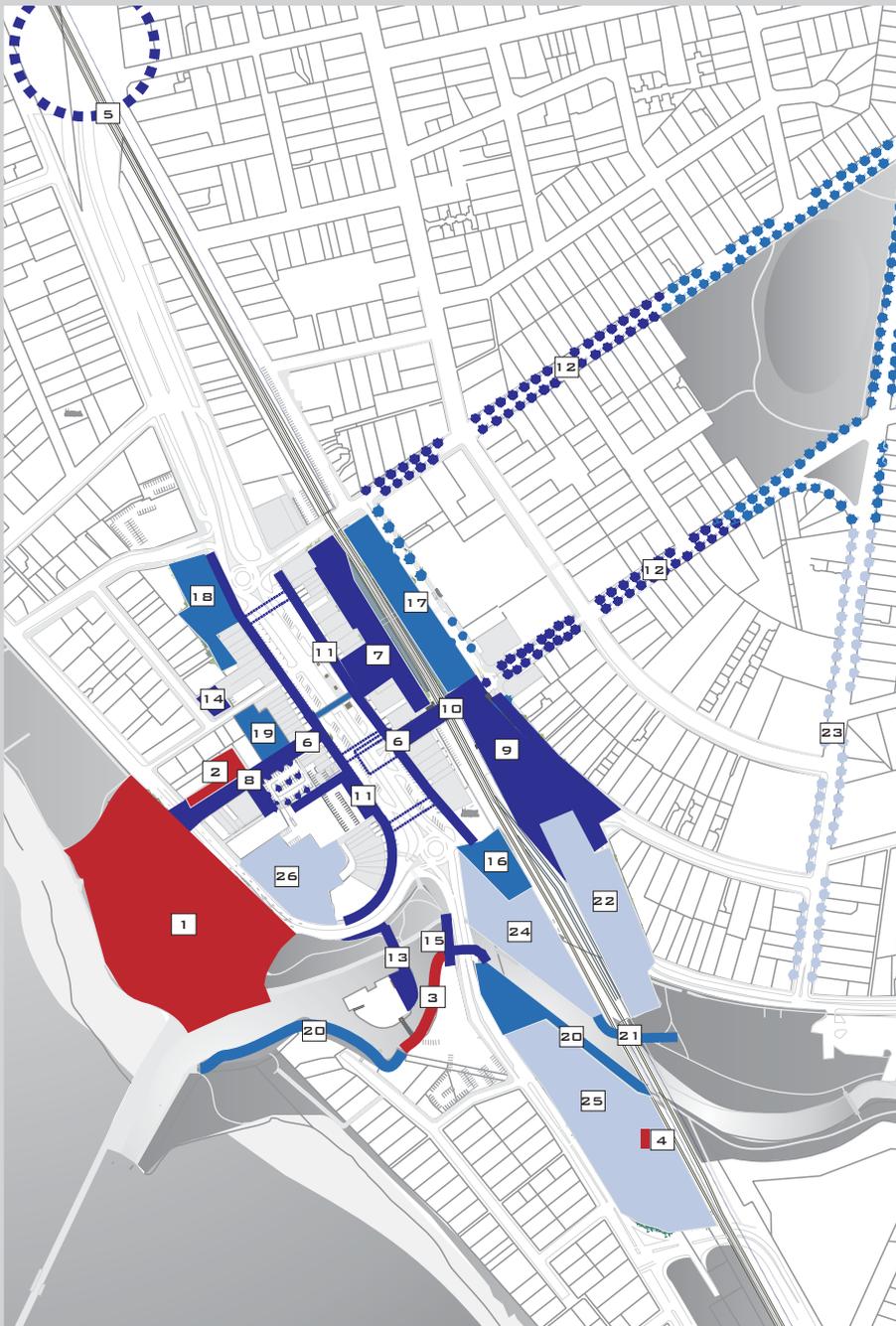
Provide identifiable ongoing development opportunities and works within the centre.

The framework is an indication of the priority of works forming part of the Pride of the Bay Structure Plan. Frequently opportunities arise, which may contribute to the advanced implementation of project elements or works. Council should ensure prospects are addressed as they arise, particularly where benefits may be achieved through inter-organisation, or public / private partnerships. Council must also be mindful that its finite resources for capital works must be equitable divided across the municipality which may at times impact on the advancement of certain projects.

### *Funding Sources*

Funding sources to implement projects within the structure plan, may be gained from a series of sources. Some of which may include:-

- *Council funding* including selected redevelopment of land assets within the centre to fund works within the *Pride of the Bay* area, direction of capital works budgets and reviewing ongoing maintenance and works conducted within the centre;
- *State Government*, through pride of place grants to fund urban improvements to open space assets and key urban design components of the plan.
- *Vic Roads* to fund pedestrian crossings and bike paths where new works result in the displacement of existing facilities.
- *VicTrack* in the rationalisation of the station parking areas and the sale of redundant land holdings.
- *Development contributions* obtained through negotiation with private sector development interests.
- *Joint Venture Projects* undertaken in conjunction with the Private Sector through the use of underutilised existing State or Local Government land assets.



**PRIORITY 1 : PROJECT UNDER DEVELOPMENT**

2004 - 2009

- 1 PETER SCULLIN RESERVE
- 2 CENTREWAY RESIDENTIAL REDEVELOPMENT
- 3 DECK RECONSTRUCTION: MORDIALLOC CREEK SOUTH
- 4 JUNIOR SKATE FACILITY

**PRIORITY 2 : FOUNDATIONAL**

2005 - 2015

- 5 GATEWAY TREATMENT
- 6 PRINCIPAL PEDESTRIAN PATHWAY
- 7 MORDIALLOC RAILWAY STATION (WEST)
- 8 CENTREWAY URBAN DESIGN IMPROVEMENTS
- 9 TIMBER YARD REDEVELOPMENT: NORTH
- 10 BEAR STREET URBAN DESIGN WORKS
- 11 MAIN STREET PEDESTRIAN ACCESS REVIEW & STREET SCAPE WORKS
- 12 STREET TREE PLANTING
- 13 MORDIALLOC ISLAND ACCESS
- 14 OWEN STREET RESIDENTIAL DEVELOPMENT
- 15 NEPEAN HIGHWAY PEDESTRIAN BRIDGE & UNDERPASS

**PRIORITY 3 : MEDIUM TERM**

2005 - 2020

- 16 POMPEI'S LANDING (NORTH)
- 17 MORDIALLOC RAILWAY STATION (EAST)
- 18 NORTHERN GATEWAY REDEVELOPMENT
- 19 CENTREWAY CAR PARK STRUCTURE
- 20 MORDIALLOC CREEK SOUTH (PEDESTRIAN PATH)
- 21 RAILWAY UNDERPASS

**PRIORITY 4 : LONG TERM**

2005 - 2025

- 22 TIMBER YARD REDEVELOPMENT SOUTH
- 23 STREET TREE PLANTING
- 24 POMPEI'S LANDING (SOUTH)
- 25 ATTENBOROUGH PARK
- 26 BEACH ROAD RESIDENTIAL RENEWAL

FIGURE 15

**MORDIALLOC STRUCTURE PLAN:  
IMPLEMENTATION FRAMEWORK PLAN**

0 0.5km 1km

*Pride of the Bay*

